



Middleton City Council

Findings of Facts, Conclusions of Law & Order

In the Matter of the applications of M3 and JUB Engineers for annexation/rezone, preliminary plat, development agreement, and comprehensive plan map amendment with respect to the Quarry East Subdivision located at 21500 Middleton Road and 11436 Lincoln Road:

A. Findings of Fact:

1. Hearing Facts: See Staff Reports for the hearing dates of July 20, 2022 and August 3, 2022, which Reports are attached hereto as Exhibit "A" and "B", respectively, and incorporated herein by this reference.
2. Process Facts: See Staff Reports for the hearing dates of July 20, 2022 and August 3, 2022 (Exhibits "A" and "B").
3. Application and Property Facts: See Staff Reports for the hearing dates of July 20, 2022 and August 3, 2022, Exhibits "A" and "B".
4. Required Findings per Middleton City Code 1-14-2(E)(7), Idaho State Statue Title 67, Chapter 65, Idaho Standards for Public Works Construction and Middleton Supplement thereto, Middleton City Code 1-14, 1-15, 5-1, 5-2, 5-3 & 5-4 and Idaho Code Title 67, Chapter 65 & Title 50, Chapters 2 and 13: See Staff Report for the hearing date of April 11, 2022, Exhibit "A".

B. Conclusions of Law:

1. That the City of Middleton has exercised the powers conferred upon it by the "Local Land Use Planning Act of 1975," codified at Chapter 65, Title 67, Idaho Code (I.C. §67-6503).
2. That due consideration has been given to the comments received from the governmental subdivisions providing services in the City of Middleton planning jurisdiction, comments received from individuals of the public, and comments from City Planning Staff and City Engineer.
3. That notice of the application and public hearing was given according to law.
4. That City Council's public hearing was conducted according to law, and the City has kept a record of the application and related documents.
5. That codes and standards pertinent to the application are the Idaho Standards for Public Works Construction, the Middleton Supplement to the Idaho Standards for Public Works Construction, and Middleton City Code 1-14, 1-15, 5-1, 5-2, 5-3, and 5-4 and Idaho Code Title 67, Chapter 65 and Idaho Code Title 50, chapters 2 and 13.

6. That public facilities and services required by the proposed development will not impose expense upon the public if the attached conditions of approval are imposed.

C. Decision & Order:


Pursuant to the City Council's authority as provided in Middleton City Code 1-5-2, and based upon the above Findings of Facts and Conclusions of Law, it is hereby ordered that:

1. The application of M3 and JUB Engineers for Annexation and Rezone to Mixed Use (M-U) is approved, subject to the conditions of approval set forth in the Staff Report for the July 20, 2022, public hearing (Exhibit "A").
2. The application of M3 and JUB Engineers for Development Agreement is approved subject to the conditions of approval set forth in the Staff Report for the July 20, 2022, public hearing.
3. The application of M3 and JUB Engineers for preliminary plat is approved subject to the conditions of approval set forth in the Staff Report for the July 20, 2022, public hearing.
4. The application of M3 and JUB Engineers for Comprehensive Plan Map Amendment is approved subject to the conditions of approval set forth in the Staff Report for the July 20, 2022, public hearing.

WRITTEN ORDER APPROVED ON: 8/3, 2022.


Steven J. Rule, Mayor
City of Middleton

Attest:


Roberta Stewart
Planning and Zoning Official

Please take notice that pursuant to MCC 1-14-2(E)(10), applicant shall have 14 days after a signed final decision to request reconsideration by the final-decision maker. Such request must identify specific deficiencies in the final decision. Failure to request reconsideration may invalidate a subsequent judicial appeal. Additionally, pursuant to Idaho State Statute 67-6521, any affected person aggrieved by a final decision may, within 28 days after all remedies have been exhausted under local ordinances, seek judicial review as provided in chapter 52, Title 67. Additionally, please take notice that Applicant has a right to request a regulatory taking analysis pursuant to Idaho State Statute section 67-8003.

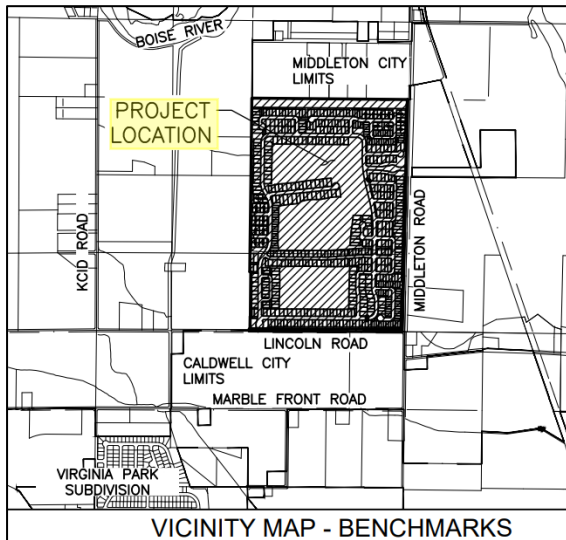


STAFF REVIEW AND REPORT

Middleton City Council

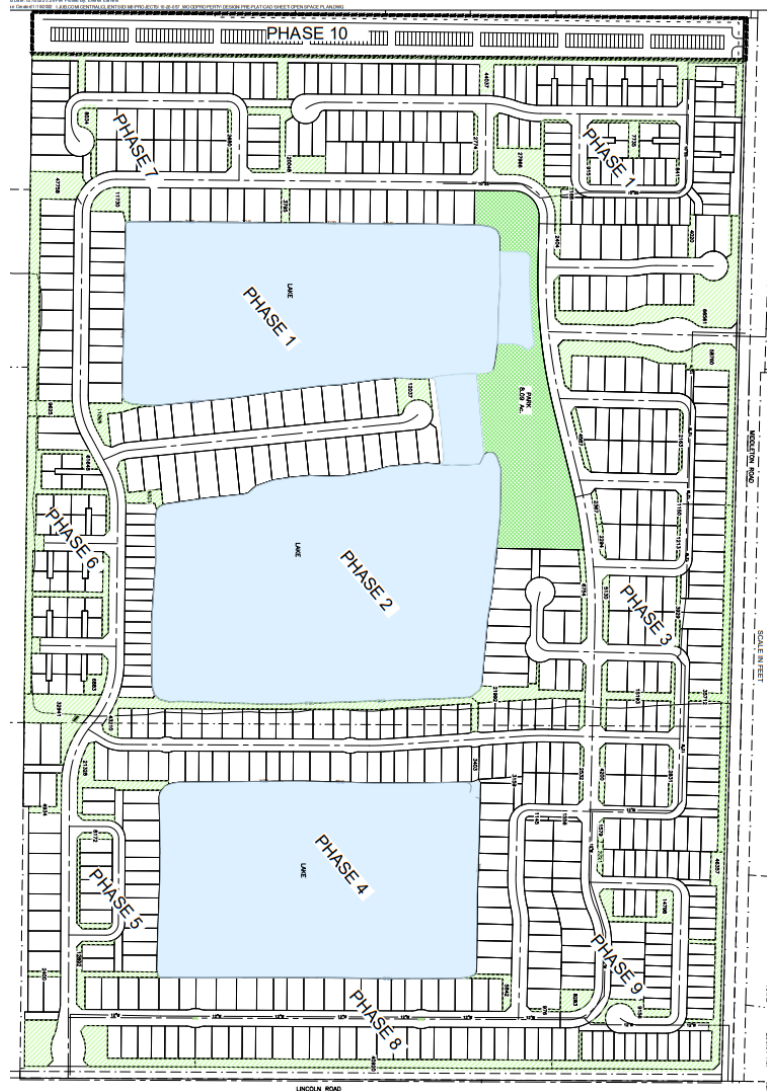
QUARRY EAST SUBDIVISION

Snapshot Summary

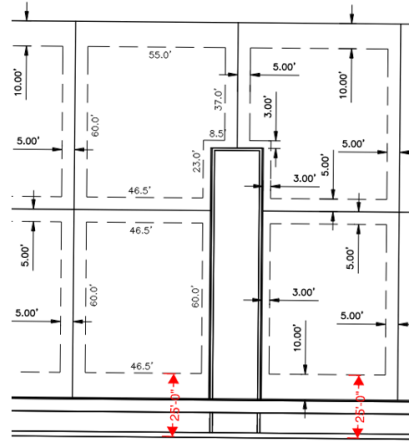


VICINITY MAP - BENCHMARKS

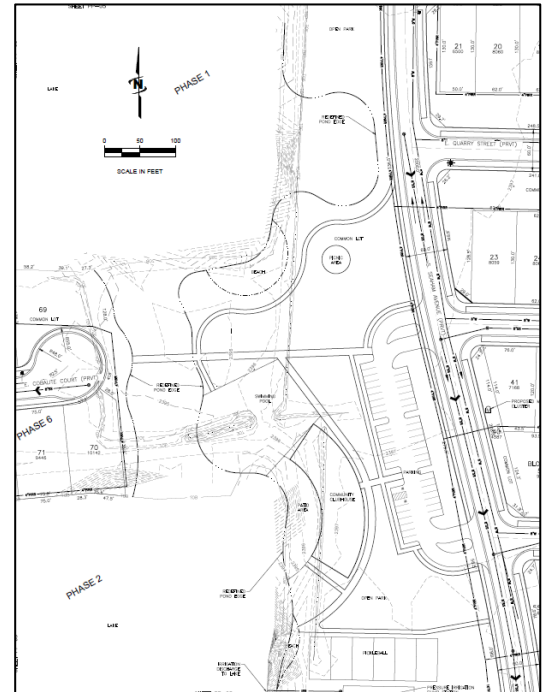
DESCRIPTION	DETAILS
Acreage	Approx. 237 acres
Current Zoning	County Agricultural, M-1 ("Light Industrial", and C-1 ("Neighborhood Commercial")
Proposed Zoning	M-U ("Mixed Use")
Current Land Use	County - "Industrial"
Proposed Land Use	City - Residential & Mixed Use
Lots	640 Residential Lots (including 102 duplex lots, 72 cluster lots, and 466 single family lots), 1 mini-storage M-U lot, 67 Common Area lots, 37 shared driveway lots, and 9 private road lots
Density	2.7
Open Space	over 25%
Amenities	clubhouse, swimming pool, 62 acres of lakes and beaches, 6 pickle ball courts, & picnic areas



- A. City Council Public Hearing Date:** July 20, 2022
- B. Project Description:** A 55+ resort style community involving 640 residential lots, one mini-self storage lot, 67 common area lots, 37 shared driveway lots, and nine private road lots located at 21500 Middleton Road and 11436 Lincoln Road (Tax Parcels Nos. R34076 and R34077). Of the 640 residential lots, 466 are traditional single family lots, 102 are duplex lots, and 72 are cluster-pack lots, which are four 4000 s.f. lots clustered around a single shared driveway. See rendering of "cluster-pack" below.

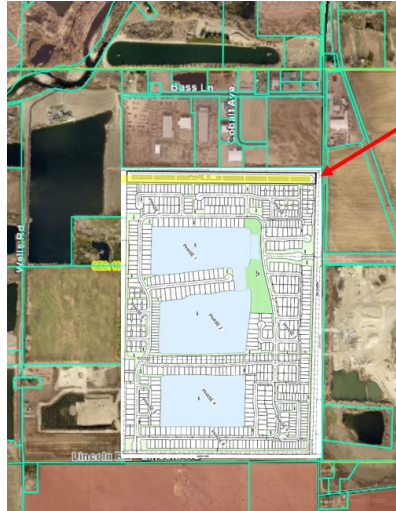


The proposed amenities include three large lakes encompassing over 62 acres, beaches, clubhouse, swimming pool, picnic areas, and six pickleball courts.



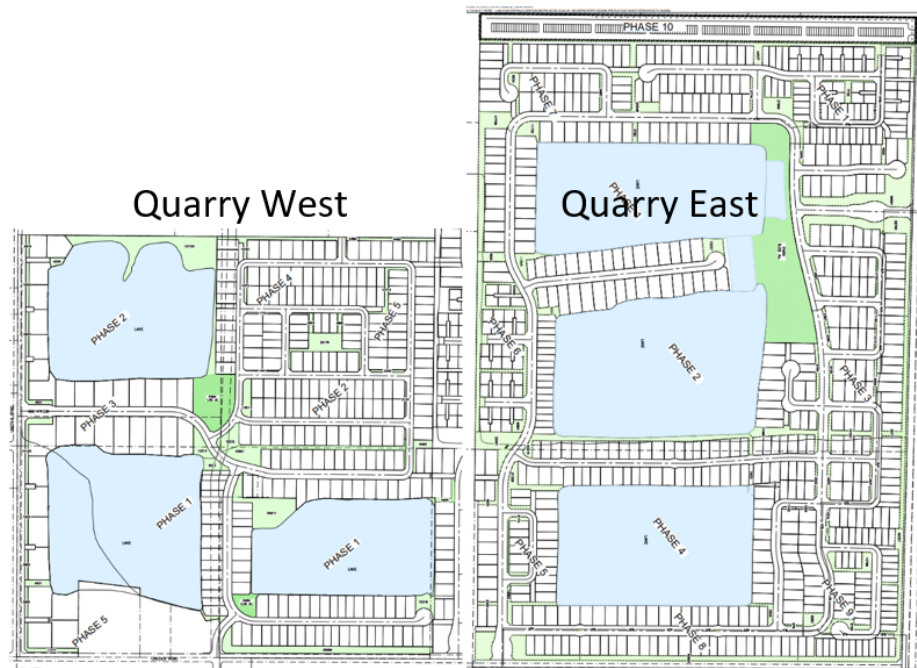
Developer's project plan proposes a private gated community that consists entirely of private streets. The homeowner's association, not the City, will be responsible for maintaining, repairing and replacing the streets throughout the years. However, service providers, including emergency services, will be given access to the private streets.

The application also includes a mini self-storage and RV storage facility at the northern end of the project. The storage facility will abut the Middleton Industrial Park directly to the north.



Quarry East subdivision is a “sister” subdivision to another subdivision proposal known as “Quarry West”. This second subdivision is proposed for a location immediately west of The Quarry East. The Quarry West also has preliminary plat/annexation applications pending before the City, and those applications will be brought to the Council in the next few weeks for consideration.

The Quarry West proposal includes 297 single family home sites on 150 acres. Developer is not proposing a private and gated 55+ community for Quarry West.

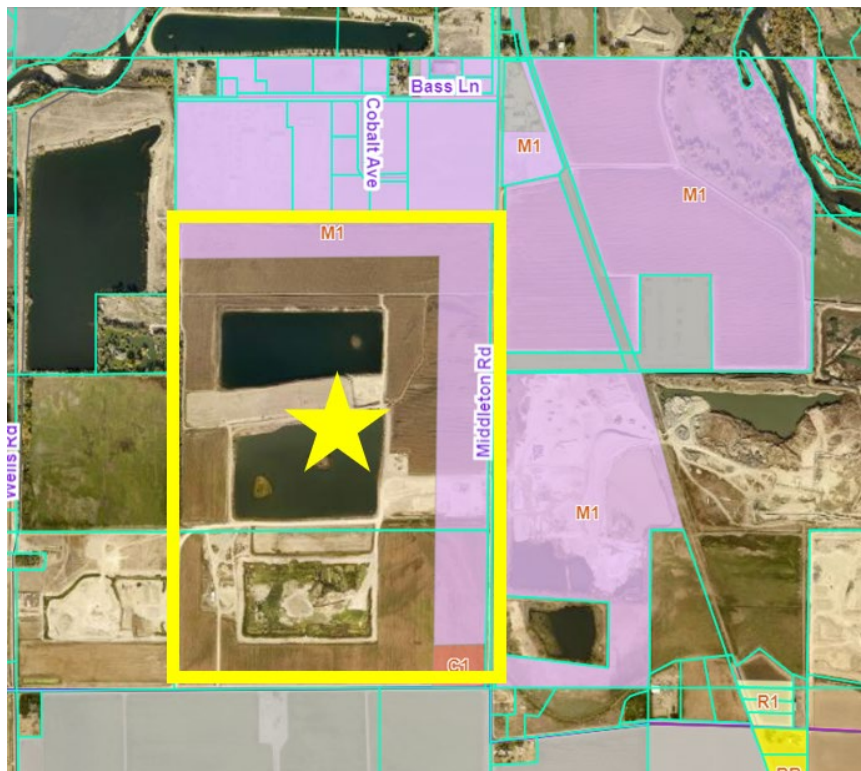


- C. Application Requests:** Applicant has four applications before City Council: (1) annexation and rezone to M-U (“Mixed Use”), (2) preliminary plat, (3) Development Agreement, and (4) Comprehensive Plan Map amendment to change the “Residential” use on the Future Land Use Map to “Industrial”.

Applicant applied to Planning & Zoning for a special use permit to construct duplex homes and a mini-self storage facility in the proposed M-U Zone. The Commission approved the requests subject to the conditions of approval set forth in the Staff Report for the April 11, 2022, public hearing.

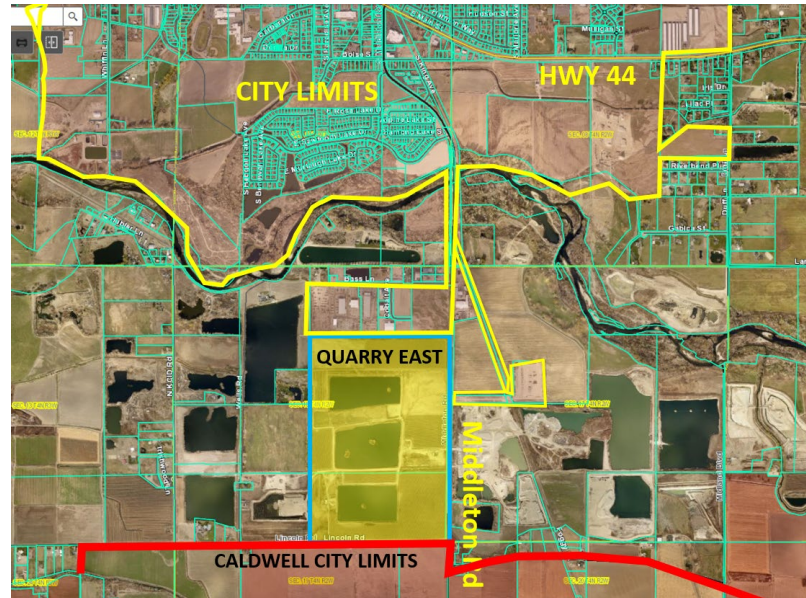
The final application is an administrative floodplain application that covers a handful of lots along the eastern boundary of the project parcel. Any approval of the other applications should be conditioned upon completion of all comments from the City’s floodplain administrator.

- D. Current Zoning & Property Condition:** The project property is located in Canyon County, and it is zoned Agricultural, C-1 “Neighborhood Commercial,” and M-1 “Light Industrial.”

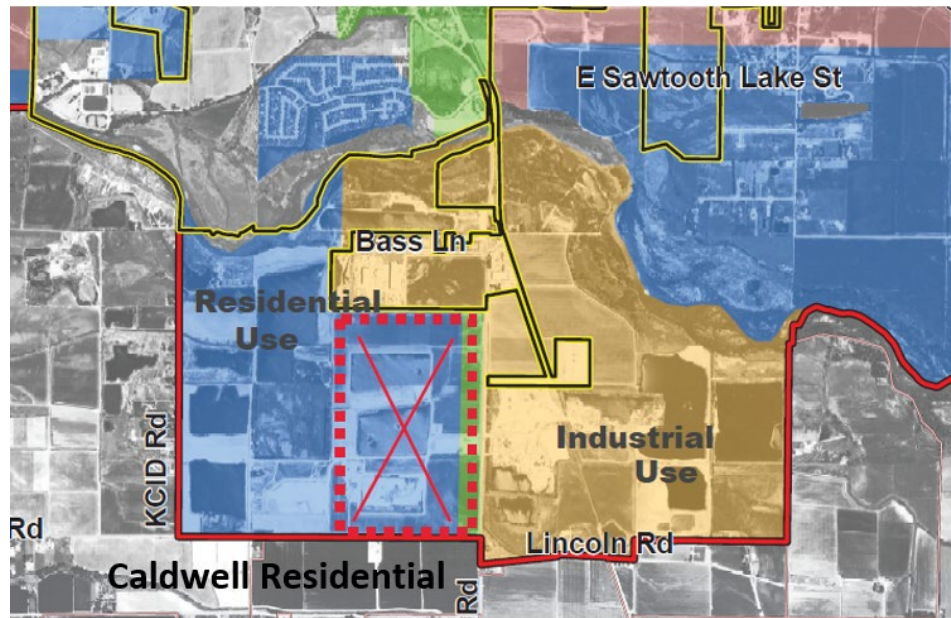


The site is currently vacant land, but it was the location of a quarry operation at one time, which accounts for the three large “lakes” already on site.

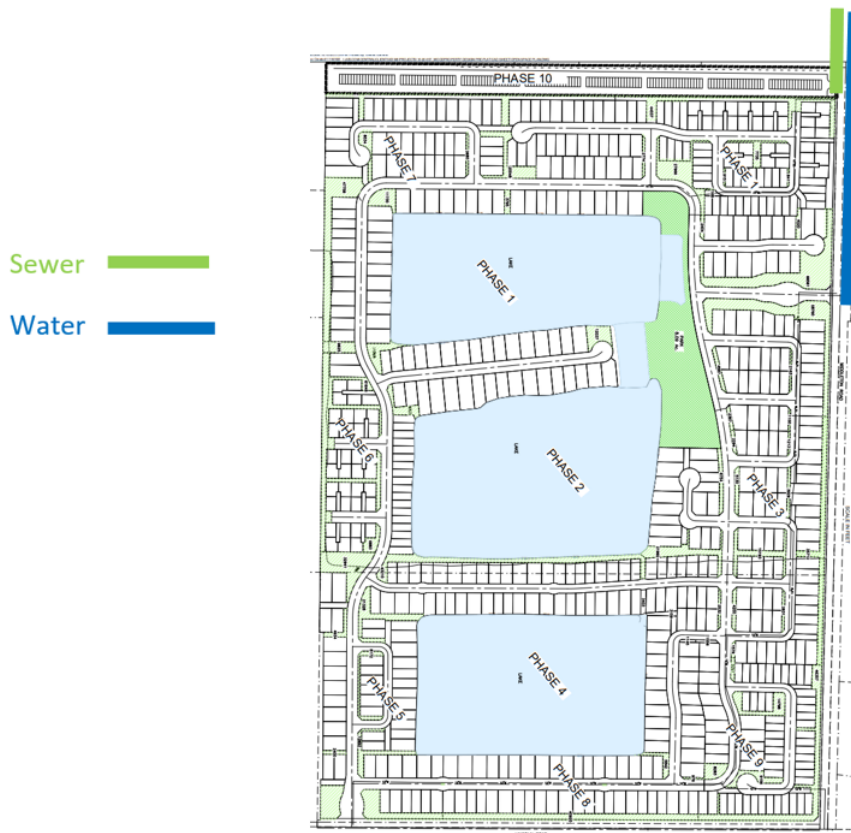
The area surrounding the project site is primarily industrial property, but there are also agricultural parcels and vacant lots nearby. Caldwell City limits are on the south side of the Quarry subdivision, and it shares the subdivision's southern border.



The City's Comprehensive Plan Future Land Use Map shows that industrial use is planned for the property to the east and north. Residential use is planned for the property to the west. Caldwell planning documents indicate that residential uses are planned for the area to the south.



- E. City Services:** City water and sewer are located immediately east of the project in Middleton Road.



If approved, the proposed Development Agreement will require Developer to construct a sewer lift station on its project site and also expand an existing lift station known as the Boise River lift station. Developer will construct or expand these improvements, at its own cost, but City will be required to reimburse Developer for any improvements that are over and above Developer's proportionate share. This should be accomplished through a later-comers fee agreement (MCC 1-17-1) or some other mutually agreeable method allowed by law.

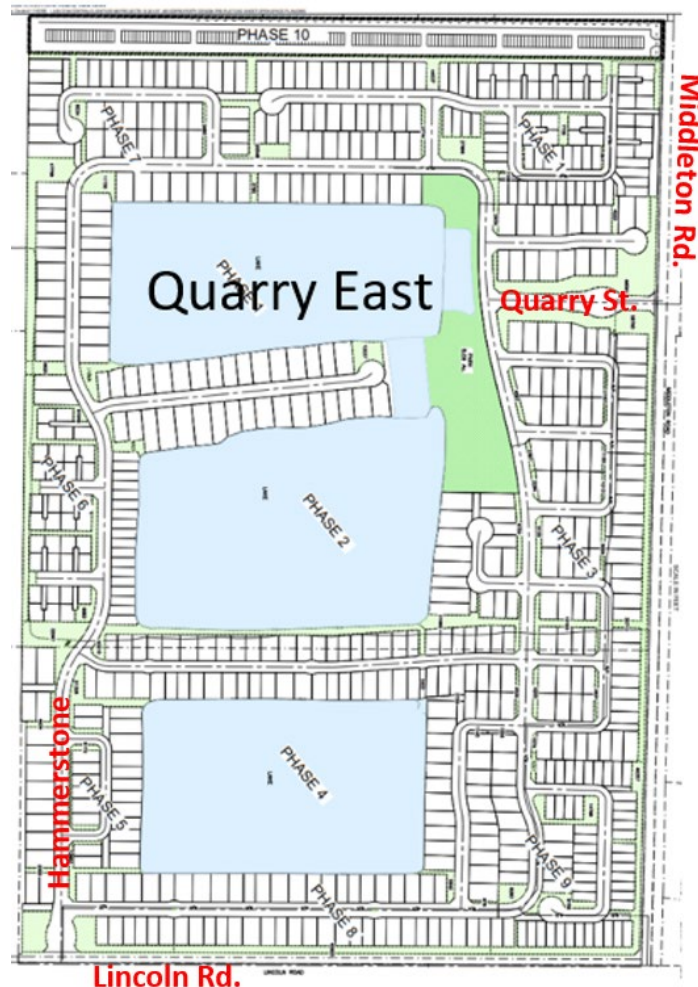
Another proposed term in the Development Agreement will require Developer to extend a second water line under, over, or through the Boise River in order to create a water loop system necessary to extend water to the project. As with sewer, Developer will be required to pay for this improvement to the extent the water loop line serves the project site, but any water capacity created over and above Developer's proportionate share will be reimbursed to Developer via a late comer fee agreement or other reimbursement agreement allowed by law.

F. Traffic, Access & Streets:

1. **Private Streets:** The City code does not contemplate private streets. Streets built inside City limits are owned, repaired and maintained by the City. Developer is requesting that all the streets inside Quarry East be "private" streets that are

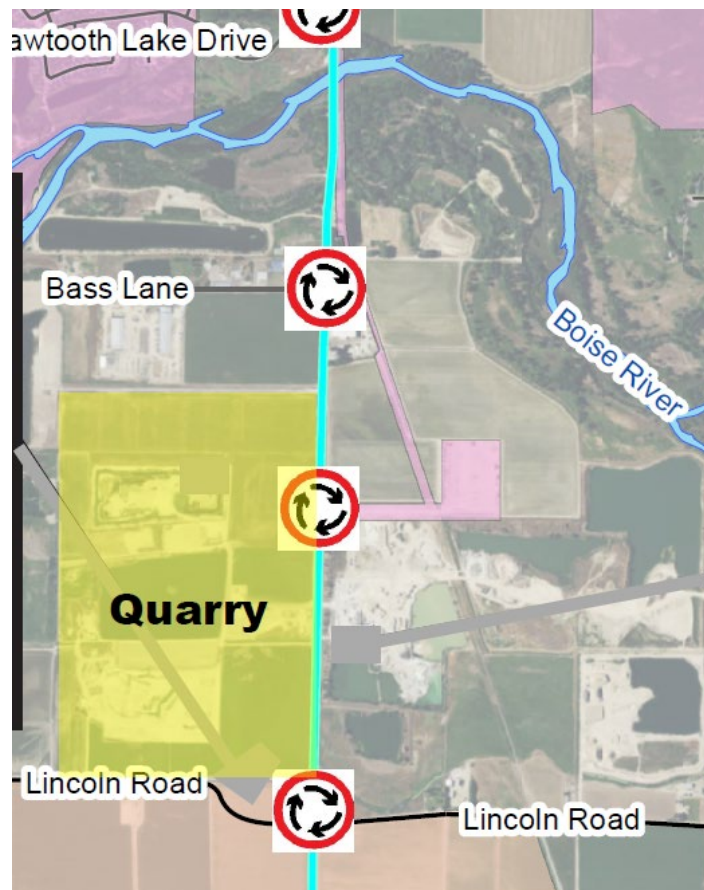
owned, repaired, maintained, and replaced by the Homeowner's Association. This form of ownership will require a waiver from the Code standards, which the Governing Bodies can approve pursuant to MCC 1-15-2. (More discussion on this below.)

2. **Frontage Construction:** S. Middleton Road and Lincoln Road front the project site. Developer will be required to construct the ½ road frontage improvements along Middleton Road and Lincoln Road. The improvement to Lincoln Road will be at Developer's sole cost and expense. However, because the portion of Middleton Road fronting the subdivision is on the Mid-Star Transportation CIP, The Mid-Star fund will be required to reimburse the Developer 100% for Middleton Road Improvements that are also included in the Mid-Star CIP. This is required to prevent the City from imposing duplicative development requirements on Developer. (Idaho Code 67-8209).
3. **Access & Intersection Controls:** There are two planned accesses to the project. The main access will be at Middleton Road and Quarry St., and the second will be located at Lincoln Road and Hammerstone Avenue.



During the initial stages of the project, access will be through the Middleton Road/Quarry St. access. Left and right turn lanes will be sufficient to handle the traffic during that time period. However, as phases are developed and residents move in, an intersection control will be required at the Middleton Rd/Quarry St., intersection to handle the increased traffic.

Pursuant to the 2016 Middleton Road Corridor Study, there is a multi-lane roundabout planned at the approximate location of the Quarry St., entrance on Middleton Road. A second multi-lane roundabout is planned about ½ mile to the south at the intersection of Middleton Road and the southern “leg” of Lincoln Road. Working together, the two roundabouts should assist in moving the traffic quickly and efficiently along Middleton Road. Both roundabouts are projects included in the Mid-Star CIP.



The most efficient and cost-effective way to get the roundabout constructed at Middleton Rd., and Quarry St., is by having Developer construct the roundabout rather than waiting for the City of Middleton and/or CHD4 to build the roundabout through the Mid-Star program at a future date and at future prices. Per the proposed Development Agreement, if the City can obtain the right of way necessary to construct the large roundabout at the Quarry entrance prior to issuing Developer 100 building permits, then Developer shall be required to design and construct the roundabout at that

location. However, since the roundabout is on the Mid-Star CIP, the Mid-Star fund will be required to reimburse Developer for those costs after construction. (Id. Code 67-8209)

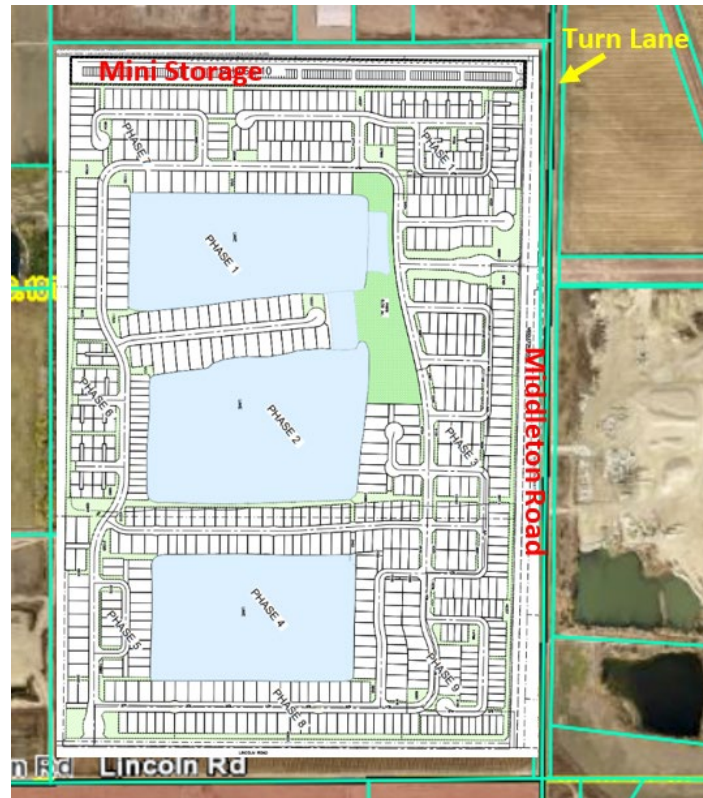
The City may not be able to obtain the right of way for the roundabout prior to 100 building permits; however, some type of intersection control will still be required to ensure safe access off of Middleton Road on to Quarry Street. Therefore, the proposed DA provides that if the City obtains the narrower right of way for a traffic signal, then Developer shall be required to design and construct the traffic signal at its own cost and expense. There will be no reimbursement for the traffic light. The traffic signal will then stay in place until the roundabout is constructed by the City or CHD4 sometime in the future through the Mid-Star program.

As to the second roundabout located at Middleton Road and the southern leg of Lincoln Road, that roundabout will be built sometime in the future through the Mid-Star program and/or through the collaboration of CHD4, Caldwell, Canyon County and/or Middleton. In the interim, a traffic control at the current intersection of Middleton Road and the northern leg of Lincoln Road will be needed to help with road safety. See below.



The proposed Development Agreement provides that when the Developer constructs the access on to Lincoln Road, the Developer shall be required to design and construct a traffic signal at Middleton Road and Lincoln Road. It shall remain in place until the more comprehensive roundabout is constructed a few hundred feet to the south. Developer will not be reimbursed for constructing this traffic signal.

The final intersection to consider is the turn lane required for entrance into the proposed Mini storage facility at the north end of the Quarry project. Per the proposed Development Agreement, City and/or CHD4 will determine, before Phase 10, what type of access will be required, and it may be limited access such as “right in/right out” only. Developer shall construct the turn lanes per the City requirements as part of frontage improvements.



Applicant completed a Traffic Study. The Traffic Study does not recommend any mitigation measures that are not already contemplated by the City.

Middleton requires development to pay for itself so the taxpayers will not be burdened with the cost. Therefore, Developer will be required to pay a Transportation Impact fee per the Mid-Star CIP program. The fee is \$5050 per residential lot, which amounts to \$3,232,000 paid to improve the roads in and around the City of Middleton. The estimated Mid-Star transportation impact fee for the mini-storage is over \$108,000.00.

For pro-rata traffic fees pursuant to MCC 4-5-3, Developer shall pay an additional \$456,355.00, making Developer's total contribution to future road improvements \$3,688,355.00.

- G. Pathway, Sidewalks & Open Space:** Developer has provided over 26% open space in the form of three recreational lakes, clubhouse and parks/sport courts. This far exceeds the minimum code requirement of 5% open space.

No specific pathways are required by the Comprehensive Plan maps; however, internal sidewalks and the pathway along Middleton Road will connect residents to the large new River Walk District planned for the north side of the Boise River.

- H. Fire District:** The subject property is in the Caldwell Fire District. On March 8, 2022, Fire Marshal Alan Perry submitted comments indicating general approval of the project so long as Developer complies with the 2018 International Fire Code. (See Agency Comments attached as Exhibit “C”.)
- I. Schools:** The Quarry East is located in the Vallivue School District.
- J. Annexation and Rezone:** Applicant is requesting that the entire 237-acre project be annexed into the City of Middleton with a zone change to Mixed Use “M-U”. Mixed Use zoning is designed to accommodate a variety of uses such as commercial uses combined with a variety of housing types. It also contemplates a higher density of housing. The M-U zone allows a density of 12 buildings per acre, minimum lot size of 6000 s.f., and lot width (or frontage) of 50’. (MCC 5-4-1, Table 2). (Per the DA, Developer is actually requesting smaller lots and narrower setbacks than allowed in the M-U Zone. See discussion below in the section regarding the Development Agreement.)

There are three findings that must be made before Annexation can be approved: (1) the property must be contiguous to City limits (2) City services such as sewer and water can be extended to serve the site, and (3) the annexation is deemed to be an orderly development of the City allowing efficient and economical extension of City services. (Idaho Code 50-222.)

An application for rezone requires two findings before the Council can approve the application: (1) the rezone will not adversely affect the City’s delivery of services and (2) the rezone request is not in conflict with the Comprehensive Plan. (Idaho Code 67-6511)

FINDINGS:

Planning Staff finds that Applicant’s project meets all three findings for annexation: (1) the property is contiguous and (2) sewer and water are available and can be extended to the site. Additionally, current capacity analyses contemplate sufficient capacity for this project, although specific capacity will not be established or assigned until the construction drawings for each phase are approved.

As to finding #3, the annexation is an orderly and economical extension of City services for the following reasons: (1) the Developer will be constructing needed road improvements and intersection controls on Middleton Road sooner than would occur through the Mid-Star road improvement program, (2) Developer will be designing and constructing extensive water and sewer facilities that will connect property on the

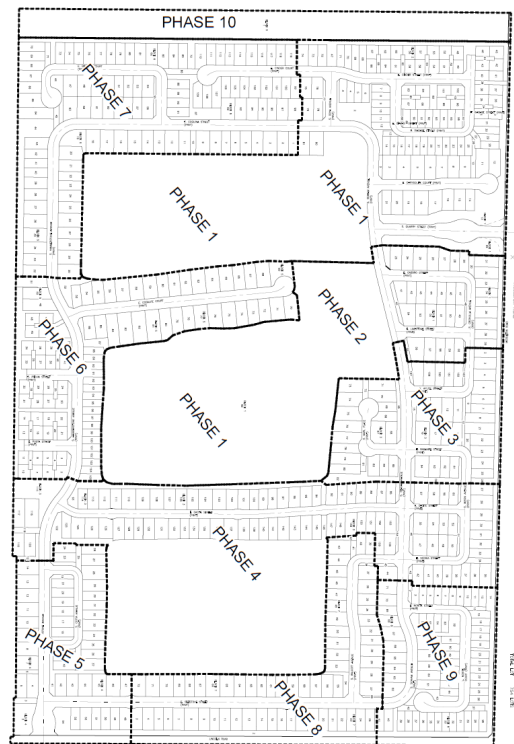
south side of the Boise River to Middleton property on the north side of the Boise River, (3) taxpayers will not be burdened with the cost of development because Developer will pay impact fees for roads, parks, and police protection, (4) the project supplies housing for 55+, which fills a need for housing in the City, (5) the 55+ community will place very minimal pressure on the school district and emergency services such as police protection, and (6) the private streets will relieve the City from repair and maintenance costs, which are costs normally placed upon the shoulders of the taxpayers.

On the flip side, potential adverse impacts to be considered are (1) increased traffic congestion, (2) over-crowding, (3) pressure imposed upon City services like sewer, water, fire protection and police and (4) loss of rural property.

As to the rezone application, Planning Staff finds that the rezone will not adversely affect the City's ability to deliver sewer and water as already noted above. Additionally, Developer will bear the cost of impacting city services by paying impact fees for transportation, parks, and police, which were designed to proportionately cover the impacts imposed by developers.

Finally, as will be shown below in the section regarding the Comprehensive Plan, the rezone is not in conflict with the Comprehensive Plan

- K. Preliminary Plat Application:** The preliminary plat shows 10 phases for development. (A full copy of the proposed preliminary plat is attached to this staff report as Exhibit "A".)



Before the Council can approve the preliminary plat, it must find that the preliminary plat complies with all Middleton City codes and standards. However, Middleton City Code section 1-15-2 allows the governing bodies to waive specific codes and standards and still approve a preliminary plat. Specifically, MCC 1-15-2 provides as follows:

1-15-2: EXCEPTIONS OR WAIVERS OF STANDARDS:



A. Exceptions or waivers of standards, other than land uses according to [Title 5, Chapter 4](#), Table 1 of this code, may be approved through one of the following public hearing processes:

1. Special use permit,
2. Development agreement accompanying a rezone application,
3. Variance,
4. Condition of approval as part of a land use application, or
5. Approval of a preliminary plat, with or without conditions.

(Ord. 609, 7-3-2018; amd. Ord. 619, 7-17-2019)

The preliminary plat application does not comply with all Middleton City codes and standards. However, Applicant is requesting “waivers” or “exceptions” to the code under MCC 1-15-2.

Directly below is an itemization of all waiver requests. (More detailed descriptions of each waiver request can be found at Exhibits “F” and “G” to the proposed Development Agreement, which is attached to this staff report as Exhibit “B”.) Planning Staff, City Engineer and Fire Department have reviewed these waiver requests and have worked with Applicant to fine tune any issues or concerns that could arise from these waivers. For instance, City Engineer and Fire Department have reviewed all waivers to City Code standards for roads to ensure that none of the changes requested are unreasonable or below acceptable engineering standards.

As to waiver requests regarding legal issues such as bonding or platting procedures, City attorney has reviewed the requests and worked with Staff and the developer to refine the requests to an acceptable level.

However, there is one waiver request City Staff did not review, negotiate or otherwise try to change. That is item #1 below regarding setbacks and lot dimensions. None of these proposed waivers pose a danger or particular technical concern, so Staff did not address the issue to any great degree. Instead, Staff is simply presenting Developer’s requests for narrower setbacks and smaller lots to the City Council for consideration.

Waiver Requests:

- 1. Setbacks & Dimensions (MCC 5-4-1, Table 2):** Minimum standards for lot dimensions and setbacks are set forth in MCC 5-4-1, Table 2. Developer is requesting that the annexed project be zoned Mixed Use (M-U). The dimensions and setbacks for M-U are as follows:

M-U Setbacks & Dimensions

Front setback- 20'
Rear setback – 20'
Interior Side setback– 12'

Street-side setback – 20'
Minimum lot size – 6000 sf
Minimum lot width – 50'

TABLE 2
HEIGHT, SETBACK AND COVERAGE SCHEDULE

District	Maximum Density (Units/Gross Acre)	Maximum Height ⁸	Minimum Front Setback ^{3,4,5,6, 7}	Minimum Rear Setback ^{4,6, 7}	Minimum Interior Side Setback ^{4,6, 7}	Minimum Side Street Setback ^{3,4, 6,7}	Minimum Interior Lot Area (Sq. Ft.)	Minimum Lot Width ¹	Maximum Lot Coverage ²
A-R	0.5	40'	30'	30'	30'	30'	87,120 (2 acres)	150'	50%
C-3		55'	10'	0'	0'	10'	0		90%
M-1		75'	10'	0'	0'	10'	0		90%
M-2		75'	10'	0'	0'	10'	0		90%
R-1 ⁹	1.0	40'	30'	30'	30'	30'	43,560	100'	50%
R-2 ⁹	2.0	35'	25'	25'	25'	25'	21,780	90'	50%
R-3 ⁹	3.0	30'	25'	20'	10'	20'	8,000	75'	60%
RRR		55'	0'	0'	0'	0'	0	0'	90%
TOD		55'	0'	0'	0'	0'	0	0'	90%
M-F ^{10, 11}	12.0	35'	20'	20'	12'	20'	10,000	75'	75%
M-U ^{8, 10}	12.0	55'	20'	20'	12'	20'	6,000	50'	75%

[Setbacks for R-3 may also be seen in the chart directly above.]

In the current application, Applicant is requesting the following narrower setbacks and lot sizes:

Duplex Lot	
Front (to residence or side-load garage)	10 feet
Front (to garage)	20 feet
Rear	15 feet
Side	5 feet (0 feet for attached)
Minimum Lot Size	4,000 square feet
Minimum Width (N/A for irregular shaped lots)	40 feet

Cluster 4 Pack Lots	
Front (to street or side-load garage)	10 feet
Front (to garage)	20 feet or 5 feet
Rear (back of back lot)	10 feet
Side (all interior property lines)	5 feet (3 feet to shared drive)
Minimum Lot Size	4,000 square feet
Minimum Width (N/A for irregular shaped lots)	45 feet

Single Family Standard Detached Lot	
Front (to residence or side-load garage)	10 feet
Front (to garage)	20 feet
Rear	15 feet
Side	5 feet
Minimum Lot Size	5,000 square feet
Minimum Width (N/A for irregular shaped lots)	40 feet

2. **Private Streets:** City Code does not allow Private Roads other than small private lanes (or shared driveways) that can accommodate up to four homes. Allowing private streets will be an exception to the Code.
3. **Driveways and Frontage (MCC 4-1-1.H & Supplement to ISPWC):** Applicant is requesting an exception to the requirement that all lots front a “public road”. Because the roads within the subdivision are requested to be “private,” this code section will also need to be waived. Additionally, Developer is requesting a waiver to allow narrower driveways.
4. **Roadway width & gutters (MCC 5-4-10-2.D.2 & Supplement to ISPWC):** City Code requires local public right-of-way/roads to be 50’ wide. Developer is requesting a narrower width of 47’, and a narrower width from back of curb to back of curb of 33’, so long as the narrower width meets minimum requirements of emergency services. Additionally, Developer is requesting that valley curbs be narrower than city required 4’ width so long as the capacity remains adequate.
5. **Cul de sac length (MCC 5-4-10-2.E):** Per the code, cul de sac roads cannot exceed 600’. Because of the large lakes and narrow channels created by the lakes, Developer is requesting that the cul de sac maximum length be extended to 1000’.
6. **Radius turns (MCC 5-4-10-2.H.2):** Code allows radius turns of 90’. Developer is requesting radius turns as low as 45’. Caldwell Fire’s final approval will require Applicant to provide an auto-turn analysis to prove that all radius turns can accommodate the Fire Department’s emergency vehicles.
7. **Block lengths (MCC 5-4-10-3.A.1):** This code prevents any long linear block lengths extending past 1100’. Because the large lakes create peninsula-like strips of land, Developer is requesting that the block length maximum be expanded to 1700’.
8. **Dead-end Roads (MCC 5-4-10-2.F):** Code prohibits dead end roads, but due to the extensive use of private lanes, which are allowed, Developer has created a few short dead-end roads.
9. **Tree Planting (Supplement to ISPWC):** Developer is requesting that it be allowed to plant trees within 3’ from a sidewalk with a root barrier and 4’ from sidewalk without a root barrier.
11. **Street lights & Street Signs (Supplement to ISPWC):** Developer is requesting that alternative styles for street lights and street signs be allowed on the private streets for aesthetic purposes.

12. **Timing for final plat approval & DA Termination (MCC 5-4-4.F):** Under the current code, Developers may take up to four years to obtain final plat approval for their first phase in order to avoid the nullification of their preliminary plat. Quarry Developer is requesting six years to reach final plat approval in light of the extensive infrastructure construction required before Phase 1 final plat. Code also allows a Developer to take up to four years for each successive final plat after Developer utilizes two extension requests, one of which must be granted by City Council. Applicant is requesting the same four year period for final plat, but Applicant is requesting that the two extension requests be submitted only to Administrative staff.

13. **Amendments to Pre-Plat (MCC 5-4-4):** If a Developer desires to change an approved preliminary plat in any way, Developer must go through the public hearing procedures for preliminary plat. Developer is requesting a waiver to this requirement so Develop may submit only an administrative application to the Planning & Zoning Official to may make minor amendments to the preliminary plat. The allowed minor amendments would be (1) reduction or increase in the size of phases, (2) decreases in number of lots so long as the decrease does not exceed a 10% decrease in overall lots, and (3) minor changes to roads and lot lines to accommodate the change in lots or phases so long as the final plat still substantially conforms to the approved preliminary plat.

The request is based upon the fact that this large subdivision may take 10 or more years to develop, and a minimal amount of flexibility will help Developer and City to efficiently and economically negotiate those minor changes. If the Planning & Zoning Official deems the proposed revisions more than “minor”, the Planning & Zoning Official shall require Developer to use the public hearing process to amend the preliminary plat.

14. **Model Home Building Permits:** Building Permits are not issued until after final plat is approved. However, Developer is seeking authorization to obtain building permits for up to 10 model homes prior to final plat approval. This will allow Developer to begin constructing its model row in tandem with infrastructure construction, which will allow it to begin marketing with model homes as soon as final plat is approved.

15. **Bonding:** (MCC 5-4-7.A.3) Currently, the code allows bonding for only landscaping and irrigation improvements and only during inclement weather. Fencing has generally been considered part of the landscaping. Developer is requesting that it be allowed to bond for these items even if inclement weather is not present. This will address issues raised by the supply chain breakdown.

16. **ITD Proportionate Share Fee \$456,335.00:** ITD reviewed both the Quarry East and Quarry West preliminary plats and determined that the Developer should pay \$456,335.00 towards the improvements at Hwy 20/26 and Middleton Road due to the adverse impacts both plats would have on that intersection. Per MCC 4-5-3,

these fees must be paid up front prior to Phase 1 final plat approval. Because of the large size of the fee, Developer is requesting that the fee be pro-rated across lots in both Quarry East and Quarry West and paid per phase at final plat approval.

Findings:

Planning Staff finds that the preliminary plat does not comply with standards and requirements of the Middleton City Code, but the Council may still approve the preliminary plat if requested waivers from the code are approved through the preliminary plat process (MCC 1-15-2).

- L. Development Agreement:** Developer has submitted an application for Development Agreement to accompany its application for Annexation/Rezone. (See full copy of the Development Agreement attached to this staff report as Exhibit “B”).

Applicant has used the City’s form for the DA, and has added the following provisions to Article III:

1. **Various Waivers:** All the variances or waivers from City Code noted in the preliminary plat section above are individually set forth in the DA as permissible waivers from code.
2. **Concept Plan:** A concept plan shall be attached to the DA. The future development must be built substantially consistent with the concept plan.
3. **Concept Renderings:** Concept renderings of home styles and community amenities shall be attached as exhibits along with Design Guidelines that set forth minimum standards for design and quality. The standards show a higher quality building product and community infrastructure design. Developer, or any other entity that buys the project from Developer, must meet the minimum standards set forth in the concept renderings and design guidelines. This will ensure the high-quality nature of the community remains intact regardless of who owns the property in the future.
4. **Amenities:** DA requires that Developer build the following amenities: (1) community swimming pool, six pickleball courts, picnic area and community beaches prior to the issuance of the 134th building permits and (2) the clubhouse amenity shall be built prior to the 272nd building permit.
5. **55+ Community:** The community shall remain a 55+ community as allowed by Federal Code and State Statute. The requirement shall be set forth in the recorded CC&Rs.
6. **Allowed Uses:** although the M-U zone allows a large number of different commercial and residential uses, the DA will limit Applicant to only these uses: (1) single family residential, (2) duplex residential, (3) model complex and sales

office/marketing/construction trailers, (4) community recreational amenities, and (5) mini-warehouse storage, including RV storage.

7. **Mini-Storage:** the storage facility must undergo the Design Review process before a building permit will be issued. The mini-storage buildings may only be single story buildings.
8. **Private Streets Reserve Study:** Because the private streets and stormwater facilities will be maintained and repaired by the HOA, City will require Developer to submit a reserve study at each phase final plat to show that sufficient money will be collected by the HOA to maintain and repair the streets and stormwater facilities to industry standards. After build-out, the City can request the HOA to continue to submit reserve studies, but no more than once every 3 years. If the Developer or HOA fails to maintain the private streets and stormwater facilities to industry standards, the City may bring an action at law or equity to force the HOA to properly maintain the streets. If City is the prevailing party in the lawsuit, HOA will owe City attorney's fees to ensure that taxpayers are not burdened with this cost of litigation.
9. **Emergency Access/Service:** Developer/HOA will ensure that the private streets are accessible to emergency services and other public services.
10. **Sewer & Water improvements:** As already discussed above, Developer shall extend city sewer and water to the site and possibly improve utility facilities over and above what is required to serve Quarry East. The DA provides that any improvements over and above Developer's proportionate share will be reimbursed to Developer via a later comer agreement or other similar reimbursement.

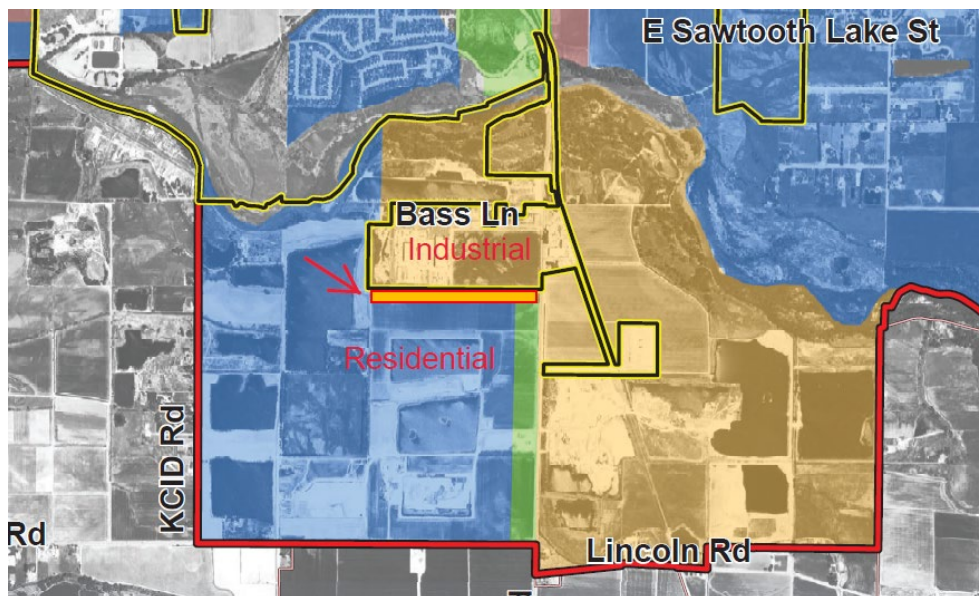
The DA further states that the sewer and water utilities are public and Developer/Owner shall grant an easement to City to access the utilities for repair and maintenance.
11. **Natural Gas:** DA provides that Developer will extend natural gas service to the project site.
12. **Urban Renewal Revenue Allocation Area:** Developer and City agree that Developer may seek reimbursement from the Urban Renewal District for public infrastructure. If reimbursement is granted by the Urban Renewal District, then Developer is not entitled to any credit or reimbursement under a late-comer fee and Developer will pay back any credit or reimbursements received to date.
13. **Road/Frontage Improvements:** DA requires Developer to construct all code required frontage improvements, including the potential roundabout/traffic signals at Lincoln & Middleton Road and at the Quarry entrance. The DA sets forth the timing and process for frontage improvements, traffic light construction,

and roundabout construction. See section above on streets for more detail on the matter.

14. **Notice of surrounding industrial use:** Developer shall place language in every purchase agreement and in the CC&Rs notifying prospective purchasers that the City's planning documents show industrial uses to the north and east of the project. This will prevent future residents from being surprised.

A copy of the proposed Development Agreement is attached as Exhibit "B".

- M. Comprehensive Plan & Land Use Map Amendment:** The vast majority of Applicant's project complies with the Comprehensive Plan's Future Land Use Map because 9/10th of the parcel is dedicated to residential use and the FLUM shows residential use at that location. However, Developer is proposing a mini-self storage and RV storage facility along the northern border of the project. To accommodate this, the Future Land Use Map must be changed to remove a small sliver of "residential use" and replace it with "industrial use."



A full copy of the proposed FLUM is attached to this Staff Report as Exhibit "F".

Pursuant to MCC 1-14-3, an application for Comprehensive Plan Map Amendment cannot be approved without a finding that the proposed amendment is "in harmony" with the Comprehensive Plan.

Finding:

Planning Staff finds that the proposed change from "Residential" to "Industrial" on the Future Land Use Map is in harmony with the Comprehensive Plan because the location of the changed designation directly abuts the industrial park located at Bass

Lane and Middleton Road, which is deemed an industrial use on the FLUM. In other words, the proposed change would result in a small expansion of an industrial use that already exists at that location.

The use also complies with Goal 4 of the Comprehensive Plan to use space for privacy considerations and to allow mixed use in appropriate locations. The storage facility will undergo design review and will create a more attractive buffer between the more intense industrial uses to the north of the subdivision. It also meets Goal 7 to promote economic growth and opportunity, while creating neighborhood storage and convenience for the Quarry subdivision.

Applicant's other applications require a finding that the project complies with the *Goals, Objectives, and Strategies* of the 2019 Middleton Comprehensive Plan.

Planning Staff finds that the proposed project complies with the following goals:

- a. *Goal 1:* the project is a new development that helps extend city services in an environmentally and fiscally-responsible manner.
- b. *Goals 3 and 23:* The project provides safe vehicle and pedestrian facilities in light of the street improvements and sidewalks shown on the preliminary plat.
- c. *Goal 4:* The project will establish a good quality of life with development that pays through impact fees and property taxes for the public services it receives when infrastructure is installed. Additionally, recreational activities and quality lots for residential use increase the quality of life and general welfare of the City.
- d. *Goal 5:* "...[e]ncourage[s] annexation of property whose property tax revenue and other benefits match or exceed the cost to provide public services." This project will generate a great deal of property tax income that will exceed the services received from the City because the City will not be maintaining and repairing the streets inside the community and the residents will generally not be utilizing the school system.
- e. *Goal 6:* the project promotes the goal to "expand water, sewer, and road systems in an orderly manner consistent with population growth in the City".
- f. *Goal 7:* The commercial/industrial storage facility promotes economic growth.
- g. *Goal 10:* Project provides open space and pathways that connect to a pedestrian system and provide outdoor recreational activities.
- h. *Goal 11:* Project provides a variety of housing types and lot sizes for residents.

N. Comments Received from Surrounding Landowners: None.

O. Comments from Agencies: CHD4 submitted five comment responses dated 11/29/2021, 6/1/2021, 2/4/2022, 3/24/2022, and 4/11/2022. Caldwell Fire Department submitted a response dated 2/14/2022 and comment letter received 3/8/2022. On March 29, 2022, Greater Middleton Parks & Recreation submitted a comment letter. ITD submitted comment letter/emails dated 5/25/2021 and 6/8/2021. Vallivue School

District submitted comment letters dated 5/18/2022 and 6/1/2022. (Agency comments are attached as Exhibit “C”).

P. Comments from City Engineer and Planning Staff: Copies of Engineering and planning staff comments are attached as Exhibit “D”.

Q. Applicant Information: Application was accepted March 1, 2021. Applicants are M3 Companies and JUB Engineers; 2760 W. Excursion Lane, Meridian, Id. 208.376.7330.

R. Notices & Neighborhood Meeting:	Dates:
Newspaper Notification	5/29/2022
Radius notification mailed to Adjacent landowners within 500'	5/31/2022
Circulation to Agencies	5/27/2022
Sign Posting property	5/27/2022
Neighborhood Meeting	6/30/2021 & 7/13/2020

(Applications were tabled at the 6/15/2022 City Council meeting due to moratorium.)

S. Applicable Codes and Standards:

Idaho Standards for Public Works Construction, the Middleton Supplement to the Idaho Standards for Public Works Construction, Middleton City Code 1-14, 1-16, 5-1, 5-2, 5-3, and 5-4, and Idaho Code Title 67, Chapter 65 & Title 50, Chapters 2 and 13.

T. Planning & Zoning Recommendations & Approvals: The Planning & Zoning Commission considered the Quarry East applications at the April 11, 2022, public hearing. The Commission approved applicant’s special use permit applications to construct duplex homes and a mini-storage/RV parking facility within the project.

The Commission further recommended that City Council approve the applications for (1) annexation/rezone, (2) development agreement, (3) preliminary plat, and (4) comprehensive plan map amendment subject to the conditions of approval set forth in the Staff Report for the hearing. (A copy of the Commission’s FCO and recommendation is attached as Exhibit “E”).

U. Conclusions and Recommended Conditions of Approval:

City Council’s decisions and order on the four applications before the Council must be based upon findings of facts and conclusions of law.

As to Findings of Facts, Planning staff has set forth findings of facts above in parentheses. If the Council agrees with these findings of facts and further agrees with the general facts presented at the public hearing, then the Council may create a record by approving a motion to accept the findings of facts presented.

As to Conclusions of Law, Planning Staff finds that City Council has the authority to hear these applications and to approve or deny the applications, with or without conditions. Additionally, Planning Staff notes that all public notice requirements were met. Planning Staff further set forth the portions of the Idaho State Code and Middleton City Code to be considered in approving or denying the applications. If the July 20th public hearing is conducted in compliance with Idaho State Statute and the Middleton City Code, then the Council may accept these conclusions of law and create a record by passing a motion to accept the conclusions of law presented.

Applications for Annexation/Rezone, Preliminary Plat, Development Agreement, & Comprehensive Plan Map Amendment: If the Council chooses to approve these four applications based upon the above *Findings of Facts* and *Conclusions of Law*, then Planning Staff recommends that any approval be subject to the following conditions:

1. City sewer and water facilities to be extended to the Project site.
2. Developer to provide City with a stamped copy of the approved preliminary plat.
3. Developer to comply with all provisions and terms set forth in the Development Agreement City Council approves during the public hearing on the matter.
4. Developer to install landscaping per updated landscape plan submitted June 30, 2022, including traffic buffer and berm as required by MCC 5-4-10-6 and set forth in the approved preliminary plat.
5. All City Engineering comments are to be completed and approved.
6. All Planner comments are to be completed and approved.
7. Developer to comply with all FEMA, Middleton floodplain code, and Floodplain Administrator requirements.
8. All Caldwell Fire Department comments are to be completed and approved.
9. The access to the mini-storage lot and facility is conditioned on City and CHD4 determining an appropriate access, which may include right in/right out only access.
10. Apply a street name and install a sign for the private lane on Lot 118, Block 3 for addressing purposes.
11. Sewer and Water capacity to be reserved at the time City approves the construction drawings for each phase.

Finally, if the Council denies any of the four applications, then pursuant to Middleton City Code 1-14(E)(8), the Council should state on the record what Applicant can do, if anything, to gain approval of the application(s).

EXHIBIT "A"

Proposed Preliminary Plat

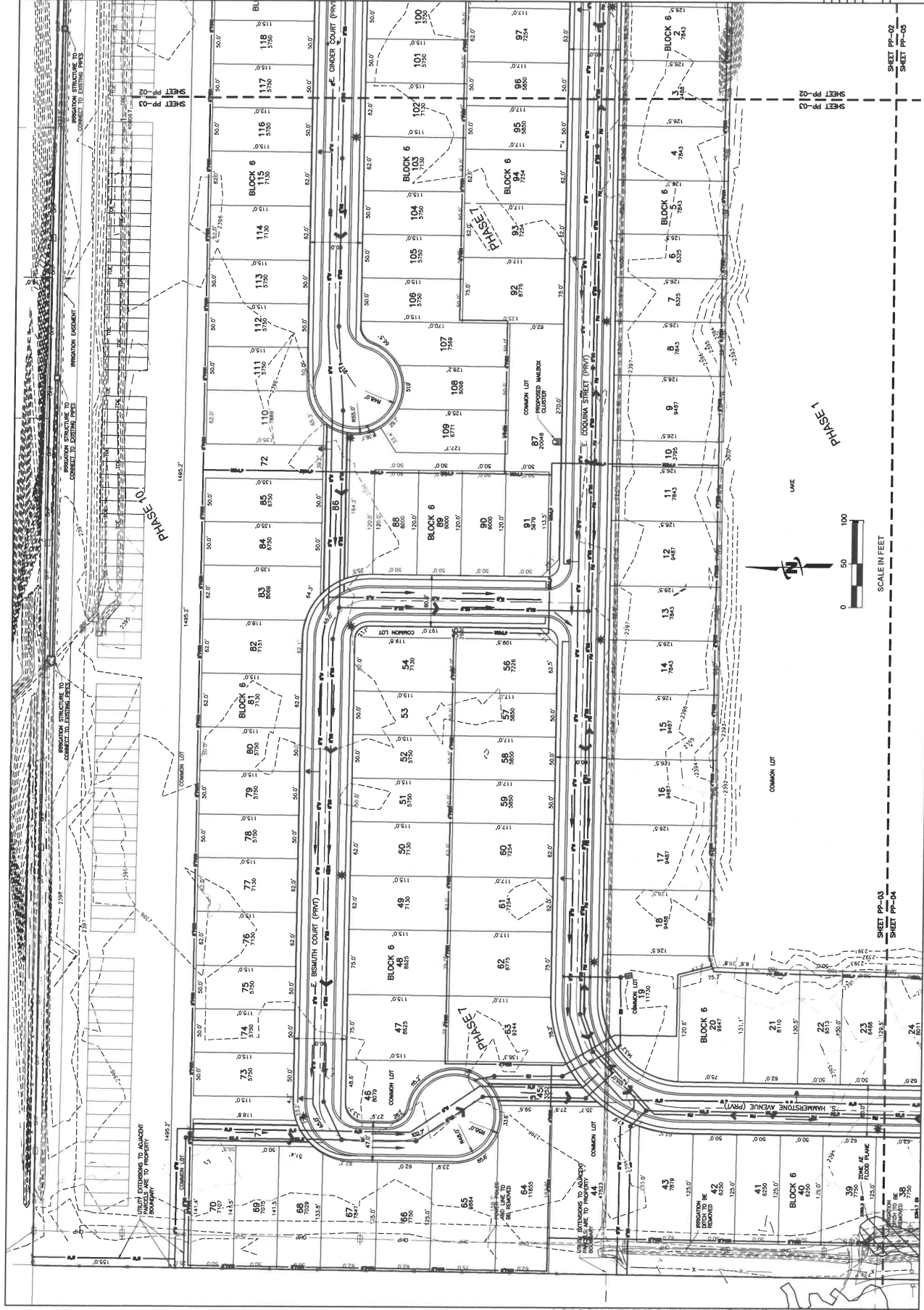




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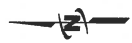
QUARRY EAST SUBDIVISION
MIDDLETON, IDAHO
EXISTING AND PROPOSED CONDITIONS

DATE: 10/26/2017
DRAWN BY: JUB
CHECKED BY: JUB
AT FULL SCALE, IF NOT ONE
LAST UPDATED: 2/28/2017
SHEET NUMBER: PP-03



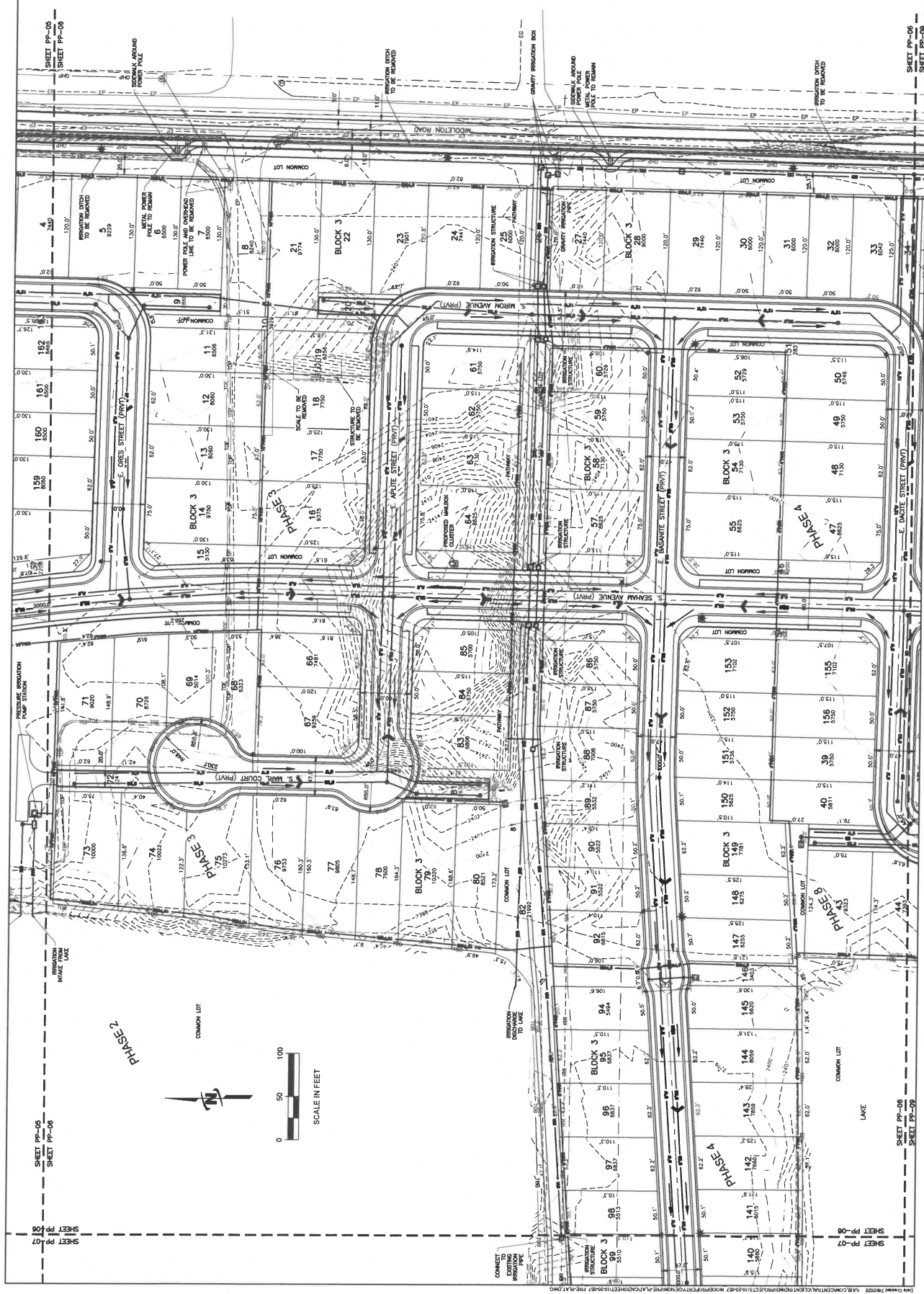
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SHEET PP-04





QUARRY EAST SUBDIVISION
MIDDLETON, IDHAO
PRELIMINARY PLAT
UTILITY PLAN

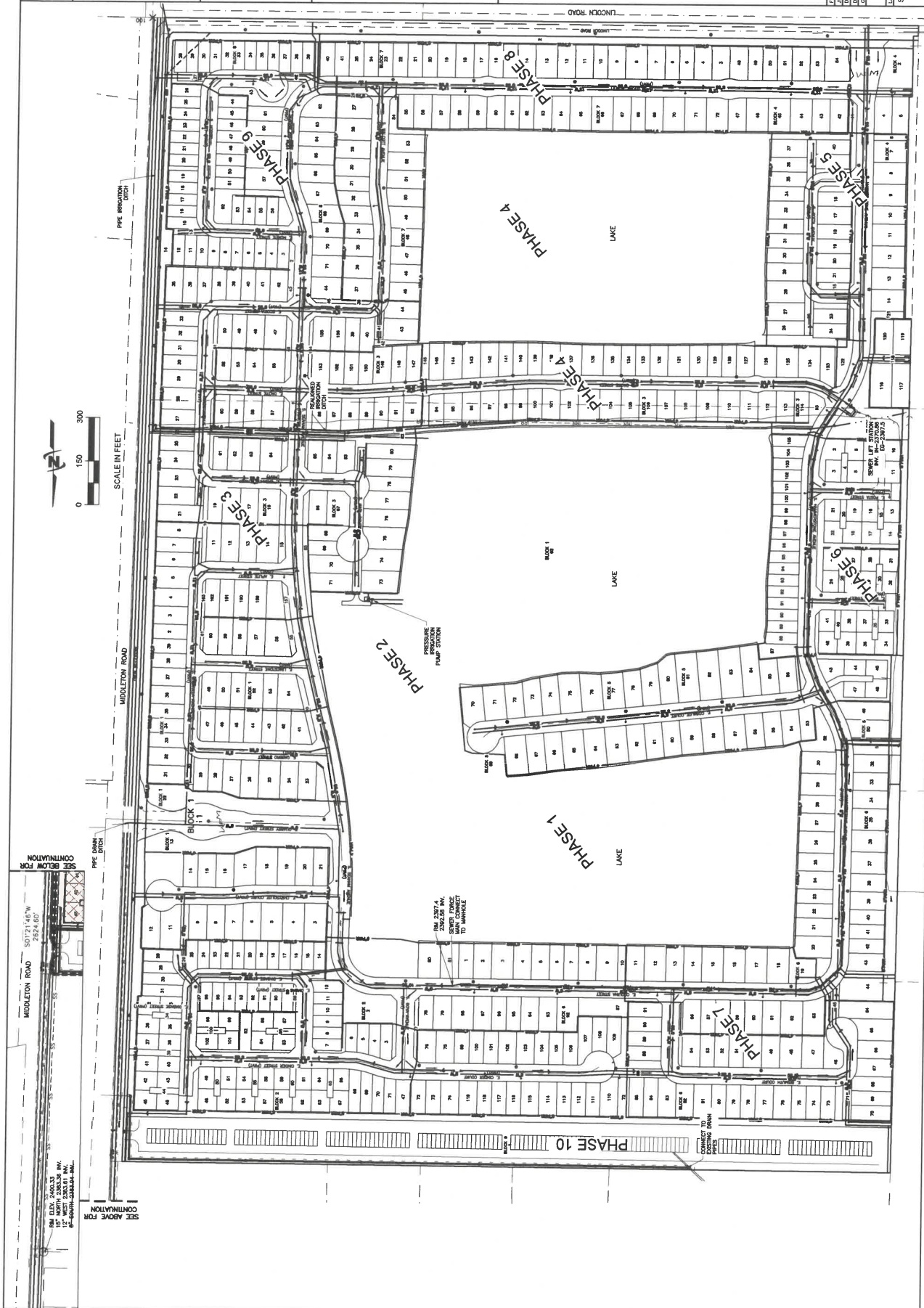
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2760 W. EXCURSION LN.
Suite 400
MERIDIAN, ID 83642
Phone: 208.376.7330
www.jub.com

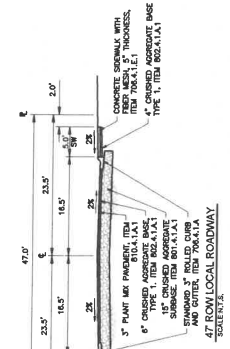
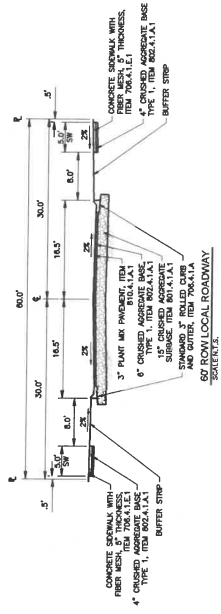
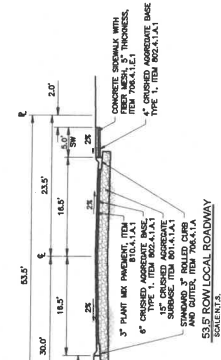
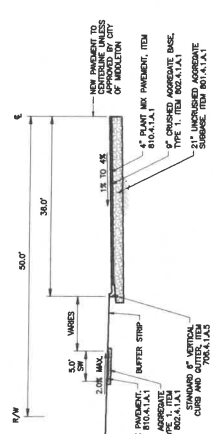
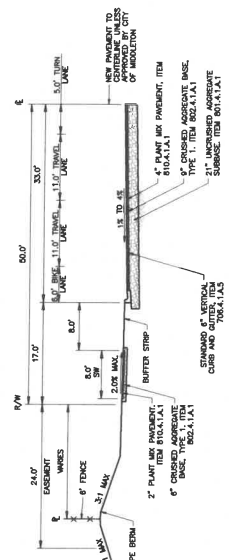
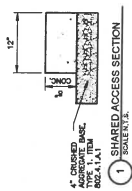
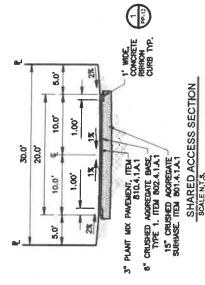
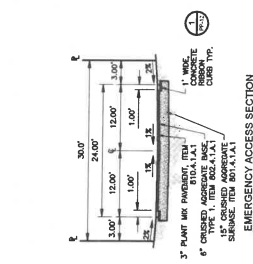


REVISIONS	
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QUARRY EAST SUBDIVISION
MIDLETON, IDHAO
PRELIMINARY PLAT
STREET SECTIONS

FILE 10/20/2017 PREPARED BY JUB ENGINEERS, INC. DATE 10/20/2017
DESIGNED BY JUB ENGINEERS, INC. DATE 10/20/2017
CHECKED BY JUB ENGINEERS, INC. DATE 10/20/2017
AT 11:00 AM, 10/20/2017
LAST UPDATED 10/20/2017

SHEET NUMBER:
PP-12





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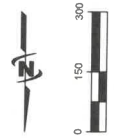
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5	19.87 Ac.	2.55 Ac.	12.8%
6	18.67 Ac.	2.55 Ac.	12.8%
7	16.43 Ac.	1.44 Ac.	8.8%
8	12.27 Ac.	1.97 Ac.	16.1%
9	238.61 Ac.	62.82 Ac.	26.5%
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TOTAL			12.1%

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REMAIN COMMON AREA 20.15 Ac.
TOTAL COMMON AREA 28.24 Ac.

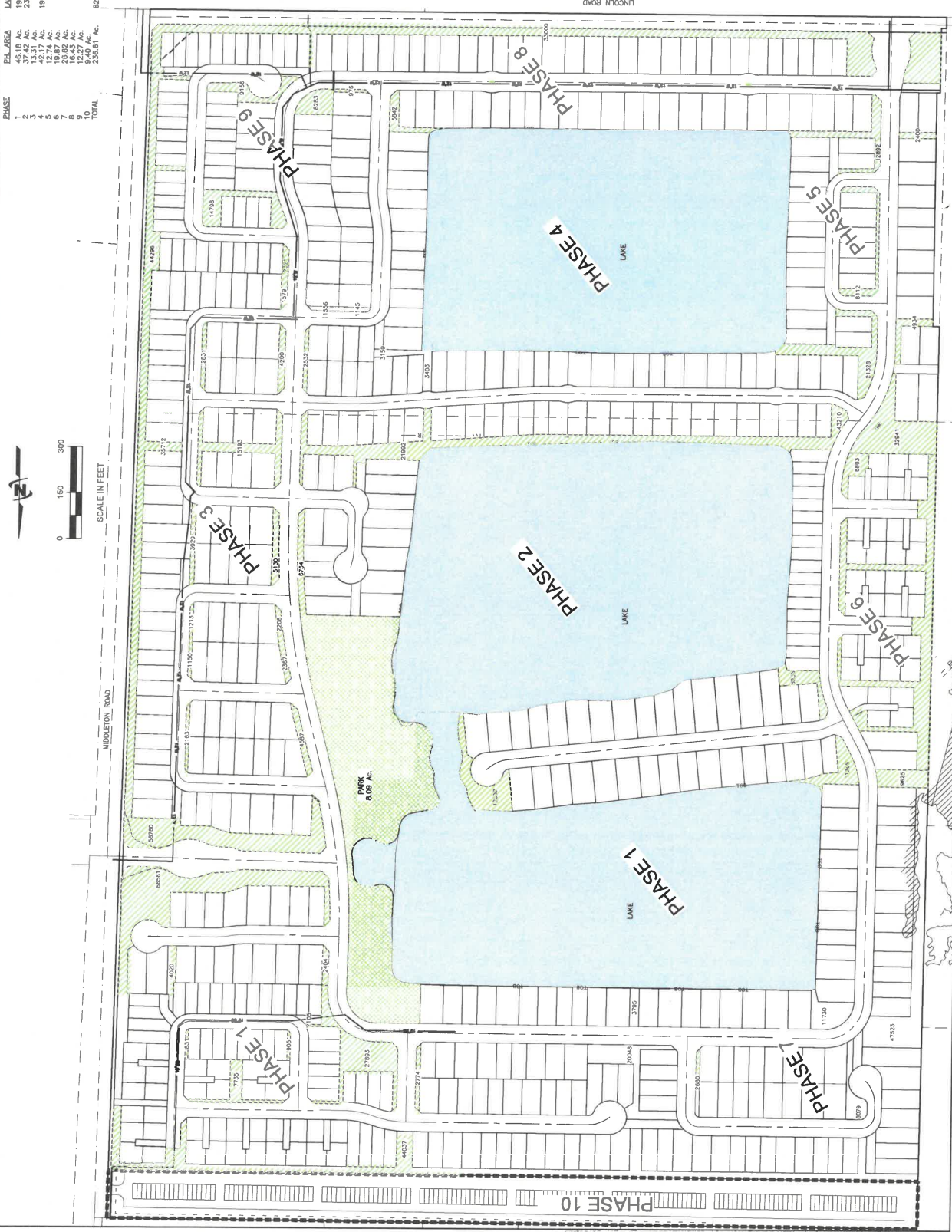
LEGEND
PARK AREA
COMMON AREA

QUARRY EAST SUBDIVISION
MIDDLETON, IDHAO
PARKS AND OPEN SPACE PLAN

FILE: OPEN SPACE PLAN
DATE: 10/20/2023
DRAWN BY: JUB
CHECKED BY: JUB
AT FULL SIZE: 11" X 17" ONE
LAST REVISED: 10/20/23
SHEET NUMBER: PP-13



SCALE IN FEET



PHASE 10

PHASE 4

PHASE 2

PHASE 3

PHASE 9

PHASE 7

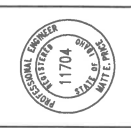
PHASE 6

PHASE 5

PHASE 8



JUB ENGINEERS, INC.
Suits 400
2760 W. EXCURSION LN.
MERRIDIAN, ID 83642
Phone: 208.376.7330
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LIST OF EXHIBITS	
NO.	DESCRIPTION
1	PRELIMINARY PLAT
2	LOT TYPE BREAKDOWN
3	QUARRY EAST SUBDIVISION
4	MIDDLETON, IDHAO
5	PHASE 1
6	PHASE 2
7	PHASE 3
8	PHASE 4
9	PHASE 5
10	PHASE 6
11	PHASE 7
12	PHASE 8
13	PHASE 9
14	PHASE 10

PRELIMINARY PLAT
LOT TYPE BREAKDOWN
QUARRY EAST SUBDIVISION
MIDDLETON, IDHAO

FILE: 10-00007.PLT
DATE: 10-00-07
CHECKED BY: JUB
AT FULL SIZE & NOT ONE
LAST REVISED: 10-00-07
SHEET NUMBER: PP-14

SINGLE FAMILY LOTS 466 LOTS
DUPLICATE LOTS 102 LOTS
CLUSTER LOTS 72 LOTS
TOTAL RES. LOTS 640 LOTS

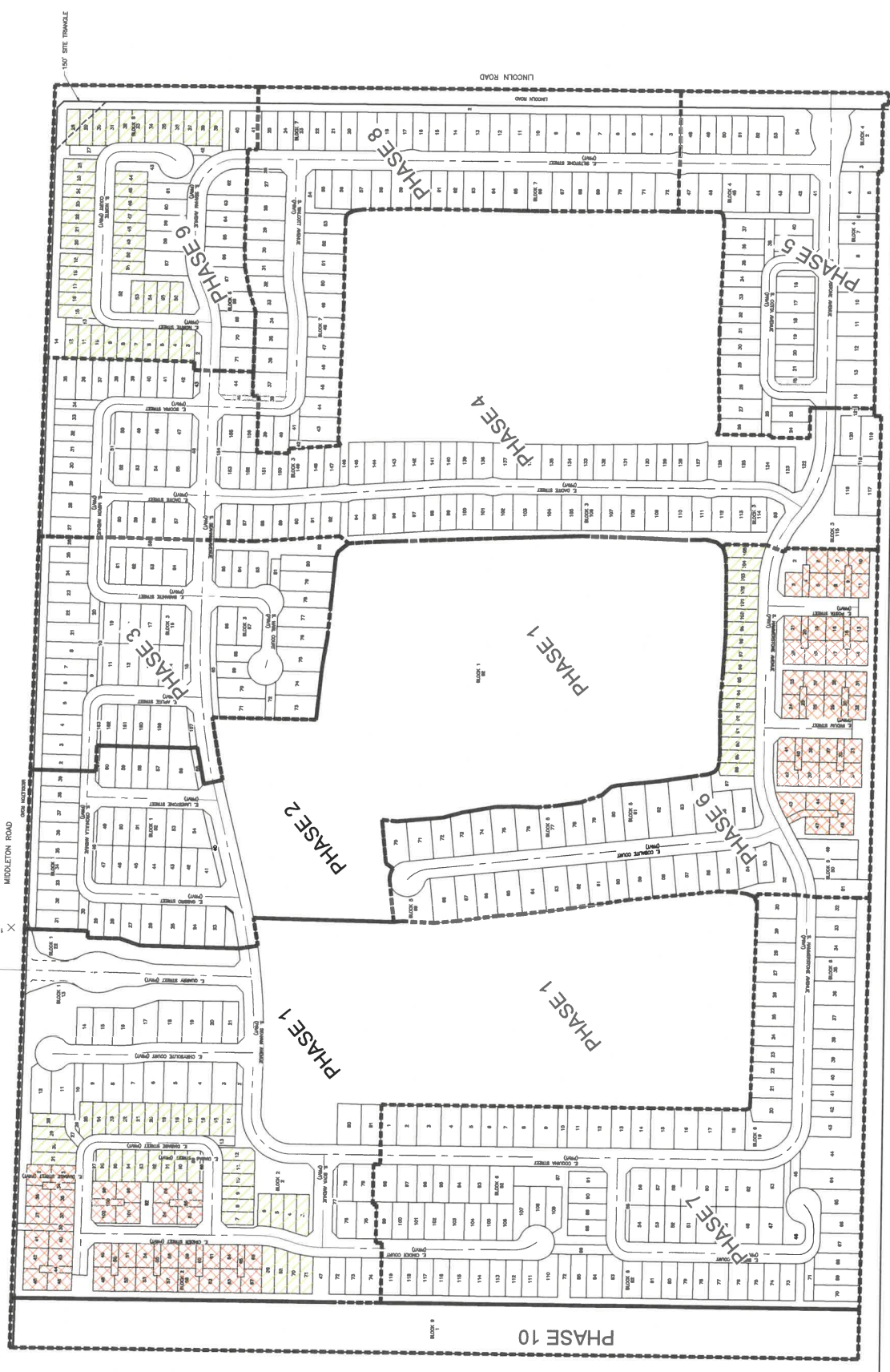
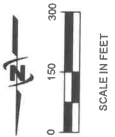


EXHIBIT “B”

Proposed Development Agreement

When recorded, send copy to:
Middleton City Hall
1103 W. Main St.
Middleton, ID 83644

DEVELOPMENT AGREEMENT

This Development Agreement (this **Agreement**) is entered into this ___ day of _____, 2022 (the **Effective Date**), by and between the CITY OF MIDDLETON, a municipal corporation in the State of Idaho (**City**), WOODLAND PROPERTIES LIMITED PARTNERSHIP, an Idaho limited partnership (**Owner**), and M3 ID WOODLAND, L.L.C., an Arizona limited liability company (**Developer**).

RECITALS

WHEREAS, Owner owns approximately 236.5 acres legally described in **Exhibit A** attached hereto and made a part hereof (**Property**). The Property is intended to be developed by Developer and referred to as "**Quarry East**";

WHEREAS, Developer has applied to the City to rezone the Property from County Agricultural, C-1, and M-1 to Mixed Use (M-U) (the **Applications**).

WHEREAS, Developer intends to improve the Property (the **Project**) in accordance with the Middleton City Code and the City's public works standards in effect at the time the construction drawings are approved (collectively **City Code**), except as modified by this Agreement; and

WHEREAS, the City, pursuant to Idaho Code Section 67-6511A, has the authority to annex and conditionally rezone the Property and to enter into a development agreement for the purpose of allowing, by agreement, a specific development to proceed in a specific area and for a specific purpose or use which is appropriate in the area, but for which all allowed uses for the requested zoning may not be appropriate pursuant to the Idaho Code and City Code.

AGREEMENT

NOW, THEREFORE, for good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, and in consideration of the recitals above, which are incorporated below, and the mutual covenants, representations, and performances herein bargained for, relied on, and expected, the parties agree as follows:

ARTICLE I LEGAL AUTHORITY

This Agreement is made pursuant to and in accordance with the provisions of Idaho Code §67-6511A and City Code, Title 5, Chapter 2.

ARTICLE II ZONING ORDINANCE AMENDMENT

The City will adopt an ordinance amending the Middleton Zoning Ordinance (the **Code**) to rezone the Property Mixed-Use (M-U) (the **Ordinance**). The Ordinance will become effective after it is approved, signed, published and recorded according to law, all of which actions the City will perform with the Developer's cooperation.

ARTICLE III CONDITIONS OF DEVELOPMENT

3.1 Zoning. The entirety of the Property shall be zoned Mixed-Use (M-U).

3.2 Development Density and Concept Master Plan. Quarry East shall contain no more than 650 residential units (the **Maximum Density**). The configuration of such units shall be substantially consistent with the approved **Master Plan** of the Project, attached as **Exhibit B** and made a part hereof by this reference. Developer, and its heirs, successors, and assigns, shall develop the Project substantially consistent with the Master Plan concept.

3.2.1 *Re-Allocation of Density*. Developer may apply administratively to the Planning & Zoning Official for an amended preliminary plat in order to change the phasing plan and/or make minor adjustments to roadways and lot lines. Developer may revise roadways and lot lines to a minimal degree so long as the final plat substantially conforms to the approved preliminary plat, which adjustments may include changes to accommodate orderly utility installation (such as where gravity sewer versus pressure sewer services are located) or traffic circulation. The minor adjustments may also include a reduction or increase in the size of phases (whether by number of lots or land area). Developer may also modify lot configurations administratively so long as such modifications do not result in an increase in density from the approved preliminary plat and any decrease in number of lots does not exceed a ten percent (10%) reduction of lots shown on the approved preliminary plat. The administrative fee for the amended preliminary plat application shall be the same fee set forth in the City's codified Fee Schedule for an application to amend a preliminary plat. If the Planning & Zoning Official deems the change to road design and/or lot lines to be more than a minor adjustment as outlined herein, then Developer must apply for an amended preliminary plat and/or phasing plan in compliance with the public hearing procedures set forth in the Middleton City Code.

3.2.2 *Elevations.* The single-family and duplex home elevation designs shall be of similar quality and design with the examples of higher quality elevation designs shown on **Exhibit C**, which exhibit is made a part hereof by this reference. City acknowledges and agrees that the foregoing elevations are conceptual in nature but show the intent of the Developer in connection with the Project.

3.2.3 *Quality of Design.* Developer, its successors, heirs, and assigns, shall construct the residential product, community infrastructure, and community amenities substantially consistent with the standards set forth in the Design Guidelines attached hereto as **Exhibit D**, which exhibit is made a part hereof by this reference. This provision does not prevent Developer from unilaterally amending its Design Guidelines with provisions that require equivalent quality or better. Rather, the Design Guidelines attached as **Exhibit D** shall set a minimum base level of quality that must be constructed by Developer, its successors, heirs, and assigns.

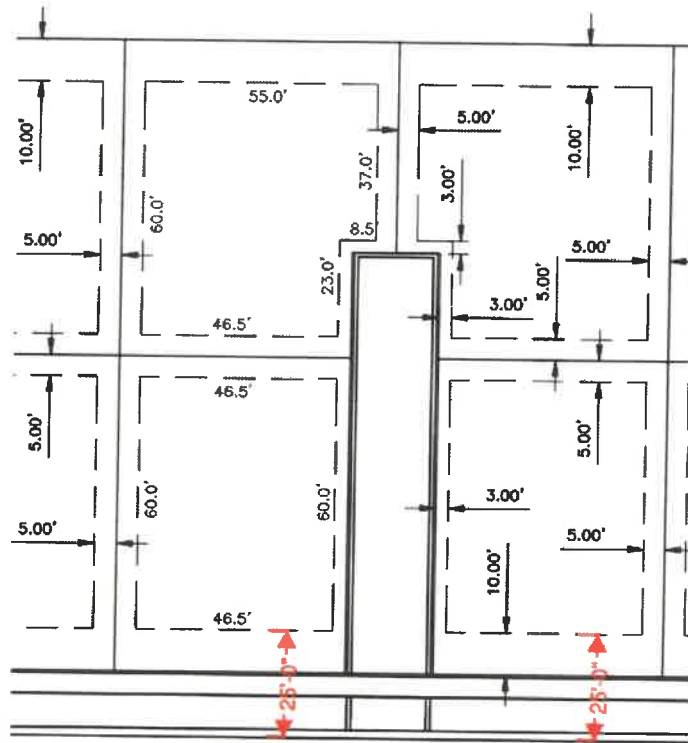
3.3 Modifications to Standards. Approved modifications of the requirements of the City's development density, lot size, and setback standards, as set forth in the Code, are attached as **Exhibit E** or are set forth in this Section 3.3. In the event of any inconsistency between the Code and this Agreement, this Agreement shall control.

3.3.1 *Setback and Lot Size Standards.* The following setback and lot size standards shall apply in Quarry East (if not specified below, general Code requirements shall apply):

Duplex Lot	
Front (to residence or side-load garage)	10 feet
Front (to garage)	20 feet
Rear	15 feet
Side	5 feet (0 feet for attached)
Minimum Lot Size	4,000 square feet
Minimum Width (N/A for irregular shaped lots)	40 feet
Cluster 4 Pack Lots	
Front (to street or side-load garage)	10 feet
Front (to garage)	20 feet or 5 feet
Rear (back of back lot)	10 feet
Side (all interior property lines)	5 feet (3 feet to shared drive)
Minimum Lot Size	4,000 square feet
Minimum Width (N/A for irregular shaped lots)	45 feet

Single Family Standard Detached Lot	
Front (to residence or side-load garage)	10 feet
Front (to garage)	20 feet
Rear	15 feet
Side	5 feet
Minimum Lot Size	5,000 square feet
Minimum Width (N/A for irregular shaped lots)	40 feet

A depiction of the Cluster 4 Pack Lot setbacks is included below:



3.3.2 *Additional Use Standards.* Notwithstanding the uses set forth in Middleton City Code for the Mixed-Use zone, the only uses allowed in the Project shall be (a) single-family residential homes, (b) duplex residential homes, (c) sales offices, including marketing trailers, model home complexes, and construction trailers during the marketing phase of the Project, (d) community recreational facilities and (e) mini self-storage, including RV storage at such intensities and at such locations as identified in this Agreement and its exhibits, including, but not limited to, the preliminary plat. The mini self-storage is subject to the Design Review Process as set forth in the Middleton City Code, and Developer may not apply for a building permit until Developer obtains approval for the design from the Middleton Design Committee. The design for the mini-self storage facility may not exceed a single story.

3.3.3 Initial Building Permits. Upon Developer's notice of completion of all-weather access roads and fire protection facilities for any phase of the Project (and City Engineer's approval of the same), Developer shall be eligible to submit applications for building permits for the construction of up to ten (10) model homes and community facilities, collectively, which will be processed and approved upon completion of applicable City requirements. Developer and City agree that the model homes and community facilities cannot receive a certificate of occupancy until Developer obtains final plat approval for the phase in which the model homes and/or

community facilities are located. A model home may not be operated as a model home for public entrance and viewing until Developer obtains a Certificate of Occupancy for the model home. During the construction of the model homes and/or community facilities, Developer shall provide access to the Caldwell Fire Department and City in accordance with then-applicable emergency service standards or as otherwise provided in this Agreement.

3.3.4 *Private/Public Streets & Storm Facilities.* City acknowledges and agrees that private streets with gated entries may be used in Quarry East, subject to the following standards:

3.3.4.1 Public Service Providers. Private streets shall be accessible to public service providers, including without limitation, police, fire, ambulance, garbage collection, electrical, cable and telephone line installation and repair, domestic and irrigation water, sewer line installation and repair, and other similar public purposes.

3.3.4.2 Street Standards. Notwithstanding anything to the contrary in City Code, street sections shall be built in accordance with the approved preliminary plat and standards shown on **Exhibits E & F** attached hereto and made a part hereof by this reference.

3.3.4.3 Private Street & Stormwater Facilities Reserve Study. The Project's restrictive covenants shall include a requirement that a maintenance plan be developed and maintained by the homeowners' association responsible for maintaining and repairing the private streets and stormwater facilities, including open water bodies receiving storm water. Said restrictive covenants shall also state that the City may, at its sole discretion, bring an action at law or in equity to cause the homeowners association to fulfill its obligation to repair and maintain the streets and storm facilities in good condition. Such rights of enforcement shall be limited to the failure of the homeowners association to provide for reasonable and customary maintenance of the private streets and storm facilities, and shall require written notice citing the repairs the homeowners association needs to make to the private streets and storm facilities and a reasonable time period during which the homeowners association may cure the alleged defect(s), which time shall not exceed ninety (90) days unless the homeowners association is undertaking commercially reasonable efforts to resolve the same and cannot, due to factors not within its control, complete such resolution within said 90-day time period. Any award by a court with jurisdiction associated with such enforcement action shall only be used to cause the homeowners association to conduct (and pay for) such maintenance, which may include requiring the homeowners association to levy a special assessment in the event that the reserve funds are inadequate for required repairs and replacement costs. In the event the City brings an action at law or in equity as outlined in this section,

the unsuccessful party to such litigation shall pay to the prevailing party all costs and expenses, including reasonable attorneys' fees, incurred therein.

For clarity, **in no way shall the provisions set forth in this section be deemed to require the City to take any particular action**, nor shall it be deemed an assignment to any extent of the homeowners association declarant rights to the City.

Prior to approval of phase 1 final plat, Developer shall submit a reserve study for the Project to the City showing the monetary amount required to adequately maintain, repair and replace the private streets and storm facilities, including in phase 1. The reserve study shall also set forth a method or timing of collection that will ensure said reserve amount will be collected in a sufficient amount of time to cover reasonably anticipated and customary maintenance, repair and replacement costs for said streets and storm facilities in accordance with industry standards. Prior to the final plat approval for each phase thereafter, Developer shall submit financial information showing that such amounts are being collected and such budgeted amounts are in accordance with the reserve study in an amount required to adequately maintain, repair and replace the private streets and storm facilities constructed to date. Prior to approval of the final phase final plat, the Developer shall submit an updated reserve study and shall provide evidence that the reserves have been funded current. If the foregoing is not the case, Developer shall deposit funds identified as deficient or required in the study to fund the reserves due as of the time of such review. Upon the request of the City, but no more frequently than every three years, the homeowners' association shall be required to submit to the City financial statements and budgets showing that the required funds are being set aside in the reserve account for repair and maintenance of all the private streets and storm facilities in the community unless the City and the homeowners' association mutually determine that such reserve studies are no longer necessary. **The association shall be responsible for the ongoing maintenance, replacement, and repair of the private streets and storm facilities and shall keep the private streets and storm facilities in good condition per state, municipal and industry standards and consistent, at the very least, with similar subdivisions in the City of Middleton. The association shall be responsible for ensuring the ongoing safe operation and maintenance of the private streets and storm facilities and shall indemnify, defend, and hold the City harmless regarding any claims associated with the operation and maintenance of the same. In no event is the City obligated to take on the operation or maintenance or operation of the private streets and storm facilities within the Project.**

3.3.4.4 Secondary Access. Developer shall provide a secondary access suitable for public use and approved by City Engineer and Canyon County

Highway District 4 ("CHD4"), prior to the 51st building permit. This second access may be re-located so long as City Engineer and CHD4 approve any changed location.

3.4 Domestic Water and Sewer Facilities. City will reserve sewer and water capacity at approval of construction drawings for each phase as the Project progresses to completion. Developer is responsible for all planning, engineering, and costs associated with extending City utilities, including sewer and water facilities, "to and through" the Property and tying the same into the existing City sewer and water systems. Such costs shall include, but not be limited to, the cost to design and construct the facilities, cost to obtain all necessary permits, and cost to acquire necessary utility easements outside of City-controlled property. City will permit installation of City utilities in areas controlled by the City (in fee or by easement). If City requires Developer to oversize the utility facilities to accommodate future growth off-site involving unrelated development, City shall reimburse Developer for over-size work or additional lines (beyond the capacity required for the Project) by entering into a mutually agreeable latecomer's fee agreement to be negotiated between Developer and City pursuant to MCC 1-17-1 or by some other mutually agreeable reimbursement agreement as allowed by law and mutually agreed to by the parties. Said reimbursement agreement shall provide for reimbursement on mutually agreeable terms for design, permitting, and acquisition of necessary easements, and shall further contain a provision wherein City will reserve sufficient capacity for Developer to fully complete the Project construction in the future. Sewer and water main lines larger than eight inches (8") are not presumed oversized and shall require evaluation on a case-by-case basis. Installation of a 12" main in Middleton Road, Lincoln Road, under, over or through the Boise River and one 12" main through the development is not considered oversized and subject to reimbursement.

A water line extension under, through or across the Boise River will be necessary to serve the Property. Developer will be responsible for constructing the water line and shall build the water line in compliance with the specifications set forth in the City's Water System Plan and/or project specific plans and specifications for the project in effect at the time of construction. The water line extension must be operational prior to final plat approval of Phase 2, although City, in its sole discretion, may elect to require completion of the water line extension in a later phase.

Developer will be responsible for only its proportionate share of the water line improvement costs (based on overall volume, including fire flow, required to serve Quarry East as compared to the volume made available by such an improvement). Therefore, Developer shall be reimbursed for any costs over and above its proportionate share via a latecomer fee agreement as set forth in MCC 1-17-1 or other mutually agreeable reimbursement agreement, which agreement shall provide for the reservation of domestic water capacity sufficient for Developer to fully complete the Project construction in the future.

Developer anticipates constructing a sewer lift station inside the project to serve the Property and a future Quarry subdivision. The Developer will also be required to reconstruct

and expand the Boise River Lift Station for purposes of completing sewer service to the Property. To the extent such on site lift station or Boise River lift station expansion provides capacity to City's sewer system beyond what is required to serve the Project based on peak hour flows, such additional capacity shall be subject to reimbursement via latecomer fee agreement set forth in MCC 1-17-1 or other mutually agreeable reimbursement agreement, which agreement shall provide for reservation of sewer capacity sufficient for Developer to fully complete the Project construction in the future.

If, prior to construction of any of the water or sewer improvements described above, the City adopts a water impact fee and/or sewer impact fee, and if any of the foregoing improvements are included on a capital improvements plan, reimbursement to the Developer for construction of eligible improvements will be permitted in accordance the terms of such impact fee ordinance for capacity greater than required to serve the Project.

3.5 Natural Gas. Developer is responsible for extending natural gas service to the Property. The City shall not share in the cost of extending the natural gas service to the Property.

3.6 Irrigation. Developer will provide its own private pressurized irrigation system for the Property in conformance with Idaho Code 38-3105.b. No irrigation hookups within the Project will connect to City domestic water, and the CC&Rs for the Project shall indicate that domestic water shall not be used for irrigation and landscaping on any common/open space lots, residential lots, or commercial/industrial lots in the community.

3.7 Open Space. The Project shall contain a minimum of twenty-five percent (25%) open space that includes ponds, common area parks, clubs, and landscape frontage along Middleton and Lincoln Roads.

3.8 Urban Renewal Revenue Allocation Area. The parties agree and acknowledge that the City of Middleton Urban Renewal Agency, an independent body, corporate and politic (the "Agency") and the City may form an Urban Renewal Revenue Allocation Area ("RAA"), as authorized by Chapters 20 and 29, Title 50, Idaho Code, as amended, which RAA boundary may include all, or a portion, of the Project. In the event Urban Renewal Plan is adopted by City Council ordinance, then the RAA shall be administered and implemented by the Agency. Depending on Idaho law and the adopted Urban Renewal Plan, certain public infrastructure projects may be eligible for reimbursement as determined by the Agency, not the City. The Agency is a separate and distinct entity from the City. The City acknowledges that it will not hinder Developer from seeking reimbursement from the Agency for certain public infrastructure projects, including those identified in Section 3.4 hereof, but eligibility for reimbursement and related terms cannot be guaranteed. Nothing contained herein shall be construed as a waiver by Developer of its ability to pursue reimbursement as provided above.

If Developer receives any reimbursement from RAA for any infrastructure improvement constructed by Developer, then to the extent such reimbursement encompasses payments

previously made to Developer pursuant to latecomer fees, credits (including impact fee credits), or reimbursements, Developer shall pay back any such latecomer fees, credits or reimbursement actually received by Developer for said infrastructure improvements.

3.9 Age Restriction. All residential lots shall be age-restricted to 55+ ownership and occupancy to the extent allowed under the laws of the State of Idaho or the United States. The CCRs for the Project will include said restrictions, which shall identify the authority for such restrictions, qualification requirements for occupants, disclosure requirements in connection with transfer or sale, as well as monitoring requirements. For clarity, the CCRs will require, in accordance with the Fair Housing Amendments Act, 42 U.S.C. § 3601, et seq. (1988), as amended (the “**Fair Housing Act**”) and the exemption therefrom provided by 42 U.S.C. § 3607(b)(2)(C) regarding discrimination based on familial status and that at least eighty percent (80%) of the occupied residential units within the project be occupied by at least one person who is 55 years of age or older. No lot shall be occupied by any person under the age of 18, where “occupied” refers to any person who stays overnight in the lot for more than 60 days in any 12-month period. The homeowners’ association shall be solely responsible for enforcement of the foregoing restrictions in accordance with the CCRs.

3.10 Roadway Construction Requirements.

3.10.1 Frontage Improvements. Except as otherwise set forth herein or as required by City or Idaho code, Developer shall be required to construct the half road improvements along Middleton Road and Lincoln Road and dedicate the fully improved rights-of-way to the City. The Road frontage improvements shall be constructed in compliance with the requirements of Canyon Highway District No. 4 (“CHD4”) or the City, depending on who has jurisdiction at the time of construction.

3.10.2 Middleton Road Frontage: The Middleton Road frontage is included in a capital improvement plan covering the Mid-Star Service Area (“Mid-Star CIP”). In order to avoid duplicate development requirements, Developer is entitled to reimbursement for all portions of the Middleton Road frontage improvements Developer constructs that are included in the Mid-Star CIP. Improvements that are not included in the Mid-Star CIP and would not be subject to reimbursement may include, but are not limited to, sidewalks, landscaping, storm facilities, pedestrian/bike facilities, and turn lanes to serve the project. Such improvements are considered “project improvements” rather than “system improvements”. Any reimbursement shall be in compliance with the provisions set forth in Idaho Code § 67-8209. Additionally, prior to Developer constructing the impact-fee eligible frontage improvements, Developer, City and/or CHD4 shall enter into a written reimbursement agreement that will address reimbursement sources and timing that is mutually agreeable to the parties and allowed by law.

3.10.3 Quarry Street Intersection Control. The Mid-Star CIP includes a multi-lane roundabout at the intersection of Middleton Road and what is referred to on the preliminary plat as E. Quarry Street. Developer shall reserve on its preliminary plat and any subsequent final plats sufficient right of way for the construction of said multi-lane roundabout. The multi-lane roundabout may be built by public agencies, including the City, pursuant to the procedures set forth in the Mid-Star CIP. However, if the City is able to obtain sufficient right of way for the full construction of the roundabout prior to issuing Developer one hundred (100) building permits, then Developer shall be required to design and construct the roundabout and dedicate its half-road portion of the roundabout to the City. Once City notifies Developer in writing that it has obtained the necessary right of way for roundabout construction, Developer will have eighteen (18) months to design and construct the roundabout. The roundabout design must be in compliance with City specifications and be approved by the City. Failure to construct the roundabout within eighteen (18) months of City's written notice will result in the City denying Developer final plat approval for any subsequent final plats at the time the eighteen (18) month period expires. Completion of the roundabout and dedication of the same shall thereafter become a condition of final plat approval for the next phase. Because the multi-lane roundabout is part of the Mid-Star CIP, Developer shall be entitled to reimbursement pursuant to Idaho Code § 67-8209 for the portion of the roundabout design and construction included in the Mid-Star CIP, with such reimbursement to occur as soon as reasonably possible and may occur through Transportation impact fee credits (in strict compliance with the reimbursement provisions set forth in Idaho Code § 67-8209) to be issued to Developer as Developer pulls building permits on an ongoing basis within the Project.

3.10.4 If City ultimately does not obtain right of way for the roundabout by the issuance of one hundred (100) building permits, then Developer shall not be required to construct the roundabout at any time in the future. However, developer shall be required to construct sufficient turn lanes and connected frontage improvements to ensure safe ingress and egress to the Project prior to final plat approval for phase 1. Additionally, Developer shall be required to design and construct a fully actuated four-way traffic signal at the intersection of E. Quarry Street and Middleton Road once the City obtains sufficient right of way for the construction of said traffic signal. Developer is not entitled to any reimbursement for the traffic signal, and Developer shall design and construct the traffic signal at its sole cost and expense. The traffic signal shall be a pole traffic signal designed to the specifications of the City. Span wire will not be allowed. The traffic signal shall include two (2) luminaries to light the intersection and pedestrian facilities with detectable domes. The poles shall be installed within the right of way existing at the time of construction. There will be no requirement to install the poles in locations that would accommodate future widening of Middleton Road. Developer will

not be required to construct the traffic signal until after two hundred (200) building permits have been issued and City has notified Developer in writing that it has obtained sufficient right of way for the traffic signal construction. Developer will have twelve (12) months after said written notice to complete the traffic signal construction. Failure to construct the traffic signal within twelve (12) months will result in the City denying Developer final plat approval for any subsequent final plats at the time the twelve (12) month period expires. Construction of the traffic signal will thereafter become a condition of approval for final plat approval of the next phase.

3.10.5 Notwithstanding the above, Developer may elect at any time to construct the roundabout planned for Middleton Road and E. Quarry Street subject to a reimbursement agreement as set forth in Idaho Code §67-8209 for any impact-fee eligible design and construction costs.

3.10.6 Lincoln Road & Middleton Road Intersection. The Lincoln Road and Middleton Road intersection design shown in the Middleton Road 2016 Corridor Study includes a re-alignment of Lincoln Road to the south and the construction of a multi-lane roundabout where there is currently no public right-of-way. Because the Lincoln Road/Middleton Road roundabout may not be constructed for a number of years, Developer agrees to design and construct a fully actuated traffic signal at the intersection of Middleton Road and the northern leg of Lincoln Road. Right of way for this traffic signal shall be shown on the Project's preliminary plat and final plat. Developer will not be entitled to reimbursement for the cost of designing and constructing the traffic signal, and Developer shall construct the traffic signal at its sole cost and expense. Developer's obligation to design and construct said traffic signal will be triggered by the occurrence of two events: (1) Developer has submitted construction drawings for the Phase that contains the first public access on to Lincoln Road and (2) City has notified Developer in writing that it has acquired sufficient right of way for the construction of the traffic signal. Once Developer receives said written notice, Developer shall have twelve (12) months to complete construction of the traffic signal. The signal shall be a pole traffic signal designed to the specification of the City. Span wire construction will not be allowed. The signal shall include two (2) luminaries to light the intersection and pedestrian facilities with detectable domes. The traffic signal shall be installed within the then existing right of way. Failure to construct the traffic signal within twelve (12) months will result in the City denying Developer final plat approval for any subsequent phase at the time the twelve (12) month period expires. Construction of the traffic signal will thereafter become a condition of approval for final plat approval of the next phase.

3.10.7 Developer may also elect to construct the roundabout planned for Middleton Road and Lincoln Road subject to a reimbursement agreement as set forth in Idaho Code §67-8209.

3.10.8 Access for Storage Facility. Access shall be allowed from Middleton Road into the storage facility lot currently shown as Phase 10 of the preliminary plat. The City shall determine the degree of access allowed, including the possibility of a limitation of access to right in/right out only and the construction of a permanent barrier to prevent left turn movement.

3.11 ITD Traffic Fee Requirement. The intersection of Hwy 20/26 and Middleton Road is not included in the Mid-Star CIP. Therefore, Developer shall pay the four hundred fifty six thousand three hundred thirty five dollars (\$456,335.00) proportionate share traffic fee set forth in Idaho Transportation Department's June 8, 2021, Development Conditions Memo . The amount due was based upon buildable lots included in both The Quarry East and The Quarry West subdivisions. Therefore, the amount due should be divided equally across the buildable lots entitled on both approved preliminary plats ("ITD Fee"). If ITD revises the fee amount or eliminates the fee, City will require Developer to pay only what ITD requires after said revision. Developer shall remit the ITD Fee to the City for payment to ITD in phases. Specifically, prior to the approval of final plat for any phase, Developer shall remit the ITD Fee to the City based on the number of residential units in said final plat.

3.12 Final Plat Requirements. Notwithstanding the provisions in Article IV, Developer shall obtain City Engineer's signature on the Phase 1 final plat within four (4) years of the preliminary plat approval. Said signature shall be processed expeditiously by City and in no event later than six (6) months from the time the Developer submits a "Completion Packet" with all items required by the Supplement to the ISPWC and final plat application. The six (6) month period will not be triggered if any information required by the Supplement is missing. Developer may obtain a two (2) year extension to obtain the City Engineer's signature on Phase 1 final plat by submitting an administrative written request for extension to the Middleton Planning & Zoning Official before the expiration of the initial four-year period, which approval shall not be withheld if Developer submits its request in a timely manner. The application request before City Council does not require a public hearing. If Developer does not obtain City Engineer's signature on final plat for Phase 1 within the time frame noted above, City may, at its sole discretion, terminate the entire Development Agreement, and the Project property will revert to M-1 (light industrial) or R-3 (single family residential) zoning, as determined by City, after following the notice and hearing provisions set forth in the Middleton City Code. Additionally, the preliminary plat will automatically become null and void. City may seek termination of the Development Agreement at any time after the noted time periods expire, and City's delay in terminating the Development Agreement shall not constitute a waiver of its right to terminate the Development Agreement.

After final plat approval for the first phase, Developer shall obtain City Engineer's signature on the final plat for each phase in two (2) year intervals (subject to the requirement by the

City to process such applications expeditiously and in no event later than six months from the time Developer submits a fully completed "Completion Packet" application and final plat application). Developer may obtain a two (2) year extension to obtain the City Engineer's signature on the final plat for the subsequent phase by submitting an administrative written request for extension to the Middleton Planning & Zoning Official before the expiration of the previous time period, which approval shall not be withheld if Developer submits its request in a timely manner. If Developer fails to comply with said timelines, the portions of the preliminary plat yet to be final platted will become null and void. The zoning for the unplatted area shall revert to R-3 (single family residential) zoning.

3.13 LOMR Requirements. Developer shall comply with all FEMA requirements, including completion of a letter of map revision (LOMR) to remove all lots with improvements out of the flood plain. Pending LOMR approval, the Developer may receive building permits if elevation certificates are provided in connection with the same. However, Developer shall still comply fully with MCC 4-3-7-5(D), which requires elevation certificates for individual lots even after the LOMR is approved.

3.14 Community Amenities. Developer shall provide, at a minimum, the following community amenities: community swimming pool, six (6) pickleball courts, picnic area, and community beach (collectively, the "**Phase 1 Amenities**"), and a clubhouse (the "**Phase 2 Amenities**"). Swimming Pool/clubhouse shall be located on common lot 62, Block 1 and shall be built generally consistent with the site plan attached hereto as **Exhibit G**. The Phase 1 Amenities shall be completed no later than the issuance of the one hundred and thirty fourth (134th) building permit. The Phase 2 Amenities shall be completed no later than the issuance of the two hundred and seventy second (272) building permit.

3.15 Utility Easements and Ownerships. All constructed utilities are public and Developer shall grant access and utility easement at width requested by agency having jurisdiction covering the constructed utilities in locations identified on the plat and as required by public utilities or agencies having jurisdiction, including the City.

3.16 CCR & Purchase Agreement Disclosure Requirement. Developer shall state the following disclosure in every purchase agreement and in the CC&Rs: *"City of Middleton and Canyon County Planning documents show that the area directly north and east of The Quarry will be the location of a significant amount of light and heavy industrial businesses. Pursuant to the City's Future Land Use Map and Zoning Map, the City of Middleton will be actively seeking industrial users to locate in the areas surrounding The Quarry."*

3.17 Additional Conditions. The Project shall be subject to those further conditions of approval associated with the approval of the Application as contained on **Exhibit H** attached hereto and made a part hereof.

ARTICLE IV FAILURE TO COMPLY WITH AGREEMENT

4.1 Event of Default. If Developer fails to comply with any of the terms or conditions in this Agreement, then the portion(s) of this Agreement pertaining to the breach may be modified or terminated by the Middleton City Council, after complying with the notice and hearing requirement contained in Middleton City Code and Idaho Code.

If after a breach, the City Council determines that the terms of this Agreement applicable to the breach should be modified, the term(s) of this Agreement shall be amended and the Developer shall comply with the amended terms.

Any breach waived by the City shall apply solely to the breach waived and shall not bar any other rights or remedies of the City or apply to any subsequent breach of any such or other covenants and conditions.

4.2 Reversion of Zoning. If after a breach and failure to cure as provided in Section 4.4, the City, after complying with the public hearing requirements of the Middleton City Code, may change the zoning for the Property as follows: (a) if prior to final plat approval for phase 1, then the City may change the zoning to M-1 (light industrial) or R-3 (single family residential) in its sole discretion, and the Developer hereby consents to such change; (b) if after final plat approval for phase 1, then the City may change the zoning to R-3 and the Developer hereby consents to such change.

4.3 Judicial Enforcement. Upon a breach of this Agreement, any of the parties in any court of competent jurisdiction, by action or proceeding at law or in equity, may secure the specific performance of the covenants and agreements herein contained, may be awarded damages for failure of performance of both, or may obtain rescission, disconnection, and damages for repudiation or material failure of performance.

4.4 Notice of Failed Performance and Opportunity to Cure. Upon any failure of any party to this Agreement to perform its obligations under this Agreement, the party claiming such failure shall notify, in writing, the party alleged to have failed to perform of the alleged failure and shall demand performance. No breach of this Agreement may be found to have occurred if performance has commenced to the reasonable satisfaction of the complaining party within thirty (30) days of the receipt of such notice.

ARTICLE V GENERAL PROVISIONS

5.1 Entire Agreement. This Agreement contains the entire agreement of the parties respecting the Property and supersedes all prior discussions and written and verbal agreements between the parties respecting the Property.

5.2 Amendment: Annexation.

5.2.1 Any amendment or addendum to this Agreement shall be in writing and made only after City has complied with the notice and hearing provisions of Idaho Code §67-6509 and Middleton City Code Title 5, Chapter 2.

5.2.2 Additional real property may be added to the definition of Property with such amendment, and the additional property identified shall, thereafter, be subject to this Agreement. If additional property is to be included with such amendment, Developer shall apply for annexation and zone change following the public hearing process set forth in the Middleton City Code. Any property annexed into Quarry East shall be subject to all provisions of the Development Agreement. A new concept plan for the additional property shall be appended to the amended development agreement.

5.3 Notice. Any notice that a party may desire to give to another party must be in writing and may be given by personal delivery, by mailing the same registered or certified mail with a return receipt requested, or by Federal Express or other reputable overnight delivery service. Notice shall be given to the parties at the following addresses or such other address and to such other persons as the parties may designate after giving notice. Any such notice shall be deemed given upon delivery if by personal delivery, upon deposit in the United States mail if sent by mail pursuant to the forgoing:

Middleton:	City Clerk City of Middleton P.O. Box 487 Middleton, Idaho 83644
Developer:	M3 ID Woodland, LLC Attn: William Brownlee 7033 E. Greenway Parkway, Suite 100 Scottsdale, Arizona 85254
With a copy to:	M3 Companies Attn: Mark Tate 1087 W. River Street, Suite 310 Boise, Idaho 83702
With a copy to:	Clark Wardle LLP Attn: Hethe Clark PO Box 639 Boise, Idaho 83701

5.4 Enforcement Costs. If either party shall fail to perform under this Agreement and said failure is cured with the assistance of an attorney for the other party, as a part of

curing said failure, the reasonable attorneys' fees incurred by the other party shall be reimbursed to the other party upon demand. In the event a suit or action is filed by either party against the other to interpret or enforce this Agreement, the unsuccessful party to such litigation agrees to pay to the prevailing party all costs and expenses, including reasonable attorneys' fees incurred therein, including the same with respect to an appeal.

5.5 Effective Date; Recordation. The Agreement shall be effective only after approval by the City Council and execution by Developer, the Mayor, and City Clerk. After its execution, this Agreement shall be recorded in the office of the County Recorder at the expense of Developer.

5.6 Binding Effect. Each commitment and restriction described in this Agreement shall be a burden on the Property and run with the land and shall be appurtenant to and for the benefit of the Property, adjacent property and other residential land near the Property. This Agreement shall be binding on City and Developer, and their respective heirs, administrators, executors, agents, legal representatives, successors and assigns. Provided, however, that if all or any portion of the Property is divided, then each owner of a legal lot shall only be responsible for duties and obligations or breaches as to their own parcels or lots. Any owner of the Property or any portion thereof (including, without limitation, any owner who acquires its interest by foreclosure, trustee's sale or otherwise) shall be liable for all commitments and other obligations arising under this Agreement with respect only to such owner's lot or parcel.

5.7 Interpretation. The Property that is the subject of this Agreement is located in Canyon County, Idaho and the terms of this Agreement shall be construed according to the laws of the State of Idaho in effect at this time this Agreement is executed. Any action brought in connection with this Agreement shall be brought in a court of competent jurisdiction located in Canyon County, Idaho.

5.8 Severability. If any term, provision, commitment or restriction of this Agreement or the application thereof to any party or circumstance shall to any extent be held invalid or unenforceable, the remainder of this instrument shall remain in full force and effect.

5.9 Time of the Essence. Time is of the essence for performance of each obligation in this Agreement.

IN WITNESS WHEREOF, the parties have hereunto caused this Agreement to be executed on the Effective Date.

[Signatures and notary on following pages]

ATTEST:

By: _____
Becky Crofts, City Clerk
Date: _____

SS.

Notary Public
My Commission Expires: _____

WOODLAND PROPERTIES LIMITED PARTNERSHIP,
an Idaho limited partnership

State of IDAHO)
County of _____) SS.

Notary Public
My Commission Expires: _____

M3 ID WOODLAND, LLC,
an Arizona limited liability company

By: M3 Builders, LLC,
an Arizona limited liability company
Its: Manager

By: The M3 Companies, L.L.C.,
an Arizona limited liability company
Its: Member

By: William I. Brownlee
Its: Manager

Date: _____

State of ARIZONA)
County of _____) SS.

I, a notary public, do hereby certify that on this _____ day of _____, 2020, personally appeared before me William I. Brownlee, the Manager of The M3 Companies, L.L.C, the Arizona limited liability company who is the Member of M3 Builders, LLC, the Arizona limited liability company, who is the Manager of M3 ID Moon Valley, LLC, an Arizona limited liability company, who, being first duly sworn, declared that he signed.

Notary Public
My Commission Expires: _____

SCHEDULE OF EXHIBITS:

- | | |
|------------|------------------------------------------------|
| Exhibit A: | Legal Description of Property |
| Exhibit B: | Master Plan |
| Exhibit C: | Elevations |
| Exhibit D: | Design Guidelines |
| Exhibit E: | Approved Code Modifications |
| Exhibit F: | Street Standards |
| Exhibit G: | Site Plan and Renderings of Clubhouse Property |
| Exhibit H: | Conditions of Approval |

EXHIBIT A
Legal Description of Property



**IDAHO
SURVEY
GROUP**

9955 W Emerald St
Boise, ID 83704

Phone: (208) 846-8570
Fax: (208) 884-5399

Quarry East
Boundary Description
Project Number 22-056 March 29, 2022

A parcel of land situated in Section 18, Township 4 North, Range 2 West, Boise Meridian, Canyon County, Idaho, and being more particularly described as follows.

BEGINNING at the south quarter-section corner of Section 18, Township 4 North, Range 2 West, Boise Meridian:

Thence N00°40'41"E, 36.00 feet along the north-south centerline of Section 18;

Thence S89°44'28"E, 16.72 feet;

Thence N00°15'32"E, 209.00 feet;

Thence S89°44'28"E, 11.50 feet;

Thence N00°15'32"E, 760.54 feet;

Thence N89°19'19"W, 21.13 feet to the north-south centerline of Section 18;

Thence N00°40'41"E, 1625.72 feet along the north-south centerline of Section 18 to the center quarter-section corner of Section 18;

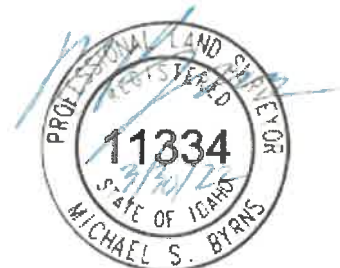
Thence continuing N00°40'41"E, 1315.68 feet to center-north sixteenth-section corner of Section 18;

Thence S89°30'28"E, 2643.62 feet along the east-west centerline of the northeast quarter of Section 18 to the west right-of-way line of Middleton Rd.;

Thence S01°21'46"W, 3936.98 feet along the west right-of-way line of Middleton Rd to the south line of Section 18;

Thence N89°44'28"W, 2596.62 feet along the south line of Section 18 to the POINT OF BEGINNING.

The above-described parcel contains 236.60 acres, more or less.



Add sketch of 2022 legal description

Exhibit "B"
Concept Master Plan



EXHIBIT C
Elevations

















































EXHIBIT D
Design Guidelines



QUARRY EAST SUBDIVISION

(Marketing Name TBD)

ARCHITECTURAL DESIGN GUIDELINES & CONSTRUCTION STANDARDS

*City of Middleton, State of Idaho
County of Canyon, State of Idaho*

Effective: _____

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I. INTRODUCTION

These Architectural Design Guidelines and Construction Standards ("**Design Guidelines**") are to be used as guidelines for the owner and builder in preparing plans and specifications for any proposed Improvement, construction, or installation within the Quarry East neighborhood and for maintaining an orderly construction environment. These guidelines are used by the Architectural Reviewer in conjunction with the Declaration. The Quarry East neighborhood will be comprised of various individual areas, each of which may have similar but varying requirements. The Architectural Reviewer reserves the right to grant variances or modify these standards as it deems appropriate and in accordance with the Declaration.

The Quarry East Declaration of Covenants, Conditions, Restrictions, and Easements, recorded as Document No. _____, Official Records of Canyon County, Idaho, as the same may be amended from time to time (the "**Declaration**") provides that no Owner may commence the construction of any Improvement without the prior written approval of the "**Architectural Reviewer**", which shall mean the Architectural Control Committee or ACC as defined in the Declaration. Pursuant to the Declaration, M3 ID WOODLAND, LLC, an Arizona limited liability company ("**Declarant**") retains the right to appoint all individuals comprising the Architectural Reviewer until one hundred percent (100%) of the Property has been developed and conveyed to Owners other than builders, unless such right is surrendered prior to such time in accordance with the provisions in the Declaration. As long as the Declarant retains such right, the Architectural Reviewer shall in no event be a committee created by or appointed by the Board or the _____ Homeowners' Association, Inc., an Idaho nonprofit corporation ("_____ HOA").

The Architectural Reviewer will not be required to review any plans until a complete review package, as required by the Declaration and these Architectural Design Guidelines and Construction Standards ("**Design Guidelines**"), is assembled and submitted to the Architectural Reviewer. The Declaration outlines other specific requirements governing architectural control imposed by the Architectural Reviewer which are not repeated in these Design Guidelines and which bind each and every Owner. The Architectural Reviewer has the authority to adopt such additional or alternate procedural and substantive rules and guidelines as it may deem necessary or appropriate in connection with exercising its rights hereunder.

The approval of the Architectural Reviewer of any plans or specifications for any work done or proposed in connection with any matter requiring the approval or consent of the Architectural Reviewer will not be deemed to constitute a waiver of any right to withhold approval or consent as to any plans and specifications on any other matter, subsequently or additionally submitted for approval by the same or a different person, nor will such approval or consent be deemed to establish a precedent for future approvals by the Architectural Reviewer.

The Architectural Reviewer may grant variances from compliance with any provisions in these Design Guidelines, when, in the opinion of the Architectural Reviewer, in its sole and absolute discretion, such variance is justified. All variances granted will be evidenced in writing to the builder or owner. The granting of such a variance will in not operate to waive or amend

any of the terms and provisions hereunder for any purpose, except as to the particular property and the particular instance covered by the variance, and such variance will not be considered to establish a precedent for any future waiver, modification, or amendment to these Design Guidelines.

The Architectural Reviewer is not responsible for: (i) errors in or omissions from the plans and specifications submitted for architectural review; (ii) supervising construction for the owner's compliance with approved plans and specifications; or (iii) the compliance of the owner's plans and specifications with applicable governmental statutes, codes, and ordinances, and municipal, county, state, and federal laws.

Capitalized words used in these Design Guidelines, but not otherwise defined herein shall have the same meaning ascribed to such terms in the Declaration.

II. SUBMITTAL AND APPROVAL PROCEDURES

A. Submittal Requirements. All submittals shall include the plans, specifications, application, and other information described below, as may be applicable. Construction may not commence until written approval is granted by the Architectural Reviewer.

The submittal plans and materials shall include specifications, dimensions, a legend of all symbols and abbreviations, and other applicable information necessary to perform such a review. Elevation views shall be provided for any proposed vertical Improvements (e.g. built-in BBQ, water feature).

The items listed below shall be digitally submitted to the Architectural Reviewer for approval (e.g. pdf, jpeg). The Architectural Reviewer may request physical samples at and/or additional material to be submitted at its discretion.

1. Application. Each submittal shall be accompanied by a completed design review application and checklist adopted by the Architectural Reviewer, (the "**Application**"), attached to these Design Guidelines as Attachment 1.
2. Site Plan. Show the Building Lot ("**Lot**") boundaries, the proposed location of all Improvements (including, but not limited to, all structures, driveways, sidewalks, fences, dog runs, patios, decks, outdoor lighting, utility meters, mechanical equipment, etc.), all easements, and all proposed setbacks. Include a roof plan as part of the site plan if a separate roof plan is not submitted. Use arrows to indicate the proposed grading and drainage flows off of the lot and away from the proposed residence and adjacent Lots. The Architectural Reviewer may require a separate grading and drainage plan be provided. (Minimum scale of 1/8" = 1'-0".)
3. Grading Plan. A separate grading plan must be provided in conjunction with an architectural submittal as well as with a landscape submittal for any Lake Front Lot (as defined below in *Section III.T*). Overlay the proposed grades over the

existing grades on a Lot plan showing all boundaries, easements, and applicable setbacks. Use arrows to indicate the proposed grading and drainage flow directions off of the Lot and away from adjacent Lots. (Minimum scale of 1/8" = 1'-0".)

4. Roof Plan(s). Depict roof masses, ridgelines, and roof pitches including pertinent dimensions (e.g. gable span, overhang depths) if not provided elsewhere. Show the locations of any roof drains or scuppers, as applicable. The roof plan may be included on the site plan in lieu of a separate roof plan. (Minimum scale of 1/8" = 1'-0".)

5. Floor Plan(s). Include the per floor square feet and total finished square feet of the residence, exclusive of garages, covered patios, storage areas, etc. Provide exterior dimensions on all floor plans. (Minimum scale of 1/4" = 1'-0".)

6. Building Elevations. Illustrate front, rear and side views, showing the maximum building height and include proposed exterior finish material descriptions. (Minimum scale of 1/8" = 1'-0".)

7. Specifications. Include the specifications and descriptions of any proposed exterior materials, finishes, equipment, etc.

8. Exterior Materials and Colors. Include digital samples of the color of proposed exterior material colors, (e.g. paint, stain). Samples of all other materials (e.g. brick, stone) shall be shown on the elevations. All proposed exterior materials and colors shall be identified by name, number, color, and manufacturer. The Architectural Reviewer may request physical samples of any proposed exterior colors or materials in which case the submittal will not be considered complete until all requested materials have been received.

9. Landscape Plan. Show the proposed landscape and hardscape including dimensions and any proposed fencing and include a legend of the plant types, sizes and quantities. A legend shall be included for plant types, sizes and quantities. Elevation views of each side shall be included for any vertical Improvement not reflected elsewhere in the submittal. Use arrows to indicate the proposed grading and drainage flows off of the lot and away from the proposed residence and adjacent Lots. It is preferred that the landscape plan be submitted with the initial architectural plan submittal, but it may be submitted up to sixty (60) days after the initial submittal. (Minimum scale of 1/8" = 1'-0".)

10. Design Review Fees. An initial design review fee of Four Hundred Fifty and No/100 Dollars (\$450.00) shall be paid to the Architectural Reviewer at the close of escrow for such Lot. If, for any reason, the initial design review fee was not paid at close of escrow for such Lot, then the applicable fee shall be paid at the time of the submittal. All design review fees are immediately non-refundable.

An additional design review fee of Two Hundred and No/100 Dollars (\$200.00) shall be due with the following types of submittals and payable to Declarant:

- (i) A Substantially Revised Submittal as defined in *Section II.D* below, and
- (ii) An Alteration Submittal to alter a previously approved submittal as described in *Section II.E* below.

All owner submittals and inquiries shall be made to the community manager.

All initial builder submittals and inquiries shall be made to:

The Quarry East Architectural Reviewer

c/o _____

EMAIL

PHONE

An online repository may be available for digital submittal uploads. Please inquire with the Architectural Reviewer for additional information if desired.

B. Notification of Action. The Owner or Builder shall be notified of the decision of the Architectural Reviewer within ten (10) business days of receipt of a complete submittal. The Architectural Reviewer may request additional information or materials and a submittal will not be considered complete until all requested items have been received. Revisions required by the Architectural Reviewer must be resubmitted and reviewed by the Architectural Reviewer prior to issuance of a formal approval unless the revisions are noted as a condition of approval on the Architectural Reviewer approval letter. Should notification of the Architectural Reviewer's decision not be received within ten (10) business days, it does not mean, nor may be construed to mean, the submittal was approved.

C. Expiration of Approval. The approval of the Architectural Reviewer shall be valid for a period of one (1) year at which time it shall expire.

D. Resubmittals and Substantially Revised Submittals. The Architectural Reviewer will review one (1) resubmittal from an Owner or Builder, if necessary, following the first formal review and response. After the resubmittal has been reviewed and the Builder or Owner has been notified of the decision regarding such resubmittal, the Architectural Reviewer's approval or denial shall be considered final.

If a submittal and subsequent resubmittal are both denied, the Architectural Reviewer will not perform any additional reviews unless the proposed Improvements are substantially revised. Provided the proposed Improvements are substantially revised, the revised designs shall be submitted for review as a "**Substantially Revised Submittal**"

and will be subject to an additional review fee as outlined in *Section II.A.9* above. Any Substantially Revised Submittal must include all applicable plans, specifications and submittal requirements set forth herein.

E. Alterations to an Approved Submittal. Proposed alterations or modifications to a previously approved submittal must be submitted as an “**Alteration Submittal**” to the Architectural Reviewer, and written approval from the Architectural Reviewer must be received prior to starting such work. Alteration Submittals must include the applicable plans, specifications and submittal requirements set forth herein and shall clearly identify those elements proposed to be altered or modified. Each Alteration Submittal will be subject to an additional review fee described in *Section II.A.9* above.

F. Construction Inspections. The Architectural Reviewer may make a physical on-site inspection during or at the completion of construction to verify compliance with the approved submittal.

G. Enforcement. If the Architectural Reviewer finds that the Improvements were not done in substantial compliance with the approved submittal corrective measures shall be taken as outlined in the Declaration, including, but not limited to, stopping construction and making physical changes to bring the Improvements into compliance with the approved plans.

III. DESIGN GUIDELINES

A. Lot Types. For the purposes of design review, Lots are divided into the following types:

- (i) “**Cluster Lot**”; Lots with a shared drive in areas designated as a cluster subdivision (typically 55’ or 65’ wide);
- (ii) “**Duplex Lot**”; Typically forty feet (40’) wide at the front setback with one shared wall;
- (iii) “**Small Lot**”: Less than sixty feet (60’) wide at the front building setback;
- (iv) “**Medium Lot**”: Greater than or equal to sixty feet (60’) wide but less than seventy feet (70’) wide at the front setback;
- (v) “**Large Lot**”: Greater than or equal to seventy feet (70’) wide at the front building setback; and

B. Street Side Setbacks. If there is additional width on a Lot with a side yard adjacent to a street, including where there is a common lot between the street and Lot, the residence shall have the building set back from such property line more than the minimum five foot

(5') setback and such area should include additional landscape where adequate space exists.

Residential Setbacks.

Duplex Lots		
	Front	10 feet to living or side load garage, 20 feet to garage door
	Rear	10 feet
	Side	5 feet
	Street Side	12 feet (if no common lot on side)
Cluster Lot Setback (4 lot cluster on shared driveway)		
	Front (to street of front lot)	10 feet
	Rear (rear of back lot)	10 feet
	Sides & Interior	5 feet
	To Common Drive	3 feet
Single Family		
	Front	10 feet to living or side load garage, 20 feet to garage door
	Rear	20 feet
	Side	5 feet
	Street Side	12 feet

Street Side Setbacks with no Common Lot. If there is a Lot with a side yard facing a street, where there is not a common lot between the street/sidewalk and Lot, the residence should be aligned along the opposite side yard setback and any fence adjacent to the street side is to be aligned 5' from the side of the home. The area between the fence and the sidewalk shall be a minimum of 7' for a total minimum 12' side yard setback and is to include additional landscape, including trees with a maximum spacing of 45', where adequate space exists.

C. Driveways. The builder will be responsible for installing a concrete driveway for any Cluster, Duplex, Small, or Large Lot. The builder will be responsible for installing a shared concrete driveway per the attached detail on **Exhibit** for the applicable Cluster Lots.

Unless otherwise approved, driveways may not extend past the edges of the garage to which they lead to allow room for more landscaping. Building lots are limited to one driveway cut at the street unless otherwise approved by the Architectural Reviewer.

D. Minimum Square Feet. Two-story homes shall have a minimum of 60% of the square footage, including garage, and excluding basements, storage rooms, covered patios, etc. on the first floor.

Finished space is exclusive of basements, garages, storage rooms, covered patios, etc. and each residence shall include a minimum amount of finished square feet based on Lot type, as set forth below.

Minimum Finished Square Feet of Residence					
	Cluster Lot	Duplex Lot	Small Lot (<60')	Medium Lot (≥60' & <70')	Large Lot (≥70'+)
One-Story Plan	1,200	1,200	1,300	1,600	2,200
Two-Story Plan	1,600	1,600	1,800	2,000	2,600

The Architectural Reviewer may require additional square feet of finish space depending on compatibility with existing homes or otherwise at its discretion.

E. Exterior Elevations.

The use of honest architectural elements relative to each architectural style is critical. Elements, such as boxed out windows, dormer windows and covered entries, are encouraged. Double gables over the entire width of a three (3) car garage are discouraged. Stacked rooms over garages shall incorporate a change in the front plane of the garage to avoid large, unbroken vertical surfaces. Where siding is used, batten boards or trim shall be located as inconspicuously and symmetrically as possible. Windows shall be consistent in type, style, trim and proportion.

Lots with rear or side elevations that face a street or Common Area are required to provide a higher level of articulation and detailing, including vertical and horizontal offsets along with breaks in the roof plane. The side of a residence on a Cluster Lot adjacent to or facing the street, highly visible common area, or open space should feature detailing, articulation (e.g. jogs in the architecture), and an overall design and appearance equivalent to a typical "front" elevation. Large expanses of flat, unbroken surfaces are not permitted on elevations facing a street or Common Area. Intermediate horizontal bands are encouraged at all two-story elements. Covered patios and balconies, horizontal offsets, overhangs and other methods to create visual depth and contrast are highly encouraged. Rear elevations that face streets or Common Areas must include covered patios.

Any "standard" (non-custom) floorplan approved for repetitive use should include multiple elevation styles that are substantially different. Similar architectural styles proposed for more than one floorplan shall have substantially different elevations. Elevation styles may be repeated within Quarry East provided a minimum of four (4) Lots with completely different floorplans and/or elevation styles separate the similar layouts

and the color schemes are different. However, the Architectural Reviewer may require some additional differentiation between any like floorplans and elevations. The requirements set forth in this paragraph are intended to mitigate undesirable monotony in the community design and finishes. Therefore, a Builder submitting for standard plan review should consider the total quantity of floorplans, elevation styles, and color/material schemes necessary to meet these requirements.

Broken roof lines and no less than three (3) primary roof masses are required. Unless otherwise approved by the Architectural Reviewer as compatible with a particular architectural design or style, the minimum pitch for roofs, excluding roofs at porches and deck covers, shall be 4:12. Steeper front-to-back roof pitches may be required on shorter roof spans if needed to provide greater street presence. Mixing or differing roof pitches on the same elevation is discouraged. Roof vents and other ventilation pipes shall be located on the rear elevation except where impractical, shall be painted to match or blend with the roof color, and shall otherwise be installed in an inconspicuous location and manner. Roof and attic vents shall be shown on the elevations.

Roof eaves are required to be a minimum of 16" deep, and gable ends are required to be a minimum of 12" deep.

Low-profile roofs with predominant pitch of 4/12 or 5/12 must be consistent with a specific architectural style and must have significant architectural details, such as:

- (i) Heavier Stacked Fascia—minimum 12"; and/or
- (ii) Corbels, exposed rafters or other details that are true to this style

Architectural detailing should be consistent with the architectural style and avoid the appearance of an over or under worked design.

Transitional 2-story or 1-1/2 story residences may be located on corner Lots provided that the single-story portion of the residence is located adjacent to the corner or side street and is approved by the Architectural Reviewer. Two-story residences are prohibited on Lots backing up to or with a side facing a main arterial street, clubhouse, or open space unless the elevation has a single story or stepped appearance.

F. Exterior Finishes and Colors. Exterior finishes should be cohesive with the architecture styles. Exterior finish materials and colors shall be consistent and continuous on all elevations of a dwelling to achieve a uniform and complete design. Hardy / Tru Lap / Louisiana Pacific or equivalent, stucco, local stone and brick are preferred exterior materials. The Architectural Reviewer will also consider high quality manufactured stone.

Architectural and aesthetic balance shall be primary concern in determining placement of materials. All materials will be required to terminate at an inside corner whenever possible or wrap the corners a minimum of thirty-six inches (36"). Materials

may be required to extend further in cases where there is a more logical terminus point. Designs are encouraged to use materials to define building forms.

1. Stone and Brick. Subject to compatibility with the overall architectural style and design, all residences, except as noted below, shall be required to incorporate brick or stone. A minimum of twenty percent (20%) of elevations that face a street(s) will be required to be stone or brick. The requirement for the use of such accent materials on exterior elevations may be waived by the Architectural Reviewer for exceptional design concepts that establish a very high quality through other elements of architectural design. The Architectural Reviewer may require upgraded siding, additional landscaping, or other elements at its discretion.

Materials shall be compatible with the exterior paint colors approved by the Architectural Reviewer. Darker brick shades are encouraged. White or gray brick will be considered when consistent with the architectural style.

2. Siding & Stucco. Proposed siding shall be eight inches (8") hardy / cement, cedar shake or board and batten. Cottage lap siding is not permitted; other types of siding not contemplated herein are subject to written approval by the Architectural Reviewer. Steel, aluminum, or vinyl siding are prohibited. If siding is used on an elevation it shall be used on all elevations.

Stucco exteriors shall incorporate appropriate articulation and accents, such as one and on-half inch (1-1/2") deep by six inch (6") wide trim around windows, twelve inches (12") intermediate horizontal bands at 2-story elements, belly bands, frieze boards, etc. If stucco is used on an elevation it shall be used on all elevations.

3. Exterior Paint Colors. Exterior wall colors and trim colors must be selected for their harmony with each other and the overall aesthetic goals of the community. Rain gutters and downspouts shall match the color of the surface to which they are attached.

Exterior colors of earth tones, warm tones or grays shall be required for the body of the residence. Bright, bold or very dark colors shall be reviewed on a case by case basis by the Architectural Reviewer.

4. Roof Materials. Roofs shall be thirty (30) year or better architectural asphalt shingles, slate, concrete tile, or clay tile and must have high-definition ridge caps. Roof colors must be black or dark grey unless otherwise approved by the Architectural Reviewer. Other roofing materials are subject to written approval by the Architectural Reviewer.

G. Solar Panels. Solar panel locations must be approved by Architectural Reviewer prior to installation. Panels shall be commercially manufactured and well maintained.

Solar panels shall not be visible from the front elevation of the home if possible. Solar panels and associated hardware shall be an integral part of the design of the home. The color of solar panels, frames, and associated hardware shall be black and shall be recess mounted (flush) into the roof structure or low profile and parallel to the roof with no visible piping. If solar panels are proposed, the roofing material shall be of a hue that minimizes the contrast between the roof and the solar panels.

H. Rain Gutters and Downspouts. Rain gutters and downspouts are required on all residences, shall be continuous, and shall be painted to match the color of the surface to which they are attached. The use of chains or copper gutters and downspouts will be considered when appropriate with the architectural style.

I. Fascia & Trim. Roof fascia shall be a minimum of eight inches (8") wide. Certain architectural styles may require wider fascia. Gables shall incorporate stacked trim detailing. Fascia and trim shall be constructed of nominal one inch (1") thick material. Stacking is not required on eaves where gutter covers the additional fascia.

J. Architectural Detail/Accents. Focus should be placed on architectural design that is true to the character or style submitted. Details and accents should represent the historical integrity of each style and should be consistent with the style on all elevations. Over-worked or unrelated detailing is prohibited.

K. Chimneys. Chimneys may be restricted for size and location. All full height chimneys shall have an architectural metal chimney cap that fully encloses the chimney pipes and painted pursuant to the written approval of the Architectural Reviewer.

L. Garages, General. Designs should de-emphasize the garage, and the main living areas of a residence should be visually dominant. Accordingly, builders are encouraged to locate front-loaded garages behind the front of the rest of the home, e.g. livable finished area, side-load garages, and/or front patios. Side load garages are encouraged, and when possible, designs should avoid placing garage doors where they are visible from the street. Side loaded garages are highly encouraged, but not required, on Lots that are deeper than one hundred thirty feet (130'). All garage doors are subject to written approval of the Architectural Reviewer.

All garage doors, including RV or oversize garage doors, should be recessed a minimum of six inches (6"). When feasible, a minimum garage door recess of twelve inches (12") is recommended. Flush or flat panel garage doors are not allowed. Garage doors are to have detailing that is consistent with the architectural style and design of the residence. Interiors of garages shall be sheet rocked, taped, sanded and painted or sheet rocked, taped sanded and textured. Trim around doors and windows shall be painted.

If a front-load garage extends in front of the rest of the home, enhanced architecture will be required. If front load garage(s) occupy more than 50% of the width of the front elevation, upgraded garage doors may be required. Taller garages will require a greater setback from the other garages as described below.

Garages on Duplex, Small or Cluster Lots shall be limited to a single two-car garage door or two single-car garage doors. Small, Cluster, and Medium Lots are limited to a maximum garage door height of eight feet (8'). Garages on Medium Lots and Large Lots may have a single two-car garage door plus a single one-car garage door, or three one-car garage doors. If a home features three garage bays, the front of at least one of the garage bays should be set back a minimum of eighteen inches (18") from the face of the other garage(s). If an RV garage is proposed on a Large Lot, typically, only a single additional, one-car garage door will be permitted. The Architectural Reviewer may elect to allow a front facing garage if it is set back behind the home and another side loaded garage.

Detached garages are prohibited.

Garage Doors by Lot Type			
	Cluster, Duplex & Small Lot	Medium Lot	Large Lot
Max Number of Garage Doors	(1) 2-car garage door, or (2) 1-car garage doors	(1) 2-car garage door and (1) 1- car garage door, or (3) 1- car garage doors	(1) 2-car garage door and (1) 1- car garage door, or (3) 1-car garage doors
Max RV/Tall Garage Door Height	8'	10'	12'

M. RV Garages & Tall Garage Doors. Although taller garage doors and RV garages may be considered on certain Lots (or Lot types as described herein), this does not guarantee approval. Plans submitted with RV garages will be subject to a more detailed review process and may be denied by the Architectural Reviewer in its sole discretion.

The Architectural Reviewer will allow and evaluate proposed architectural RV or oversize garage doors on specific Lots, subject to any restrictions and the below guidelines. The Architectural Reviewer's review of any proposed RV or oversize garages will be subjective and in the sole discretion of the Architectural Reviewer.

1. The Architectural Reviewer will typically pre-designate specific Lots that may feature an oversized or RV garage door as well as the maximum height of such door. If there is any question relating to RV or oversize garage doors on a specific Lot, and/or the maximum oversize/RV garage door height permitted, please contact the Architectural Reviewer.
2. Except as may be otherwise specified by the Architectural Reviewer, Large Lot types that are less than seventy-five feet (75') in width at the front setback may

have a single one-car garage door higher than eight feet (8') up to a maximum height of twelve feet (12'), subject to the standards set forth below.

3. Except as may be otherwise specified by the Architectural Reviewer, one-story homes with garage doors greater than twelve feet (12') in height and up to a maximum height of fourteen feet (14') will only be considered on Large Lot types that are seventy-five feet (75') or wider at the front setback.

4. For an RV or oversize garage to be considered, it must meet the following minimum setbacks from the front of any other garage on the Lot:

(i) Three feet (3') for doors taller than eight feet (8') and up to ten-feet (10') in height,

(ii) Five feet (5') for doors taller than ten feet (10').

5. A garage door should not be one of the most prominent features on the façade. The intent is to minimize the impact as much as possible by keeping the taller garage door behind both the front of the residence and the eight-foot (8') garage door. If the front of an RV garage or taller garage extends in front of the finished livable area, enhanced architecture will be required.

6. Turning the other garages into side-load garages are encouraged.

7. Incorporating an architectural pop-out with a lower roof line on the side of a tall garage is strongly encouraged on 12' doors and required on 14' doors and may be required depending on the particular site and proposed location. Tall garages should incorporate other architectural elements to provide visual interest, such as windows, banding at the level of the roof line, etc.

8. Upgraded, architectural garage doors are encouraged.

9. The overall mass and roof shall be integrated with and in harmony with the rest of the residence. The top of the RV garage should not exceed the roof height of the residence.

10. RV and oversized garage spacing will be designated by the Architectural Reviewer. Notwithstanding any designation the Architectural Reviewer may make to the contrary, Lots with RV garages will typically be required to be separated by a minimum of three (3) Lots on the same side of the street and a minimum of two (2) Lots to each side of an opposite Lot, where applicable.

11. If an RV garage is proposed on an end Lot condition adjacent to Common Area and/or streets, the RV garage shall be located on the side of the Lot opposite such adjacency.

N. **Detached Storage Structures.** All vehicles, trailers, tools, and equipment shall be stored out of view in enclosed structures. A maximum of one (1) detached storage facility per Lot will be considered, such as a storage shed, provided it is of the same construction, finish, and color as proposed and approved for the residence. Metal storage sheds or other structures that do not reflect the architectural style and quality of the residence on such a Lot are prohibited.

Such a detached storage facility may not exceed two-hundred (200) square feet in area, and detached structures may not be located within three feet (3') of the property line on a Lot. Any such structure shall be placed on a concrete pad in a location approved by the Architectural Reviewer and may not back to a neighboring view fence. Depending on location, additional landscaping may also be required.

The Architectural Reviewer encourages the storage of boats, RV's, camp trailers and other similar vehicles or trailers in offsite storage facilities.

O. **Fences.** Prior to the construction of any fence, plans shall be submitted as part of the landscape plans to and approved in writing by the Architectural Reviewer. The submittal shall include a site plan showing the location of fencing proposed, including setback dimensions, and designate the type and height of fence proposed. The only fence types permitted are those specified by these Design Guidelines, and no fence may be installed prior to approval by the Architectural Reviewer. Refer to Exhibit A through Exhibit C attached to these Design Guidelines for the permitted fencing types for most Builder and Owner installations. The developer may install an alternate style screen fencing shown in Exhibit D in select locations, and where such exists, it may not be replaced with any other type of fencing, including a Builder/Owner style screen fence.

1. General fence requirements include:

- (i) Wherever possible, adjoining Lots shall use common corner posts.
- (ii) Fence returns terminating into the side of a house shall be a minimum of four feet (4') behind the front of the residence on the side where the return is located.
- (iii) Transitions in fence height shall be accomplished by stepping, (not angling), the fence top.
- (iv) Where view fence is used along rear lot lines adjacent to Common Areas, view fence should also be used for a minimum of two fence panels on the adjacent side yard fence.

2. Permitted locations for each fence type are as follows:

- (i) All Lots located adjacent to club, Common Area, and/or open space, including water features, hillsides, etc. shall install view fencing along such rear or side property line(s).
- (ii) All Lots located adjacent to any Common Area or streets shall install screen fencing or demi-screen fencing along such adjacent boundary line(s).
- (iii) Screen fence may be installed on or alongside property lines between residential Lots, except where view fence is required as described above in *Section III.O.1(iv)*.
- (iv) Where view fence is opted to be installed along a rear boundary of a Lake Front Lot, the view fence shall be located at the top of the slope leading down to the water, as applicable, and should include a gate to accommodate maintenance access.

Refer to Exhibit E for a depiction of the Fence Plan by Phase. Depending on location, the Architectural Reviewer may require an on-site inspection prior to fence construction.

P. Dog Runs. Dog runs must be approved by the Architectural Reviewer prior to installation. The size and location may be restricted. If approved, they shall be commercially manufactured and well maintained. Coated chain-link, galvanized and stainless steel are the only approved materials unless constructed of one of the permitted fence styles.

Q. Patios. All residences are recommended to include covered rear patios. All exterior patios are required to be a minimum of ten feet (10') deep and shall be covered. Covered patios may be a minimum of eight feet (8') deep (or equivalent alternate dimension) where Lot depth is a constraint. Patio covers shall extend to the edges of the patio and be integral to the design and appear as extensions of the architecture of the residence. Patios and covers on corners shall extend past and wrap the corner of the building. All columns must be a minimum size of twelve inches by twelve inches (12" x 12").

R. Landscaping of Small, Medium, and Large Lots. A landscape plan shall be prepared and submitted to the Architectural Reviewer for approval. Although certain minimum standards have been established, additional landscaping is encouraged and may be required by the Architectural Reviewer.

Builders and Owners are encouraged to consider adjacent yards when formulating a landscape plan and to the extent practical, shall blend the Improvements with the neighboring yard landscape Improvements, including planters and berms. The Architectural Reviewer will consider how the proposed landscaping blends with and promoted the overall aesthetics of the site in conjunction with structures. The use of berms

and clustered planting groups such as garden beds with trees, shrubs and flowers are encouraged.

Exterior mounted utility meters, heat pumps, air conditioners, and other such equipment shall be properly screened from view of the street and surrounding homes by landscaping or fencing.

Landscaping in front, rear, and side yards is required to be completed in accordance with these standards within 30 days of substantial completion of the home, except in the event weather makes installation unfeasible, in which case written waiver from the Architectural Reviewer is required.

1. Trees shall be planted in front yards, corner yards and rear yards according to the following minimum standards:

(i) Deciduous trees shall be 2" caliper or larger and evergreen trees shall be 8' high or higher

(ii) An ornamental tree or 5 additional 2-gallon shrubs may be substituted for one of the trees required in the front yard if approved by the Architectural Reviewer.

(iii) A Lot on which the residence is required to have an increased side setback as described in *Section III.B* shall have additional trees planted within such area along the property line provided the width of the side yard is a minimum of ten (10) feet, and such trees shall have a maximum spacing of forty (40) feet.

MINIMUM TREE QUANTITIES			
	Small Lot ($<60'$)	Medium Lot ($\geq 60'$ & $<70'$)	Large Lot ($\geq 70'$ & $<90'$)
Front Yard	2 trees, one deciduous tree and one evergreen tree.	2 trees, at least one deciduous tree and one evergreen tree	3 trees, at least one deciduous tree and one evergreen tree
Rear Yard	1 tree per 1500 square feet of rear yard	1 tree per 1500 square feet of rear yard	1 tree per 1500 square feet of rear yard

2. Shrubs shall be planted in the front yards, corner yards and rear yards according to the following minimum standards:

MINIMUM SHRUB QUANTITIES			
	Small Lot ($<60'$)	Medium Lot ($\geq 60'$ & $<70'$)	Large Lot ($\geq 70'$ & $<90'$)

Front Yard	Minimum 16-20, 2 gallon or larger shrubs	Minimum 18-22, 2 gallon or larger shrubs	Minimum 20-24, 2 gallon or larger shrubs
Corner Lot Side Yard	Minimum 10, 2 gallon or larger shrubs	Minimum 12, 2 gallon or larger shrubs	Minimum 14, 2 gallon or larger shrubs
Rear Yard	Minimum 8, 2 gallon or larger shrubs	Minimum 9, 2 gallon or larger shrubs	Minimum 10, 2 gallon or larger shrubs

3. Planter beds planted with shrubs and flowers shall cover a minimum of 25% of the front yard, 20% of the side yard on corner lots, and 15% of the rear yard.
4. Except at planter bed locations or side yards, sod shall be laid throughout (excluding driveways, walks, and patios).
5. Sod or turf grass shall be planted covering a minimum of 50% of the front yard and 50% of the rear yard. Artificial turf is prohibited in front yards.
6. Ground cover used in planter beds shall either be a natural wood or soil product with no dyes or color added, or a grey / dark colored perma bark/rock chips no larger than 1 inch in size. There shall be no red, white, or light-colored rock chips.
7. An automatic underground irrigation system shall be installed throughout. Such irrigation system shall include appropriate backflow prevention and utilize zones for landscape with different water demands. Sprinkler systems shall be designed to minimize overspray and are encouraged to incorporate low trajectory spray nozzles.
8. Builders are responsible for installing irrigation and landscaping in the planter strips located between sidewalks and curbs adjacent to their lot if applicable. One street tree shall be planted on each property line within the planter strip, except in restricted areas. Refer to Exhibit G for a depiction of those Lots where trees are prohibited within the planter strip. Street trees should be selected from the Treasure Valley Tree Selection Guide as appropriate for the proposed location. Trees in the planter strip are in addition to minimum standards. Turf grass shall be planted in the planter strip except where the driveway and street trees are located.
9. Landscape grading shall be designed to produce a graceful contouring; harsh geometric shapes and slopes will not be approved. Mounding or contouring used to add interest to the landscaping must be an appropriate scale in relation to the rest of the yard and be sufficiently compacted and covered to prevent erosion. Any imported soil shall be free of weeds and debris and equal to the quality of the

existing soil on the Lot. Driveway site coverage shall be minimized to provide additional area for landscaping. Gravel pads for parking of vehicles, trailers, etc. are not acceptable.

10. If a raised patio is proposed as part of the landscape, it must be a minimum of three (3) feet from any property line, and the height may not exceed twenty-four (24) inches in order to be considered.

11. Lots backing to a lake edge must also comply with the standards set forth in Section III.T below.

12. In no event shall the landscape and/or grading alter the Lot drainage as approved by the City of Middleton, Idaho. All drainage water must be retained on the Lot or directed off the Lot in accordance with the drainage details attached as Exhibit F; no Lot drainage may be permitted to flow onto or across an adjacent Lot.

Variances to the landscape requirements set forth herein may be granted in cases such as flag lots or pie shaped lots with narrow street frontages. Extensions for completion of landscaping may be granted when weather conditions hamper landscape construction from December 1 through April 1.

S. Landscaping of Duplex & Cluster Lots. A cohesive landscape plan shall be developed and submitted to the Architectural Reviewer for each group of Duplex or Cluster Lots that is accessible from a single shared drive or adjacent drives. Although certain minimum standards have been established, additional landscaping is encouraged and may be required by the Architectural Reviewer. The Architectural Reviewer will consider how the proposed landscaping blends with and promoted the overall aesthetics of the site in conjunction with structures. The use of berms and clustered planting groups such as garden beds with trees, shrubs and flowers are encouraged.

Builders and Owners are encouraged to consider adjacent yards when formulating a landscape plan and to the extent practical, shall blend the Improvements with the neighboring yard landscape Improvements, including planters and berms. Exterior mounted utility meters, heat pumps, air conditioners, and other such equipment shall be properly screened from view of the street and surrounding homes by landscaping or fencing.

Landscaping in front, rear, and side yards of each Duplex or Cluster Lot shall be completed to these minimum standards within thirty (30) days of substantial completion of the residence on such lot, except in the event weather makes installation unfeasible, in which case written waiver from the Architectural Reviewer is required.

1. Trees and shrubs shall be planted in front yards, corner yards and rear yards according to the minimum standards described below.

- (i) Deciduous trees shall be 2" caliper or larger and evergreen trees shall be 8' high or higher
- (ii) An ornamental tree or 5 additional 2-gallon shrubs may be substituted for one of the trees required in the front yard if approved by the Architectural Reviewer.
- (iii) A Lot on which the residence is required to have an increased side setback as described in *Section III.B* shall have additional trees planted within such area along the property line provided the width of the side yard is a minimum of ten (10) feet, and such trees shall have a maximum spacing of forty (40) feet.

DUPLEX & CLUSTER LOT MINIMUM TREE AND SHRUB QUANTITIES		
	Trees	Shrubs
Front Yard	2 trees, one deciduous tree and one evergreen tree.	Minimum 16-20, 2 gallon or larger shrubs
Corner Lot Side Yard	See <i>Section III.S.1(iii)</i> above	Minimum 10, 2 gallon or larger shrubs
Rear Yard	1 tree per 1500 square feet of rear yard	Minimum 8, 2 gallon or larger shrubs

- 2. Planter beds planted with shrubs and flowers shall cover a minimum of 25% of the front yard, 20% of the side yard on corner lots, and 15% of the rear yard.
- 3. Except at planter bed locations or side yards, sod shall be laid throughout (excluding driveways, walks, and patios).
- 4. Sod or turf grass shall be planted covering a minimum of 50% of the front yard and 50% of the rear yard. Artificial turf is prohibited in front yards, but would be considered in rear yards.
- 5. Ground cover on planter beds shall be rock chips no larger than 1 inch in size and match or be substantially similar in appearance to the boulders reflected in Exhibit H.
- 6. An automatic underground irrigation system shall be installed throughout. Such irrigation system shall include appropriate backflow prevention and utilize zones for landscape with different water demands. Sprinkler systems shall be designed to minimize overspray and are encouraged to incorporate low trajectory spray nozzles.

7. Builders are responsible for installing irrigation and landscaping in the planter strips located between sidewalks and curbs adjacent to their Lot if applicable. One street tree shall be planted every thirty-five (35) feet within the planter strip along each property line or right-of-way that borders such Lot as applicable. For street tree specifications, contact the design review committee. Trees in the planter strip are in addition to minimum standards. Turf grass shall be planted in the planter strip except where the driveway and street trees are located.

8. Landscape grading shall be designed to produce a graceful contouring; harsh geometric shapes and slopes will not be approved. Mounding or contouring used to add interest to the landscaping must be an appropriate scale in relation to the rest of the yard and be sufficiently compacted and covered to prevent erosion. Any imported soil shall be free of weeds and debris and equal to the quality of the existing soil on the Lot. Driveway site coverage shall be minimized to provide additional area for landscaping. Gravel pads for parking of vehicles, trailers, etc. are not acceptable.

9. If a raised patio is proposed as part of the landscape, it must be a minimum of three (3) feet from any property line, and the height may not exceed twenty-four (24) inches in order to be considered.

10. In no event shall the landscape and/or grading of a Lot alter the Lot drainage as approved by the City of Middleton, Idaho. All drainage water must be retained on the Lot or directed off the Lot in accordance with the approved grading and drainage plans; no Lot drainage may be permitted to flow onto or across any adjacent Lot.

Variances to the landscape requirements above may be granted in cases such as flag lots or pie shaped lots with narrow street frontages. Extensions for completion of landscaping may be granted when weather conditions hamper landscape construction from December 1 through April 1.

T. Lake Front Lots. The rear or side property line of a Lot may be located along a body of water or “lake”, and where there is no landscaped common area between the Lot and water’s edge, such a Lot may be specifically referred to herein as a “**Lake Front Lot**”. (A Lot with landscaped common area between the Lot and a body of water, would not be considered a Lake Front Lot as it pertains to these Design Guidelines.)

As the Property is developed, certain Lake Front Lots may be permitted entry into a lake directly from the Lot, subject to any applicable rules and restrictions. Refer to Exhibit I identifying by phase any Lake Front Lots where direct entry into a lake is permitted.

A grading plan must be included with (a) the architectural submittal and (b) the landscape submittal for each Lake Front Lot when grade changes are proposed or if requested by the Architectural Reviewer. The Architectural Reviewer may require a

grading plan be provided with a submittal for an alteration, reconstruction, modification, and/or addition to the rear yard Improvements, in its discretion.

The overall design of a rear yard should provide a gracious transition between the residence and the water's edge. Refer to [Exhibit J](#) for illustrations of lake edge Improvements and refer to [Exhibit K](#) for illustrated approximate high and low water levels.

The standards described below will apply to proposed Improvements near or along the water's edge on a Lake Front Lot.

1. [Retaining Walls](#). Local, surface select boulders are the preferred material for retaining walls; poured in place concrete will also be considered. Block retaining walls are discouraged but may be approved provided such are designed to be an extension of the residential architecture on the Lot.
2. [Beach Areas](#). A recessed sand "beach front" may be constructed on a Lot, provided it is substantially level and does not exceed 40% of the Lot width at the rear property line. Beach front sand should be natural yellow, tan, or white hues; specialty colors, including black, will not be approved. The sand "beach front" should be setback from the side property line a minimum of five (5) feet.
3. [Docks](#). If direct entry from a Lot into a lake is permitted per [Exhibit I](#), a dock may be considered for such a Lake Front Lot provided the Lot is greater than one-hundred feet (100') wide at the rear property line meeting the water's edge. The Architectural Reviewer may adopt a standard style, color, size, or other dock specification(s). A dock may be constructed of wood or composite wood, such as Trex decking or similar, and should be a natural wood color or other earth tone. White, bright, or very light colors will not be approved.
4. [Diving Boards & Slides](#). Diving boards and/or slides leading into the lake are not permitted.
5. [Patios](#). A patio located near the water's edge is to be harmonious with the lake edge and will be subject to a higher level of scrutiny. Covered structures must be kept within the residential setbacks unless otherwise approved.
6. [Water Levels](#). Water levels are expected to regularly fluctuate by several feet and any vertical portion of a proposed Improvement in or along the water should appear consistent and cohesive both when the water level is high as well as when it is low. Refer to [Exhibit K](#) for illustrated approximate high and low water levels.
7. [Setbacks and Width](#). Patios, retaining walls, docks, and other Improvements, excluding landscape, along the water's edge will typically be required to be setback at least five (5) feet from the nearest side property line and be limited to no more than 40% of the Lot width at the rear property line.

8. Shore Rock. The landscape for each Lake Front Lot shall include local river cobble installed along the water's edge, in a 3" to 8" size blend, available from Premier Aggregates in Emmett, ID (208-901-8189). Shoreline cobble must be the specified size range, and a single size or alternate size/range is not permitted. Boulders along the shore and slope are to match the color/appearance of the river cobble.

9. Shore Planter Bed Cover. Ground cover in planter beds along the rear of the Lot is to match the color of the ground cover used in planter areas elsewhere on the Lot and shall be rock chips (perma bark) between 1" and 1-1/2" in size. Bark chips may not be used in planter beds located along the back of Lot and shore.

10. Grasses. Grasses may be planted from the top of the slope down to the edge of the water, which is subject to seasonal fluctuation. Due to the grade of the slope, fescue is recommended.

11. Boulder Retaining Walls. Owners are encouraged to install boulder retaining walls to create raised planter beds along the water behind such Owner's Lake Front Lot. In addition, the developer may have installed or may install boulder retaining walls for raised planter beds along the water behind some, but not all, Lake Front Lots. Where there is a raised boulder planter bed behind a Lake Front Lot, the initial landscape for such Lot must include the planter area behind such Lot to meet the following minimum standards, which are in addition to any other minimum landscape requirements on the Lot:

(iv) At least one tree is to be planted meeting or exceeding the minimum tree size for the type (evergreen or deciduous) as specified in *Section III.R*;

(v) Plant at least one (1) shrub per one-hundred (100) square feet of planter area or a minimum of five (5) shrubs, whichever is greater, and such shrubs shall be two-gallon size or larger;

(vi) Unplanted areas are to be covered with ground cover as described in *Section III.T.9* above.

12. Appearance of Lake Edge. Each Owner of a Lake Front Lot is responsible for maintaining the water's edge adjacent to or along the Lot in a sightly manner, removing any trash or debris, weed abatement, and replacing rock as necessary.

Direct lake entry will not be allowed from all Lake Front Lots, and where permissible, the accessibility to a lake from a Lot may vary throughout a year and/or change over time. Not all Lake Front Lots will have water's edge along the entirety of the adjacent Lot boundary. If there is any dispute as to whether a Lot is considered a Lake Front Lot or whether direct entry to a lake from a Lot may be permitted, the Architectural Reviewer's decision shall be final, conclusive, and binding.

U. **Lakes, Generally.** Each Lot Owner must follow all applicable rules and standards set forth by the Association for each individual lake, without exception. Physical entry and/or recreational activities in or on the water, (e.g. swimming, boating, rafting, paddle boarding, kitesurfing, jet-skiing) may be permitted on some, but not all, of the community lakes. Dumping of trash, debris, dirt (to include any spoils from regrading or excavation), or other materials into a body of water is strictly prohibited. Owners may not introduce or allow any foreign substance or additive, including fertilizers, that may alter the water quality to enter any body of water.

V. **Boulder Retaining Walls.** A well-designed natural boulder retaining wall brings a thoughtful, natural element to a landscape concept. Local, surface select boulders are the preferred material for retaining walls; poured in place concrete will also be considered. Block retaining walls are generally discouraged but may be approved in side yards, or provided such are designed to be an extension of the residential architecture on the Lot. If proposed, boulder retaining walls are to be comprised of surface select boulders to match the boulders shown in Exhibit H. Boulder retaining walls are to be substantially similar in design and appearance those seen in Exhibit H. General design principles for boulder retaining walls include:

1. Locate each rock to have a minimum of three bearing points, two in front and one in back;
2. Place each rock so it is in contact with at least two rocks below it;
3. Avoid continuous vertical seams or “columns” of rocks;
4. Avoid continuous horizontal planes in the rockery;
5. Use larger base rocks than used in the upper rocks;
6. Incline rocks back into the slope;
7. Use smaller rocks (“chink rocks”) to fill in voids; and
8. Select rocks free from obvious signs of distress, (e.g. fracturing, disintegrating).

Where boulders are used for slope stabilization, the boulders must be naturally spaced and installed so that the lower circumference is in contact with the grade. Additionally, plants that have a strong root system to help lock soil into place should be incorporated around and between boulders, such as approved vines, grasses, and annual flowering plants.

All retaining walls should be designed to accommodate proper lot drainage and control surface water as necessary. Each Owner is solely responsible for obtaining any engineering and/or governmental approvals that may be necessary.

W. Exterior Lighting. Exterior lighting that is visible from streets, neighboring lots or Common Areas must have a concealed light source (e.g. covered, shielded) and shall not illuminate neighboring properties. Exterior light fixtures may cast light upward only if they are located under a roof and the beam of light does not extend beyond such roof. Exterior lighting shall be harmonious with the building design. Individual lights are limited to 100 watts. All exterior lighting must be identified on the submittal and is subject to approval by the Architectural Reviewer.

Exterior security lighting will be permitted provided the following standards are met:

- (i) Security lighting shall be limited to lighting that is triggered by motion on such Lot and may not be triggered by motion on neighboring properties.
- (ii) Such lighting shall not be directed at any neighboring properties and shall be mounted no higher than ten feet (10') above ground.
- (iii) The motion detector shall be programmed to shut off the light(s) no longer than five (5) minutes after motion is detected and the light has been illuminated.

X. Mailboxes. Mailboxes shall be in cluster boxes provided by the HOA and may be subject to a mailbox fee.

Y. Miscellaneous Equipment and Signage.

1. **Satellite Dishes.** A satellite dish should be located in consideration of neighbors and potential views. Wherever possible, the satellite dish should be installed on a rear corner of the home, in a location selected to minimize visibility from common areas and streets. Satellite dishes are prohibited on the front of the home or within the front yard. If mounted on a building, the top of the satellite dish may not exceed the highest part of the nearest roof. In the absence of adequate reception in such a location described, the Owner may request approval for an alternate location. Other types of antennae are generally not permitted unless approved.

2. **Basketball Equipment.** Basketball backboards and hoops are not permitted on the roof or walls of the residence. Basketball hoops with glass or plexiglass backboards may be installed on fixed poles adjacent to a driveway but are encouraged to be installed in less prominent areas such as rear or side yards. Moveable basketball hoops are not permitted in front yards.

3. **Signs.** An Owner may not display signs to public view without advance approval from the Architectural Reviewer, except as described below.

- (i) One (1) commercially manufactured sign indicating the residence is for sale or lease may be placed on the applicable Lot without advance

approval from the Architectural Reviewer, provided the dimensions do not exceed three (3) feet by two (2) feet;

(ii) Up to a maximum of two (2) commercially manufactured signs in support of or in opposition to a candidate for public office or a ballot measure, provided the criteria below is met.

- a. The sign(s) may not exceed two (2) feet in height or width;
- b. Sign(s) may be displayed no earlier than twenty-one (21) days prior to the date voting takes place for the associated election or ballot measure;
- c. Any such sign displayed on an Owner's Lot must be applicable to the voting district in which the Lot is located; and
- d. The sign(s) must be removed within three (3) days after such election date.

Additional rules and standards related to signs are set forth in the Declaration.

IV. CONSTRUCTION STANDARDS

A. Condition of Lot. An Owner or Builder shall inspect the Lot prior to purchase and construction of Improvements for condition of all utilities, location of property pins, and general conditions and report any defects or damages to the developer. Unless otherwise notified, all Improvements shall be considered in good repair and all damages or deficiencies thereafter shall be the sole responsibility of the Owner or Builder.

The developer or its engineer shall relocate missing property pins, utility connections or sewer markers, that cannot be located by ordinary inspection (including light digging to uncover buried pins or markers) prior to closing or commencement of construction. Resetting property pins or locating pressure irrigation, utility services, or sewer markers after possession will carry a minimum charge of Two Hundred and No/100 Dollars (\$200.00) from the developer.

B. Excavation. Excavators are required to contact Dig Line at 208-342-1585 prior to commencing excavation on the Lot. Lots shall be excavated in a manner that will not adversely impact neighboring Lots. Excess dirt shall be removed from the community and may not be deposited or dumped on other Lots, Common Areas, any portion of the community or any other vacant ground slated for future development.

C. Elevation of Foundations. Unless otherwise approved, foundations shall be set a maximum of eighteen inches (18") to twenty-four inches (24") above the back of the curb

elevation. An Owner or Builder shall take all necessary steps in setting the foundation elevation to ensure that drainage onto neighboring properties will be eliminated. All drainage from a Lot will be retained on site or drained into the adjoining street. It is the responsibility of the Builder to ensure proper elevations as it relates to the base flood elevation and any code or requirement related to the elevation of a home, if applicable.

D. Timeline to Complete Construction. Upon commencement of construction, the construction of the Improvements shall be diligently pursued in accordance with the submittal approved by the Architectural Reviewer, including all conditions of approval. Construction shall be completed within one (1) year from date construction commences. The Architectural Reviewer may require a completion deposit and such funds may be used toward any construction related violations, including to return the Lot to its original condition as described below.

If construction is not completed within one (1) year from the date construction commences and an Owner or Builder is not diligently pursuing completion (with no activity on site for thirty (30) days), the Architectural Reviewer and/or _____ HOA shall have the option to require the Owner and/or the Builder to return the Lot to its original condition within twenty (20) days. If an Owner and/or Builder has not returned the Lot to its original condition within the timeframe required, the Architectural Reviewer and the _____ HOA has the right to immediately access the site and return the Lot to its original condition at the Owners and/or Builder's expense. Should any applicable completion deposit not be sufficient to cover the associated expense of such corrective action, any remaining amount due shall earn interest at the rate of eighteen percent (18%) per annum and may be assessed against the Owner and/or Builder as an Individual Assessment as contemplated by the Declaration.

E. Lot Drainage. The Owner and/or Builder are responsible for constructing and/or maintaining proper Lot grading and drainage in accordance with the grading and drainage plans approved by the City of Middleton, Idaho both during and after any construction, including, but not limited to, landscape. All water on a Lot shall be directed to the street and/or retained on such Lot, and no Lot is permitted to drain onto an adjacent Lot. The grading and drainage detail(s) as may be applicable are attached hereto as Exhibit F.

Modifications to the approved grading and drainage plans approved by the City of Middleton will require approval by the Architectural Reviewer prior to modifications taking place. Builder and/or Owner assumes responsibility for such modifications.

F. Construction Standards and Jobsite Maintenance. It is important to the community and the Declarant that the community experience is a positive one for all Builders, contractors, Owners and their guests. In order to facilitate such, any entity and/or individual performing work within the community shall comply with the guidelines set forth below.

1. Washout of concrete trucks and equipment will be performed outside of the subdivision or in a designated concrete washout area. Developer will determine a location and provide direction to the concrete washout area within the community. Contractors are required to utilize this area to clean concrete trucks, pumpers, or other concrete coated equipment if washed within the subdivision.
2. Construction shall not begin prior to 7:00 a.m. or continue after sunset; however, during the mid-summer months (June-August), contractors may begin as early as 6:00 a.m. as long as they are sensitive to Owners and Occupants and comply with all applicable jurisdictional and municipal requirements and laws.
3. Jobsites shall be tidied up and free of debris each evening and prior to each weekend. Each Builder shall provide a trash bin and/or dumpster at the jobsite. Jobsite trash or debris that may be scattered by wind shall be properly contained in trash bins and/or dumpsters or by other means. Builders and Owners who fail to maintain the Lot in an orderly manner or allow construction debris to clutter surrounding properties may be subject to appropriate action from the Architectural Reviewer or the _____ HOA.
4. Temporary toilets, construction equipment, and construction material shall be contained within the Lot boundaries.
5. All vehicles will be parked within the Lot boundaries or on public streets adjacent to the jobsite and shall not block traffic, mailboxes, or otherwise interfere with existing Owners or Occupants.
6. Dogs shall not be allowed at the construction site.
7. Inappropriate language, shouting, or other inappropriate behavior will not be allowed.
8. Radios or other music must be kept to a minimum volume and should not be audible outside of the Lot limits.
9. Power and water must not be used from existing residences without permission from the Owner.
10. All personnel shall comply with the maximum speed limit within the community. Speeding or unsafe driving will not be tolerated.
11. Streets shall remain free from dirt, gravel, or other excavation material and shall be maintained by the Owner or Builder in a clean and orderly manner. Washing of equipment or materials may not occur in the street or any Common Areas.

12. Materials or equipment may not be stored on any other Lot or Common Area. Access to a Lot across any other Lot or Common Areas is prohibited.

13. Contractors shall be fully clothed at all times, including, but not limited to, shirts and footwear. Notwithstanding the foregoing, attire shall be appropriate for the work being performed.

14. Appropriate safety equipment including, but not limited to, hard hats, safety eyewear, and high visibility clothing, shall be used while work is being performed.

15. No fires are permitted anywhere in the community for the purpose of construction.

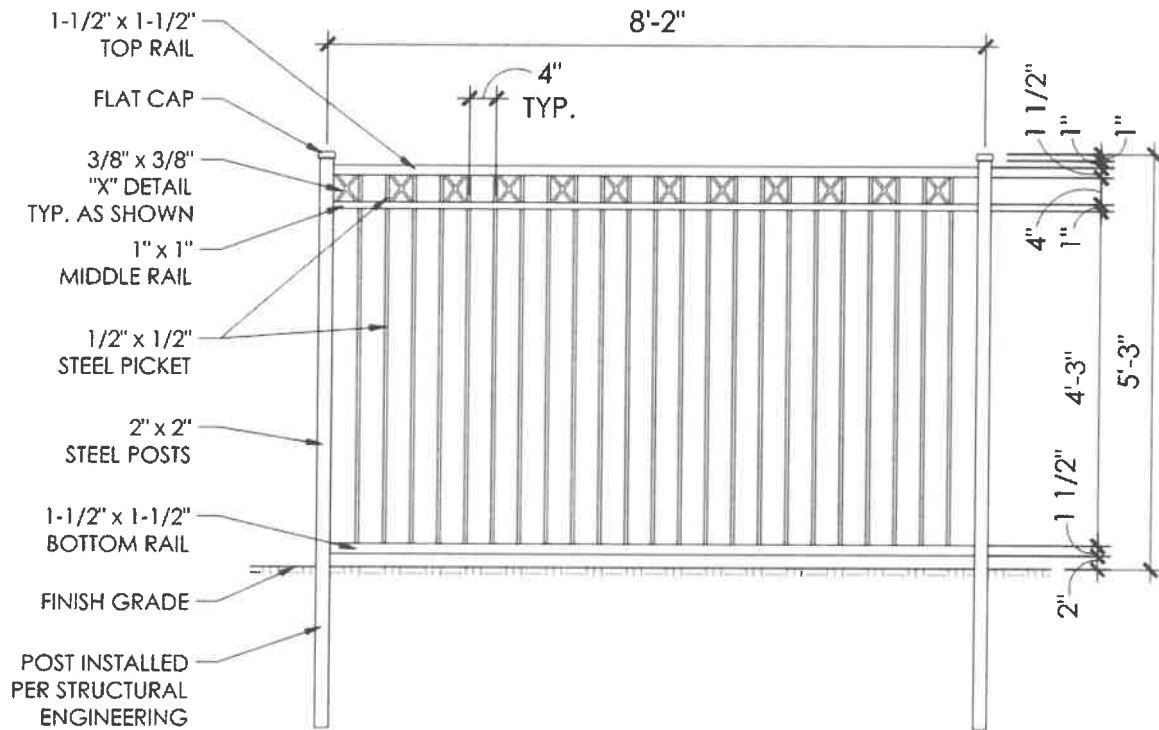
16. At their own discretion, Builders may seek a Temporary Right-of-Way Use Permit from the City of Middleton to temporarily block the sidewalk in front of a residence under construction. For more information and specific requirements, please call _____.

G. Complaints. Any complaints will be conveyed to the applicable Builder or Owner, and each Builder or Owner will be held responsible by the Architectural Reviewer to ensure their contractors and subcontractors remain compliant with these Design Guidelines at all times.

EXHIBIT A.
VIEW FENCE

Manufacturer: Boise River Fence

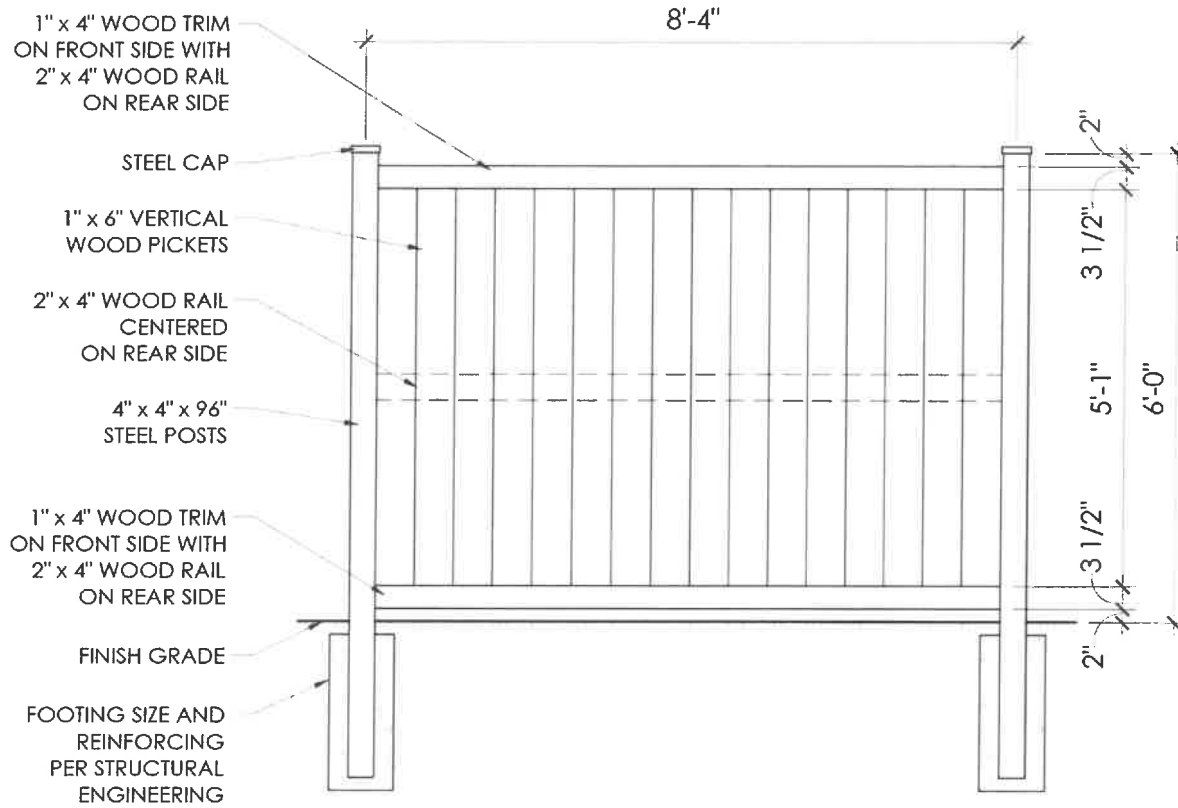
Product: M3 Wrought Iron



NOTE:
ALL METAL TO BE FINISHED WITH POWDER COAT COLOR "RUST TEXTURE
T091-BR47" BY CARDINAL.

EXHIBIT B.
BUILDER/OWNER SCREEN FENCE

Manufacturer: Boise River Fence
Product: M3 Custom Picture Frame Cedar with Steel Posts

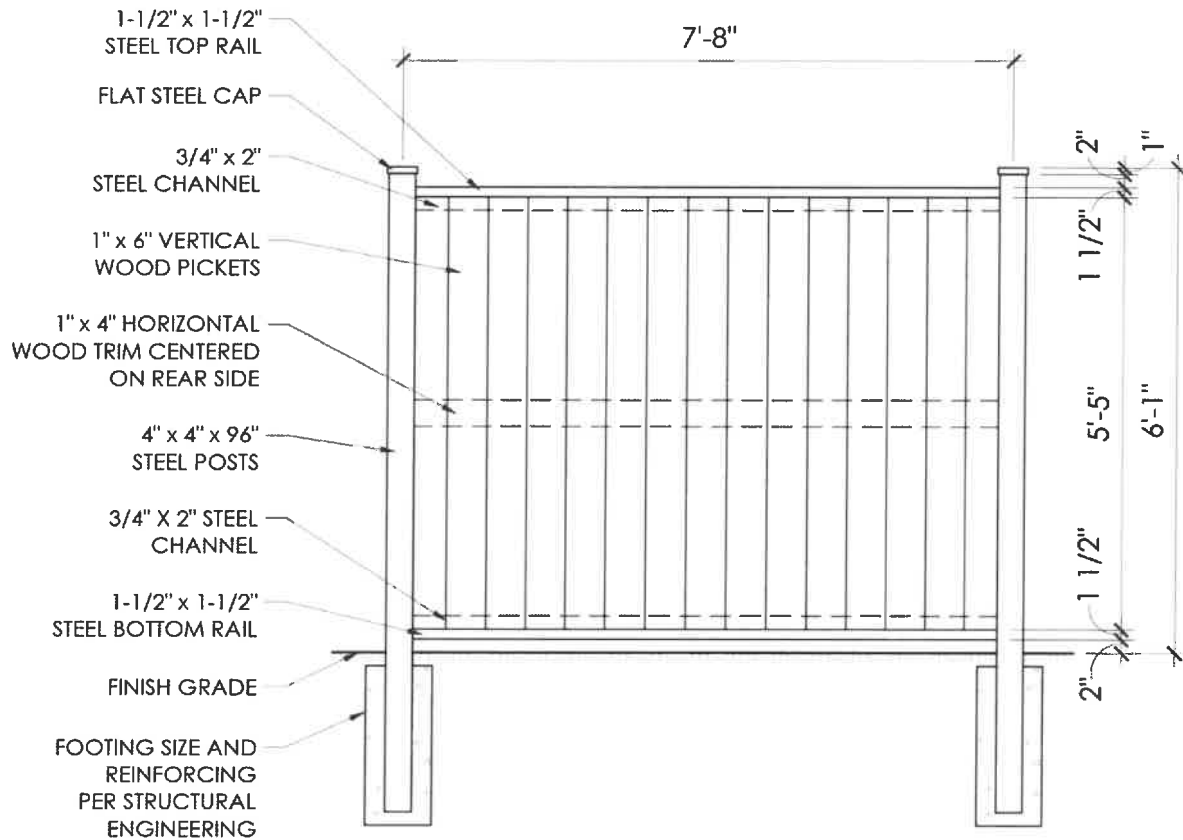


NOTES:

1. ALL METAL TO BE FINISHED WITH POWDER COAT COLOR "RUST TEXTURE T091-BR47" BY CARDINAL.
2. ALL WOOD TO BE STAINED WITH STAIN COLOR "PECAN" BY READY-SEAL.

EXHIBIT C. DEVELOPER SCREEN FENCE

Manufacturer: Boise River Fence
Product: M3 Vertical Privacy 2.0

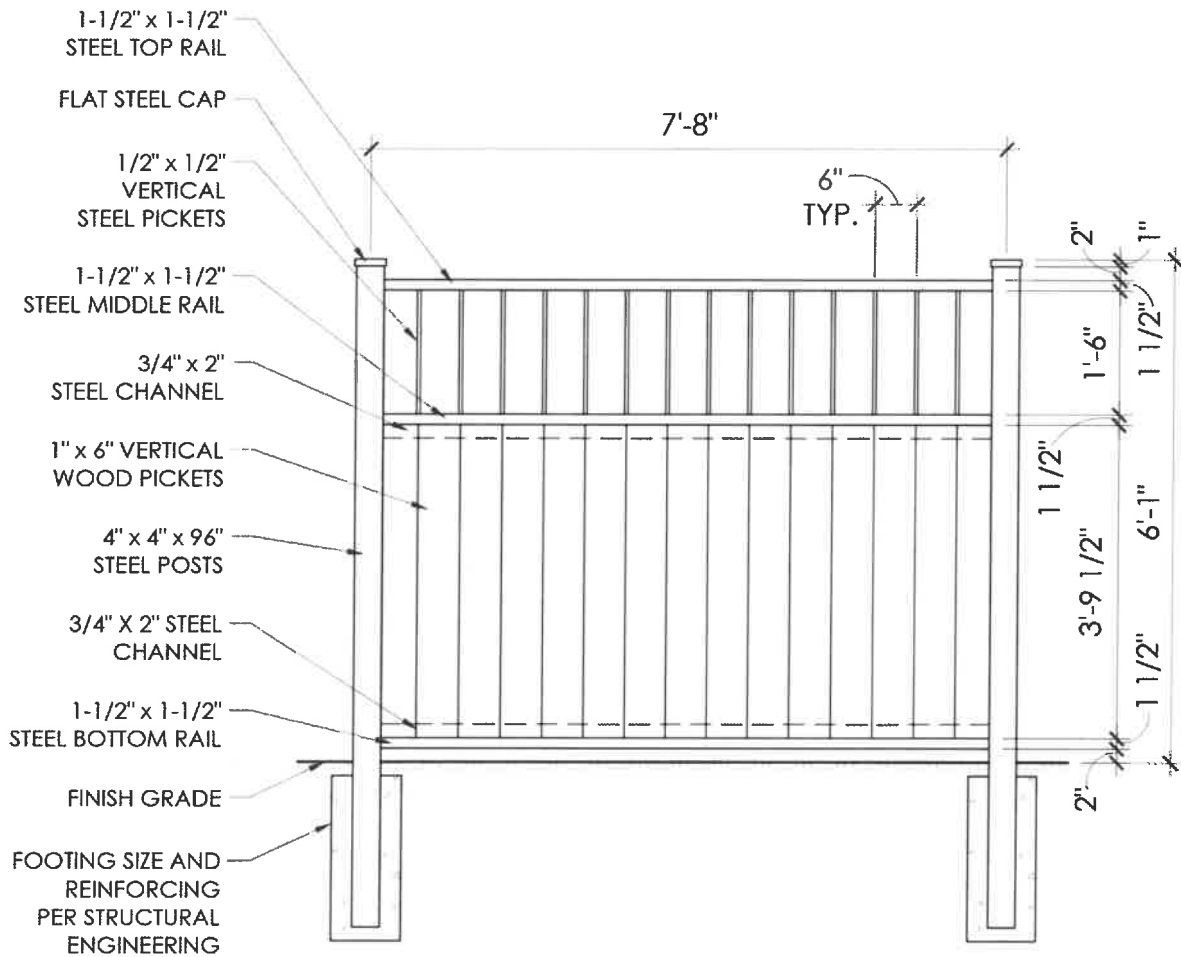


NOTES:

1. ALL METAL TO BE FINISHED WITH POWDER COAT COLOR "RUST TEXTURE T091-BR47" BY CARDINAL.
2. ALL WOOD TO BE STAINED WITH STAIN COLOR "PECAN" BY READY-SEAL.

EXHIBIT D.
DEMI-SCREEN FENCE

Manufacturer: Boise River Fence
Product: M3 Picket Top Privacy 2.0



NOTES:

1. ALL METAL TO BE FINISHED WITH POWDER COAT COLOR "RUST TEXTURE T091-BR47" BY CARDINAL.
2. ALL WOOD TO BE STAINED WITH STAIN COLOR "PECAN" BY READY-SEAL.

EXHIBIT E.
FENCE PLANS BY PHASE

EXHIBIT F.
FINAL LOT GRADING & DRAINAGE DETAILS

All excess surface water on any Lot shall be directed to the street in accordance with the below detail and/or retained on such Lot, and no Lot is permitted to drain onto an adjacent Lot or Common Area.

Detail 1 – All Lots

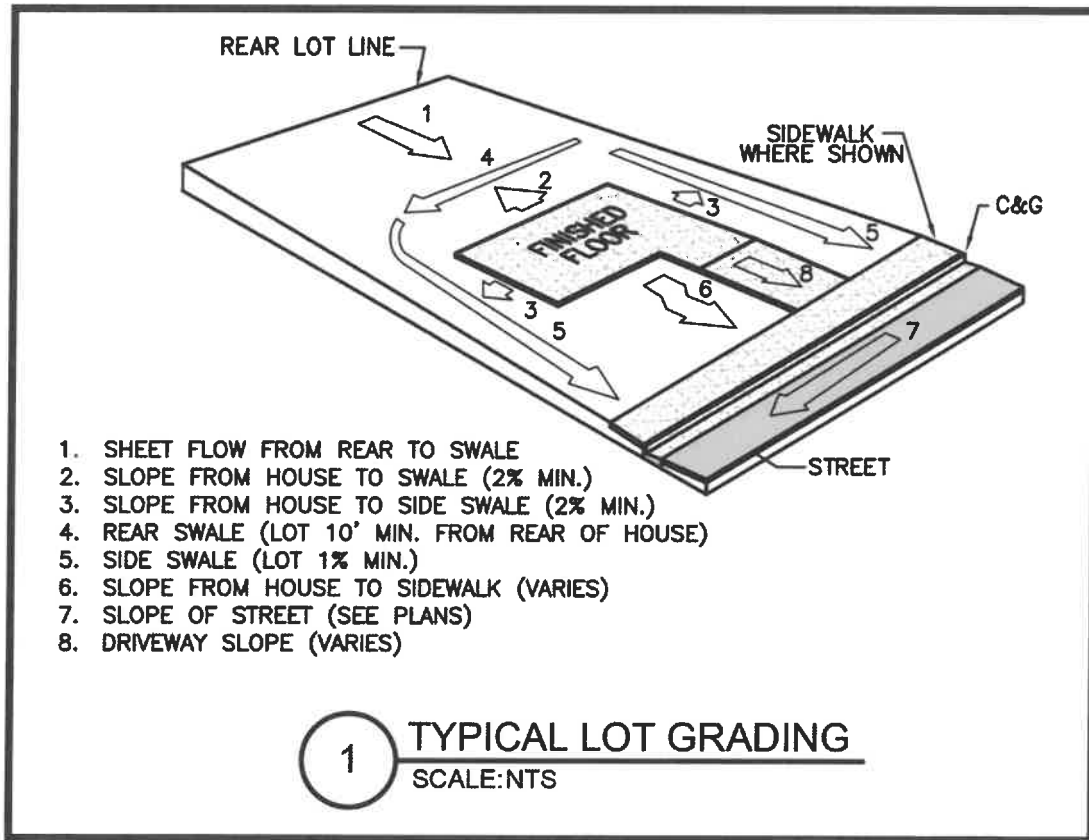


EXHIBIT G.
PLANTER STRIP TREE PLANTING RESTRICTIONS

EXHIBIT H.
ROCK COLOR/TYPE & BOULDER WALL DESIGN EXAMPLES

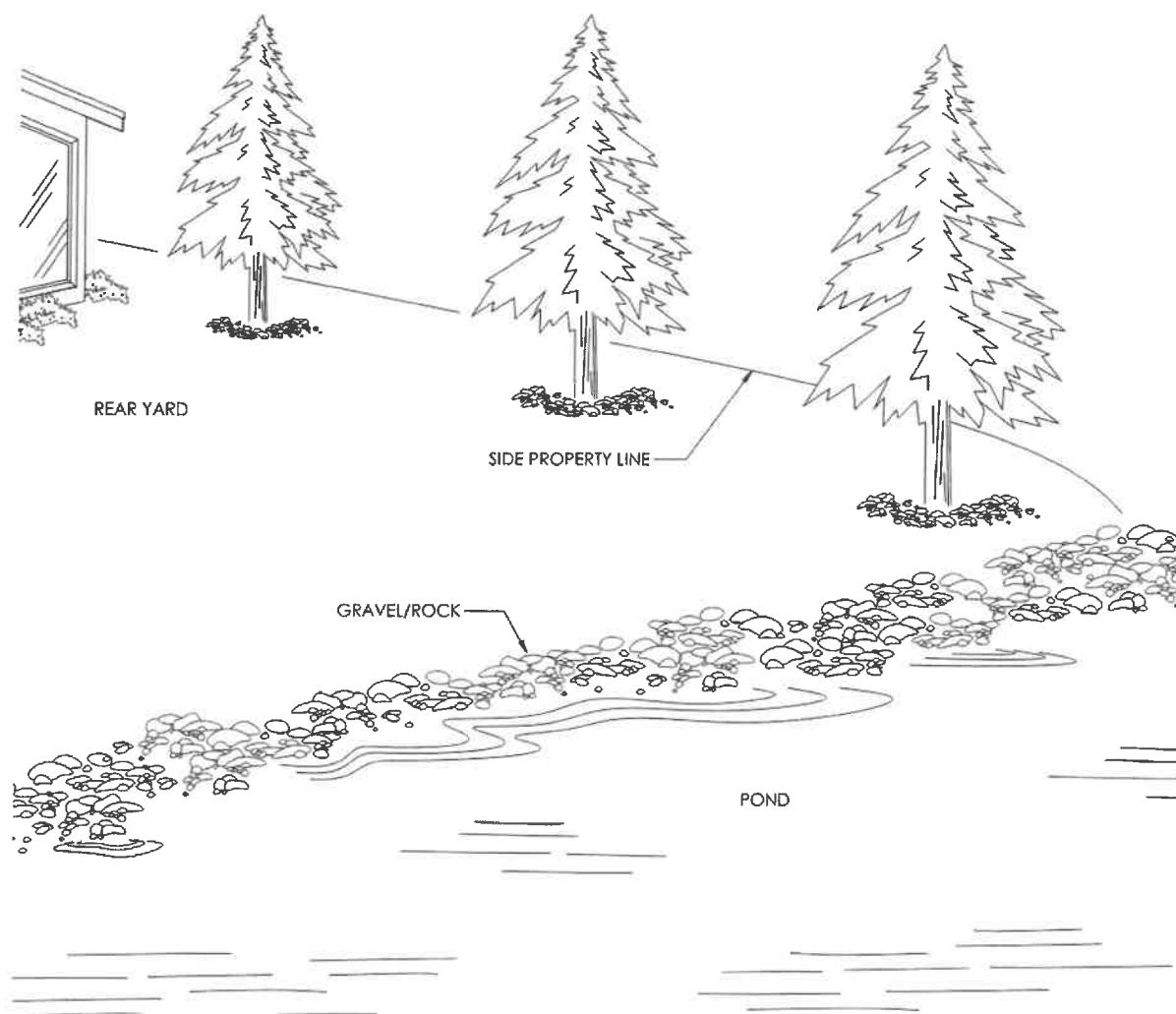
Rock chips, rip-rap, and boulders, including boulders used for retaining, are to match or be substantially similar to the rock color/type shown below. Additionally, boulder retaining walls shall be substantially similar in design and appearance as those constructed below.



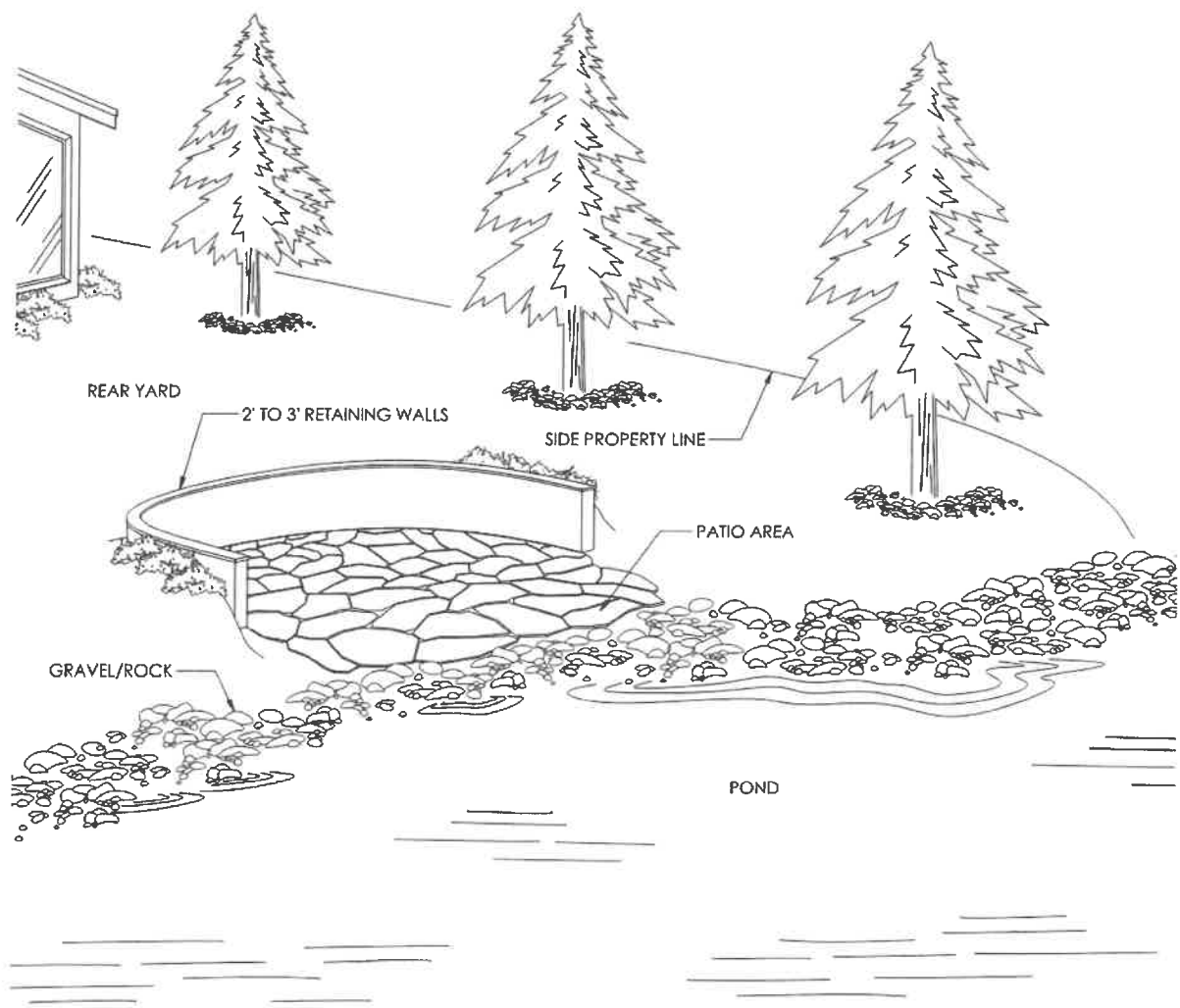
EXHIBIT I.
DIRECT ENTRY LAKE FRONT LOTS BY PHASE

EXHIBIT J.
LAKE EDGE IMPROVEMENTS

Cobble Along Edge of Water



Recessed Patio by Water's Edge



Recessed Sand "Beach" at Water's Edge

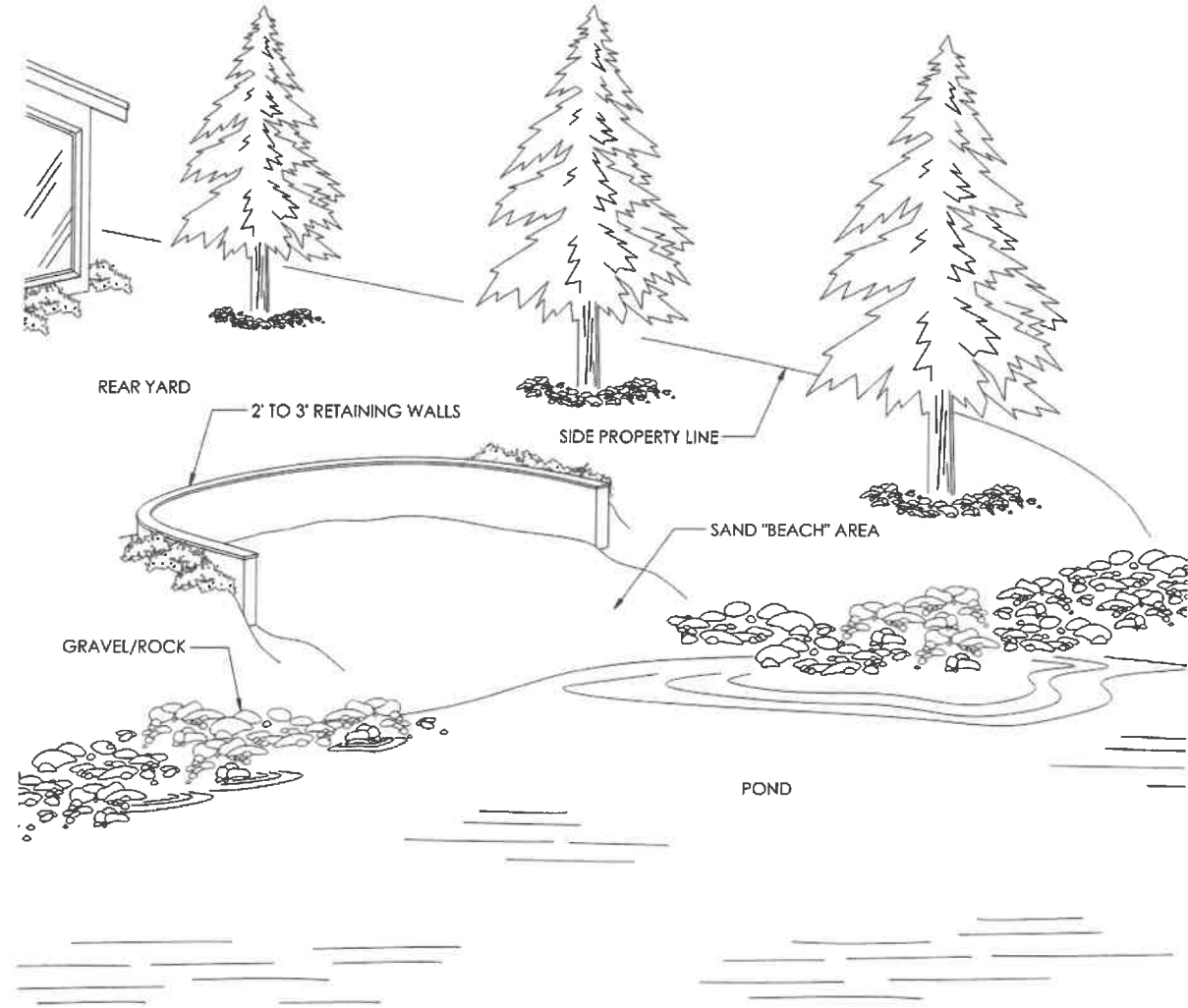
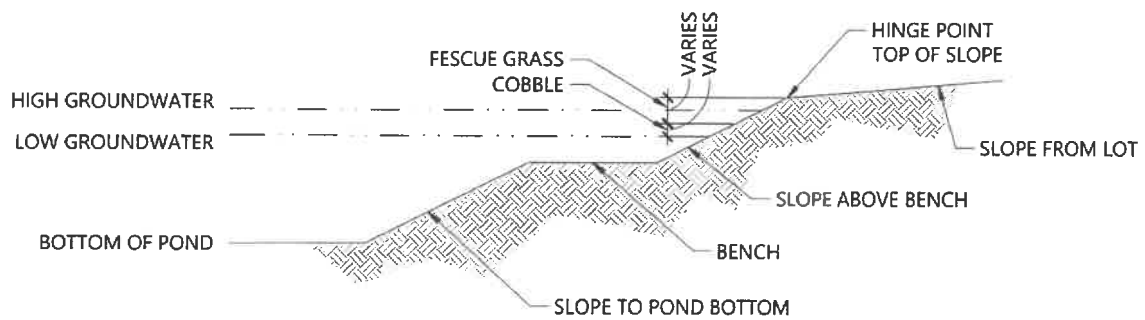


EXHIBIT K.
POND WATER LEVEL ILLUSTRATIONS & TYPICAL CROSS-SECTION

*Illustrative use only. Water levels shown are approximate.
Actual water levels and conditions will vary and are subject to change over time.*

TYPICAL CROSS-SECTION - ALL PONDS



ATTACHMENT 1.
APPLICATION FOR DESIGN REVIEW

Part 1 - Design Review Application

Submittal Type:

☐ Initial Design Review Submittal

☐ Substantially Revised Submittal

☐ Alteration Submittal

☐ Other: _____

Owner / Builder Information:

Builder _____ Email _____ Phone _____

Address _____ Zip _____

Owner _____ Email _____ Phone _____

Address _____ Zip _____

Site Information:

Project Address _____

Lot/Block/Sub Number _____ Lot Width at Front Setback _____

Proposed Plan Information:

Building Height _____ Number of Stories _____ Plan Name or # _____

Gross Square Feet (SF):

First Floor _____

Second Floor _____

Other _____

Total: _____

Finished Square Feet (SF):

First Floor _____

Second Floor _____

Other _____

Total: _____

Exterior Materials and Colors:

Roof _____

Main Body _____

Windows/Doors _____

Fascia _____

Stone/Brick _____

Trim _____

Other _____

Proposed Improvements Included in Submittal:

☐ Residential Plan

☐ RV Garage

☐ Solar

☐ Accessory Building

☐ Detached Storage

Part 2 – Design Review Checklist

Each of the items listed below must be submitted along with this completed Application. This is not a complete list of submittal requirements; please refer to the applicable section of the Design Guidelines for specific guidelines and submittal requirements. The Architectural Reviewer may request additional information at its sole discretion, and no submittal is considered complete until all requested items have been received.

- ☐ 1. Application. Each submittal shall be accompanied by this completed Application
- ☐ 2. Site Plan - Show the Lot boundary, the proposed location of all Improvements (including, but not limited to, all structures, driveways, sidewalks, fences, dog runs, patios, decks, outdoor lighting, utility meters, mechanical equipment, etc.), all easements, and all proposed setbacks. Include a roof plan as part of the site plan if a separate roof plan is not submitted. Use arrows to indicate the proposed grading and drainage flows off the Lot and away from the residence and adjacent Lots. (Minimum scale of 1/8" = 1'-0".)
- ☐ 3. Roof Plan(s) - Depict masses, ridgelines, and roof pitches and include pertinent dimensions (e.g. gable span, overhang depths) if not provided elsewhere. Show the locations of any roof drains or scuppers, as applicable. The roof plan may be included on the site plan in lieu of a separate roof plan. (Minimum scale of 1/8" = 1'-0".)
- ☐ 4. Floor Plan(s) - Include the per floor square feet and total finished square feet of the residence, exclusive of garages, covered patios, storage areas, etc. Show exterior residential lighting locations and specifications. Provide exterior dimensions on all plans. (Minimum scale of 1/4" = 1'-0".)
- ☐ 5. Building Elevations - Illustrate front, rear and side views, showing the maximum building height and include proposed exterior finish material descriptions. (Minimum scale of 1/8" = 1'-0".)
- ☐ 6. Specifications - Include the specifications and descriptions of any proposed exterior materials, finishes, equipment, etc.
- ☐ 7. Exterior Materials and Colors - Provide exact color samples of proposed exterior material colors, (e.g. paint, stain). Samples of all other materials (e.g. brick, stone) shall be shown on the elevations. All proposed exterior materials and colors shall be identified by name, number, color, and manufacturer.
- ☐ 8. Landscape Plan - Show the proposed landscape layout including elevations and dimensions as applicable, along with any proposed fencing, landscape lighting, and a legend of the plant types, sizes and quantities. Use arrows to indicate the proposed grading and drainage away from the proposed residence and adjacent Lots. It is preferred that the landscape plan be submitted with the initial submittal, but it may be submitted up to sixty (60) days after the initial submittal. (Minimum scale of 1/8" = 1'-0".)
 - ☐ Please mark this box if the landscape plan will be submitted separately.
- ☐ 9. Design Review Fee – If not already paid, include the applicable design review fee and mark the box below indicating the applicable fee type pursuant to *Section II.A.9*.
 - ☐ Initial design review fee - \$450
 - ☐ Additional design review fee - \$250

Exhibit E

Approved Modifications to Code

EXHIBIT E
Approved Modifications to Code

Original Code	Redline Effective Changes to Code
<p>4-1-1.H (Driveways)</p> <p>All lots shall front upon a public road unless otherwise approved by the City. Residences having a garage shall have a hard surface of concrete or asphalt driveway and approach with a width at least equal to the garage width that extends from the garage to the abutting public road. Driveways from garages not facing the street shall be a minimum twelve feet (12') wide between the garage and publicly maintained roads.</p>	<p>All lots shall front upon a public road <u>or approved private road</u>, unless otherwise approved by the City. Residences having a garage shall have a hard surface of concrete or asphalt driveway and approach with a width at least equal to the garage width that extends from the garage to the abutting public road. Driveways from garages not facing the street shall be a minimum twelve feet (12') wide between the garage and publicly maintained roads.</p>
<p>5-4-7.A.3 (Final Plat – Letter of Credit)</p> <p>The City may accept an irrevocable letter of credit, cashier's check or other guarantee in the amount of one hundred fifty percent (150%) of the estimated costs to ensure completion of only landscaping and irrigation system improvements in the event that inclement weather precludes the successful installation of landscaping and irrigation.</p>	<p>The City may accept an irrevocable letter of credit, cashier's check or other guarantee in the amount of one hundred fifty percent (150%) of the estimated costs to ensure completion of only landscaping, <u>and</u> irrigation system <u>and fencing</u> improvements <u>for reasons that may include in the event that</u> inclement weather <u>that</u> precludes the successful installation of landscaping and irrigation.</p>
<p>5-4-10-2.D.2 (Roads)</p> <p>All other roads in the City are considered local roads and shall have a half road width of twenty five feet (25') and a total right-of-way width of fifty feet (50') or as determined by the City, provided that a ten foot (10') permanent utility easement is given along each side of the right-of-way line. Roads with fifty foot (50') rights-of-way shall have a right-of-way allowance for a minimum of twenty foot (20') curb radii and pedestrian ramps at the intersections.</p>	<p>All other Interior roads in the City Quarry East are considered local roads and shall have a half road width of twenty five feet (25') and a total right-of-way width of fifty feet (50') forty-seven feet (47') or as determined by the City, provided that a ten foot (10') permanent utility easement is given along each side of the right-of-way line. Roads with fifty foot (50') rights-of-way shall have a right-of-way allowance for a minimum of twenty foot (20') curb radii and pedestrian ramps at the intersections.</p>

<p>5-4-10-2.E (Cul-De-Sac Roads)</p> <p>Cul-de-sac roads shall terminate in a circular turnaround with a right-of-way radius of at least fifty feet (50'). The City may approve an equally convenient form of turning space where extreme conditions justify. The maximum length shall be six hundred feet (600') from the entrance to the center of a turnaround.</p>	<p>Cul-de-sac roads shall terminate in a circular turnaround with a right-of-way radius of at least fifty feet (50'). The City may approve an equally convenient form of turning space where extreme conditions justify. The maximum length shall be six hundred feet (600') <u>one thousand feet (1,000')</u> from the entrance to the center of a turnaround.</p>
<p>5-4-10-2.H.2 (Road Intersections and Alignments)</p> <p>Where any road deflects at an angle of ten degrees (10°) or more, a connecting curve shall be required having a minimum centerline radius of three hundred feet (300') for arterial and collector roads and one hundred twenty five feet (125') for local roads.</p>	<p>Where any road deflects at an angle of ten degrees (10°) or more, a connecting curve shall be required having a minimum centerline radius of three hundred feet (300') for arterial and collector roads and one hundred twenty five feet (125') <u>forty-five feet (45')</u> for local roads, <u>subject to approval by Caldwell Fire.</u></p>
<p>5-4-10-3.A.1 (Block Lengths and Design)</p> <p>Block lengths shall not exceed one thousand one hundred feet (1,100') as measured along the road centerline from centerline intersection to centerline intersection.</p>	<p>Block lengths shall not exceed one thousand one hundred feet (1,100') <u>one thousand seven hundred feet (1,700')</u> as measured along the road centerline from centerline intersection to centerline intersection.</p>
<p>5-4-10-4.A (Lot Requirements: Lot Design)</p> <p>Lot Design: The lot size, width, depth, shape and orientation and minimum setback lines shall comply with the minimum requirements of the zoning regulations of the City as shown in section 5-4-1, Table 2 of this chapter. Lot lines shall be at right angles from the front, side and back property lines, unless otherwise approved as part of a preliminary plat.</p>	<p>Lot Design: The lot size, width, depth, shape and orientation and minimum setback lines shall comply with the minimum requirements of the zoning regulations of the City as shown in section 5-4-1, Table 2 of this chapter, <u>unless otherwise approved by the City in connection with any preliminary plat, variance request, or in connection with an approved development agreement.</u> Lot lines shall be at right angles from the front, side and back property lines, unless otherwise approved as part of a preliminary plat.</p>

<p>5-4-10-4.C (Lot Requirements: Lot Access)</p> <p>Lot Access: All lots shall front on paved public roads, and no lots shall have direct access to collectors, boulevards, or arterials, unless otherwise approved by the City.</p>	<p>Lot Access: All lots shall front on paved public roads, and no No lots shall have direct access to collectors, boulevards, or arterials, unless otherwise approved by the City.</p>
<p>5-4-10-2.F (Dead End Roads)</p> <p>Dead End Roads: Dead end roads will not be approved except in locations designated by the City as necessary to future extensions in development of adjacent lands. In any case, a dead end road serving more than four (4) lots shall provide by easement a temporary turning circle with a fifty foot (50') radius or other acceptable design to accomplish adequate access. Turnaround to be improved with an all weather surface meeting the requirements of the International Fire Code.</p>	<p>Dead End Roads: Dead end roads will not be approved except in locations designated by <u>an applicant and approved by</u> the City as necessary to future extensions in development of adjacent lands. In any case, a dead end road serving more than four (4) lots shall provide by easement a temporary turning circle with a fifty foot (50') radius or other acceptable design to accomplish adequate access. Turnaround to be improved with an all weather surface meeting the requirements of the International Fire Code.</p>

Exhibit F

Street Standards

EXHIBIT F
Street Standards
(Modifications from ISPWC)

Original Code	Redline Effective Changes to Code
<p>B.</p> <p>Improved Section: All improved sections classified as local roads shall have widths from back-of-curb to back-of-curb of thirty-eight (38) feet. Developments that submit improved sections different from the standard may be evaluated and approved on a case-by-case basis</p>	<p>B.</p> <p>Improved Section: All improved sections classified as local roads shall <u>may</u> have widths from back-of-curb to back-of-curb of <u>thirty-three (33) feet minimum</u>. Developments that submit improved sections different from the standard may be evaluated and approved on a case-by-case basis. <u>Private streets must meet the minimum street section of 3" of asphalt, 6" of road mix, and 15" of pit run or meet the City's adopted local roadway section if different than above.</u></p>
<p><u>4. Private Lane</u></p> <p>A. A private lane may be constructed to access up to 3 (3) single-family residences. Private lanes will be reviewed on a Case -by-case basis and specifically approved b y the City. Private lanes will not be allowed to access more than three (3) single-family residences.</p>	<p><u>4. Private Lane</u></p> <p>A. A private lane may be constructed to access up to <u>four (4)</u> single-family residences. Private lanes will be reviewed on a Case -by-case basis and specifically approved by the City. Private lanes will not be allowed to access more than <u>four (4)</u> single-family residences.</p>
<p>6.</p> <p>D. Valley Gutters: Valley gutters shall be a minimum of 10" thick and 4'-0" wide with #4 rebar longitudinal at 12" on center and #4 rebar on both ends and in the middle. Valley gutter base shall be a minimum of 6" thick of three-quarter inch (3/4") crushed aggregate gravel placed as specified in Section 802 ISPWC.</p>	<p>D. Valley Gutters: Valley gutters shall be a minimum of 10" thick and 4'-0" wide with #4 rebar longitudinal at 12" on center and #4 rebar on both ends and in the middle. Valley gutter base shall be a minimum of 6" thick of three-quarter inch (3/4") crushed aggregate gravel placed as specified in Section 802 ISPWC. <u>If a narrower valley gutter is requested, the revised width will be approved if conveyance capacity is adequate.</u></p>
<p>7.</p> <p>D.</p> <p>3. Tree Planting of any type is prohibited within ten feet (10') of any seepage bed or sand filter facility, structure, piping system, fire hydrant or utility box.</p> <p>4. Class I, class II, and class III trees (listed in Appendix A) may be planted a minimum of five (5') away from sidewalk. All other trees not listed shall be planted at least fifteen (15') feet behind the back of the sidewalk.</p>	<p>7.</p> <p>D.</p> <p>3. Tree Planting of any type is prohibited within ten feet (10') of any seepage bed or sand filter facility, structure, piping system, fire hydrant or utility box.</p> <p><u>4. Class I, Class II, and Class III trees in Appendix B may be planted a minimum of three (3') away from sidewalk with the installation of root barriers and 4' without root barriers. All other trees not listed shall</u></p>

	be planted at least fifteen (15') feet behind the back of the sidewalk.
<p>10. <u>Street lights</u></p> <p>A. Street lights shall be installed at intersections, cul-de-sacs and at a maximum of 400-foot intervals, or as sufficient to support safety for all users, including pedestrians and non-motorized users.</p>	<p>10. <u>Street lights</u></p> <p>A. Street lights <u>on private streets</u> may be shall be installed at intersections, cul-de-sacs and at a maximum of 400-foot intervals, or as sufficient to support safety for all users, including pedestrians and non-motorized users. <u>Alternative styles or aesthetics of street lights may be permitted on private streets with prior review and approval of the City Engineer, but must otherwise meet dimensional standards.</u></p>
<p>11. Street Signs. Post shall be type E-1 as shown in SD-1 130 of the ISPWC with 14-gauge wall thickness. Street signs shall be in conformance with Manual on Uniform Traffic control Devices and shall be a nine-inch (9") blank, six-inch (6") all capital lettering, no border, with retro-reflective, high intensity background, with the street type superscript (upper right). Submit a shop drawing to the City for approval. Speed limit signs shall be installed by the devoper-20 mph for subdivision roads.</p>	<p>11. Street Signs. Post shall be type E-1 as shown in SD-1 130 of the ISPWC with 14-gauge wall thickness. Street signs shall be in conformance with Manual on Uniform Traffic control Devices and shall be a nine-inch (9") blank, six-inch (6") all capital lettering, no border, with retro-reflective, high intensity background, with the street type superscript (upper right). Submit a shop drawing to the City for approval. Speed limit signs shall be installed by the devoper-20 mph for subdivision roads. <u>Alternative styles or aesthetics of street signs may be permitted on private streets with prior review and approval of the City Engineer, but must otherwise meet dimensional standards.</u></p>
<p>15</p> <p>1. The width of the driveway abutting the public street shall match the width of the garage.</p>	<p>15</p> <p>1. The width of the driveway abutting the public street shall match the width of the garage. <u>Alternative driveway configurations, including configurations narrower than the garage, are allowed.</u></p>
<p>Access</p> <p>Local Roads</p> <p>1. Residences having a three (3) car garage shall have a driveway width at least equal to the three (3) car garage that extends from the garage to the abutting public street</p> <p>2. Driveway spacing near intersections. Driveways on local roads shall be located a minimum of seventy-five (75) feet in width.</p>	<p>Access</p> <p>Local Roads</p> <p>1. Residences having a three (3) car garage shall have a driveway width at least equal to the three (3) car garage that extends from the garage to the abutting public street. <u>Alternative configurations, including configurations narrower than the garage, are allowed.</u></p> <p>2. Driveway spacing near intersections. Driveways on local roads shall be located a minimum of seventy-five (75) feet <u>from centerline of the applicable intersection</u> in width.</p>

EXHIBIT G
Site Plan of Clubhouse Property

Exhibit G
Amenity Site Plan

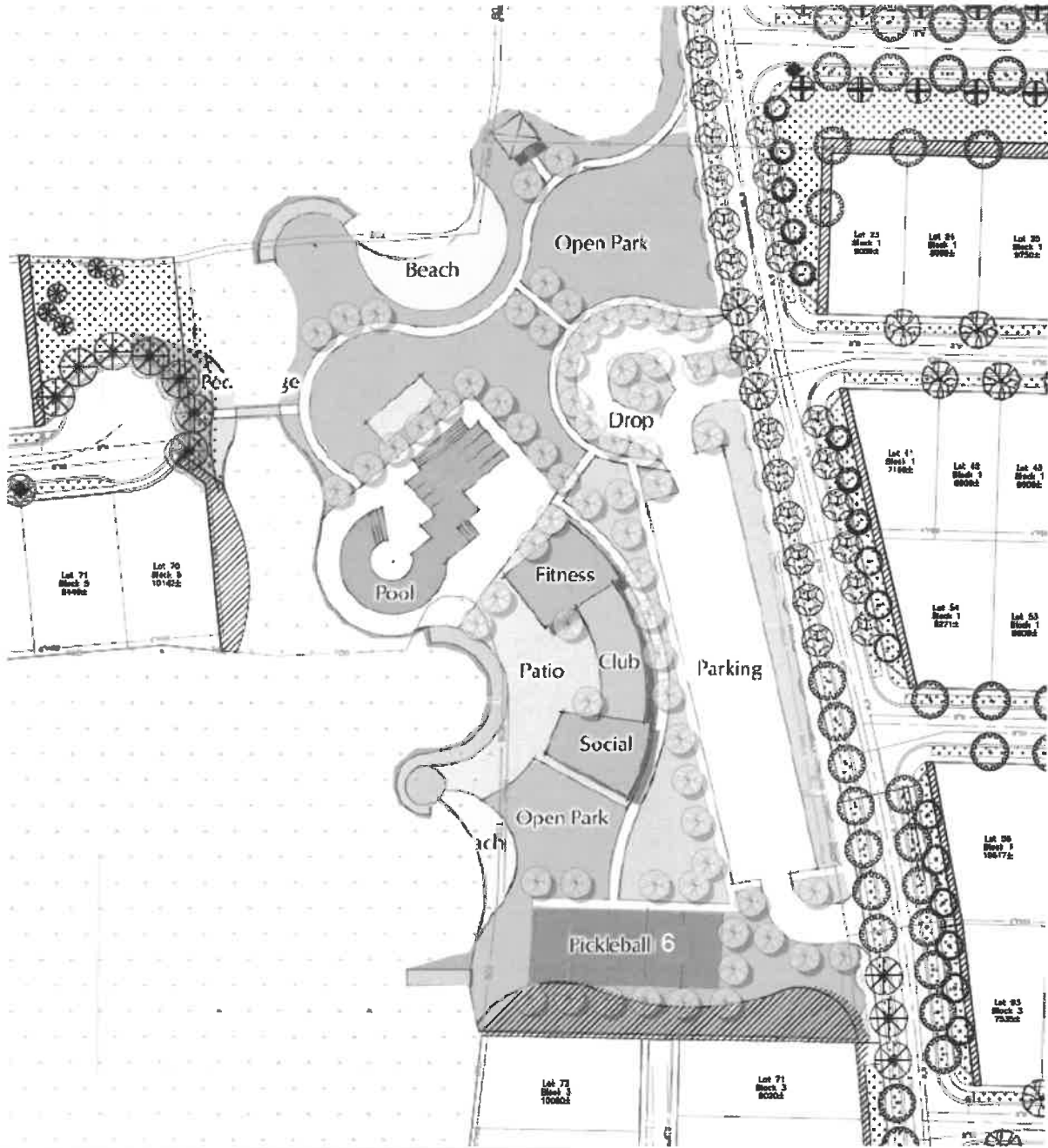


EXHIBIT H
Conditions of Approval

[TBD at City Council Public Hearing]

EXHIBIT "C"

Agency Comments



CITY OF *Caldwell, Idaho*

JAROM WAGONER
Mayor

208.455.3011
(f) 208.455.3003

City Hall
411 Blaine Street
Caldwell, Idaho 83605

Post Office Box
P.O. Box 1179
Caldwell, Idaho 83606

For a list of the City
Council members, visit:
Website
www.cityofcaldwell.org



STEVE DONAHUE
Deputy Chief

ALAN PERRY
Division Chief / Fire Marshal

BRAD JOHNSON
Division Chief

403 Blaine Street
Caldwell, ID 83605

208.455.3032

Greetings Roberta Stewart,

As requested I have reviewed the proposed access plan for the "Quarry West & East" project. Based on the information provided at our January 13, 2022 meeting with the developer Mark Tate and Canyon County Fire Protection District, District Manager Mark Wendelsdorf the attached documentation is in compliance with the 2018 International Fire Code. The Fire Protection District agrees that this site plan is acceptable with a few additional conditions.

- "Full gated access" points shall be equipped with an Opticom System allowing emergency access to first responders.
- "Emergency access" point(s) along Middleton Rd. are request to have a swing type gate with a knox lock as opposed to ballads, D103.5 2018 IFC. Appropriate signage stating "Emergency Access and No Parking" is required.
- Internal emergency access is approved with Knox compatible/lockable ballads as identified on the attached plan.
- "Hammerhead Turnarounds" identified at the duplex locations or any travel distance exceeding 150' shall meet the minim requirements found in figure 8 D103.1 of the 2018 IFC.
- All roads shall be a minimum of 26' clear with for emergency response.
- Turning radius are measured from the centerline of the road and shall comply with appendix D of the 2018 International Fire Code

Alan Perry
Division Chief / Fire Marshal
Canyon County Rural Fire District

Roberta Stewart

From: Mark Tate <MTate@m3companiesllc.com>
Sent: Monday, February 14, 2022 1:34 PM
To: Roberta Stewart; Amy Woodruff
Cc: Alan Perry
Subject: FW: Quarry Middleton Follow Up

Thank you Alan for the follow up and explanation.

Roberta and Amy, below is Alan's response on the 90 degree turning radius.

From: Alan Perry <aperry@cityofcaldwell.org>
Sent: Monday, February 14, 2022 1:26 PM
To: Mark Tate <MTate@m3companiesllc.com>
Subject: RE: Quarry Middleton Follow Up

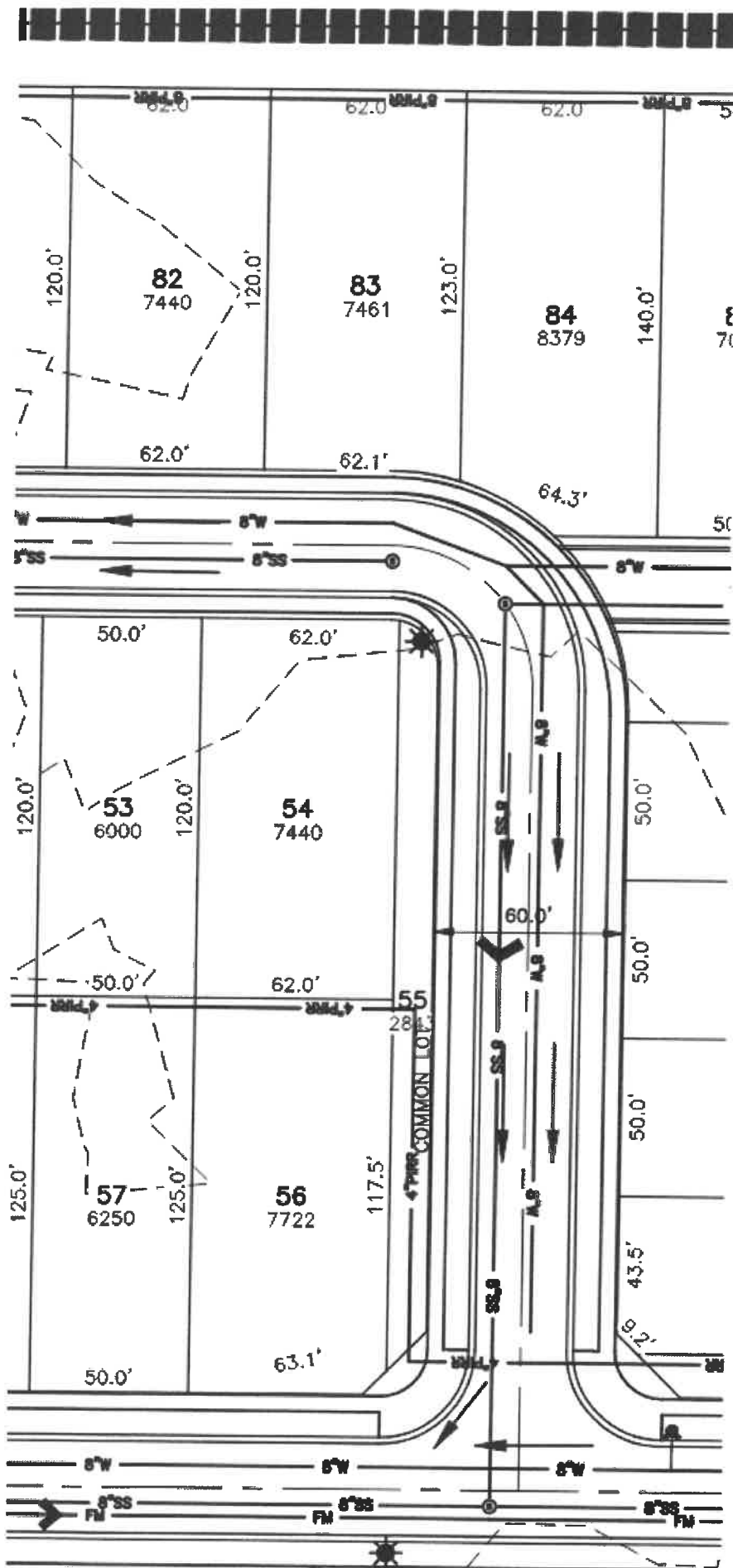
Greetings,

The Middleton/Star Fire Department requires an Autoturn model provided in plan review that reflects the apparatus turning radius. In this instance I will follow suit and request it for this project to eliminate any confusion. Our longest apparatus (Ladder Truck) is 34 ft. in length. If the Autoturn documentation indicates that the ladder truck can make the proposed 90 degree bends with a 45' turning radius I believe it will be accepted.

From: Mark Tate <MTate@m3companiesllc.com>
Sent: Thursday, February 10, 2022 12:27 PM
To: Alan Perry <aperry@cityofcaldwell.org>
Subject: Quarry Middleton Follow Up

Alan,

Middleton wanted to confirm one more thing with you and that was that 90 degree bends with a 45' turning radius is acceptable to Caldwell Fire Department. Below is a snapshot of an example. I'm not exactly sure what the turning radius at a 4 way intersection is, but it looks the same to me. An email to middleton or me saying whether you are okay with it is all they are looking for.



From: [Chris Hopper](#)
To: [Roberta Stewart](#); [Mark Tate](#); hdark@clarkwardle.com; [Wendy Shrief](#)
Cc: [Amy Woodruff](#); [Becky Crofts](#); [Doug Waterman](#); [Mark Hilty](#); [Tim Richard](#)
Subject: RE: The Quarry East: City redlines to M3 3-21-2022 DA
Date: Thursday, March 24, 2022 2:05:01 PM
Attachments: [image002.png](#)

Roberta-

I apologize for the delayed response to your emails but I can only put out one fire at a time.

1. Middleton Rd intersection controls: CHD4 on principal does not support traffic signals proposed for use on Middleton Rd as interim solutions. The signals are not in accordance with the corridor plan CHD4 adopted for Middleton Rd, and in our opinion do not provide the best alternative for safety and efficiency for the corridor. If the city intends to pursue the two signals proposed to mitigate traffic from the Quarry development, CHD4 will continue to request the city take over operation and maintenance of Middleton Rd from Sawtooth Dr to Lincoln Rd, including the Middleton/Lincoln intersection.
2. CHD4 is not staffed or equipped to maintain traffic signal equipment, and does not anticipate adding this capability for the benefit of one development.
3. We have made inquiry with Caldwell to evaluate ROW acquisition for the Middleton/Lincoln roundabout shown on the 2016 Middleton Corridor plan. Caldwell is researching the annexation and development agreement for the parcel at the SW corner of Middleton/Lincoln West regarding requirements for r/w dedication and timing of such. Caldwell has a significant number of developments impacting this intersection along Middleton Rd, Lincoln Rd, and in the vicinity, and should be included early and often in planning, design, and funding for this project.
4. Middleton Rd frontage improvements:
 - a. Mid-Star CIP shows Middleton Rd along the Quarry project frontage as an existing deficiency, with left turn lanes needed at intersections to mitigate existing traffic conditions. Construction of these turn lanes where required along the project frontage could be considered "project improvements" and made a requirement of the development, as opposed to "system improvements" which are funded by impact fees and would be eligible for credit or reimbursement under 67-8209. The remainder of the Middleton widening (from 3-lanes existing deficiency to 5-lanes ultimate) is impact fee eligible and credit or reimbursement for construction of those improvements by the developer should be provided.
5. We recommend the city include a clause in the DA that would allow the city to expedite the requirement for traffic mitigation at either the project entrance onto Middleton Rd or at the Middleton/Lincoln intersection if safety or traffic operations concerns attributable to the development are identified at either of these intersections.

Please note our request for the city to undertake maintenance for the segment of Middleton Rd adjacent to this large development is consistent with requests made to Caldwell and Nampa on other joint jurisdiction road segments similarly affected by large developments. It is principally a means to streamline the efficiency and management of development reviews, inspections and approvals between our agencies, as the current differences in opinion on best management of the corridor will be ongoing through the design process for this project.

Let me know if there are questions on these comments.

Respectfully,

Chris Hopper, P.E.

District Engineer

Canyon Highway District No. 4
15435 Hwy 44
Caldwell, Idaho 83607
208-454-8135

From: Roberta Stewart <rstewart@middletoncity.com>

Sent: Wednesday, March 23, 2022 8:39 AM

To: Mark Tate <MTate@m3companiesllc.com>; hclark@clarkwardle.com; Wendy Shrief <wshrief@jub.com>

Cc: Amy Woodruff <amy@civildynamics.net>; Chris Hopper <CHopper@canyonhd4.org>; Becky Crofts <bcrofts@middletoncity.com>; Doug Waterman <dwaterman@nampalaw.com>; Mark Hilty <Mhilty@nampalaw.com>

Subject: The Quarry East: City redlines to M3 3-21-2022 DA

Hey Mark and Hethe: We accepted most of the redlines in your 3/21/2022 DA for The Quarry. I'm adding Chris Hopper to this email again because I want to make sure CHD4 is tracking along with us because they have a stake in what happens on Middleton Road too.

Couple of things:

1. We agree with you that since Middleton Road frontage work is on the Mid-Star CIP, then you are entitled to 100% reimbursement via 67-8209. So, we agree the "proportionate share" language should be deleted.
2. As to the interim traffic light at the current intersection of Middleton & Lincoln, the City cannot wait until the 400th building permit to get that installed. The intersection is already failing. I went back to the language that the moment (1) you construct an access on to Lincoln and (2) City informs you in writing that it got the needed ROW, you have 12 months to construct the interim traffic light. The number of building permits will not be relevant.

I have to re-notice your hearing by this Friday because I have to get that "stinkin" SUP regarding duplex/townhomes into the notice. Remember, the SUP gets the final approval or denial at P&Z, not City Council. P&Z can really affect your plat.

Since I'm completing notice this Friday, I have to also upload the DA to the website for public viewing by this Friday. If you're going to propose any further changes to the DA, you need to do so quickly. Thanks,

Roberta L. Stewart



CANYON HIGHWAY DISTRICT No. 4

15435 HIGHWAY 44
CALDWELL, IDAHO 83607

TELEPHONE 208/454-8135
FAX 208/454-2008

April 11, 2022

Middleton City Planning and Zoning
Commission and City Council
1103 West Main Street
Middleton, ID 83644
Attention: Roberta Stewart

JUB Engineers
2760 W. Excursion Lane
Meridian, Idaho 83642
Attention: Wendy Shrief

**RE: The Quarry East Subdivision
Preliminary Plat- Middleton Rd and Lincoln Rd Frontage**

Dear Ms. Stewart:

CHD4 provides the following comments on the preliminary plat dated February 4, 2021:

By agreement with the City, (Exchange Maintenance Agreement, 2006) Canyon Highway District No. 4 (CHD4) operates and maintains Middleton Rd and Lincoln Rd where utility work and frontage improvements are planned for the proposed subdivision. Under this agreement, CHD4 is responsible for access permitting and utility permitting along the corridor, and is provided opportunity to review and comment on development plans as they may impact the operation and maintenance of the highway.

CHD4 provides the following comments on the preliminary plat dated January 27, 2022:

General

The proposed development, which consists of 236 acres, and 640 new residential lots, is anticipated to have a significant impact on the local and regional transportation system. This project, in addition to a number of residential and commercial developments recently approved by the City along the Middleton Rd corridor will require near-term improvements to add turn lanes and additional through lanes on Middleton Rd, and intersection capacity at a number of intersections adjacent to or nearby the project.

In view of the magnitude of impact from city development projects on the Middleton Rd corridor, CHD4 requests the City accept for maintenance that portion of Middleton Rd from Sawtooth Drive to the west Lincoln Rd approach. This change in maintenance from one agency to another does not impact the underlying responsibility to participate in capital projects on the corridor; those are established by agreement in proportion to the jurisdiction of each agency (what proportions are annexed or remain within the county).

Responsibility for maintenance operations includes permitting of utility work, frontage improvements, and access points. This project, as well as others proposed between Lincoln Rd and Sawtooth Drive, would benefit from having plan review and permitting performed solely by the City. Although CHD4 generally supports the City's plan for requiring this project make corridor and intersection improvements to mitigate traffic impacts as part of the development, there are differences in the timing and specifics of the proposed mitigation that are likely to cause delays in project approval.

Additionally, CHD4 is reviewing and/or overseeing many dozens of development applications within the county and the cities of Star, Caldwell, and Nampa which also impact CHD4 roadways; review times for new projects are substantially in excess of 60 days. It is unlikely any development application along the Middleton Rd corridor between Sawtooth Drive and Lincoln Rd will be made unless it is annexed into Middleton, as city utilities are available along this corridor, and oversight of this portion of the corridor is a logical extension of the City's roadway network.

Development Access

Middleton Rd is designated as a principal arterial on the functional classification maps adopted by CHD4, Canyon County, and the City of Middleton. Lincoln Rd is classified as a minor arterial on these maps. Both CHD4 and City of Middleton prohibit direct private road access to arterial roadways in their access management policies. Direct access to arterials is generally limited to public collector roadways at one-half mile intervals in order to support the higher speeds and traffic volumes intended for arterial corridors.

Principal access to the development is proposed via two private road connections; one to Middleton Rd approximately 2,700 feet north of Lincoln Rd (west), and one to Lincoln Rd approximately 2,440 feet west of Middleton Rd (160 feet east of SW property boundary). A third private road approach is proposed to Middleton Rd approximately 3,860 feet north of Lincoln Rd (west) to serve the proposed Phase 1 storage unit complex. Direct private road access to arterial road is prohibited or limited to right-in-right-out only (RIRO) under current CHD4 and Middleton access management policy. If exception to this policy is considered by the City, CHD4 recommends addressing the following items of concern:

1. Phase 1 (Storage Units) approach should be provided with a left turn lane (NB Middleton) in the near term and restricted to a RIRO only at buildout. Indirect U-turns to accommodate left turns at this approach should be accommodated by roundabouts at the central private road approach and at Sawtooth Drive in accordance with the Middleton Rd Corridor Plan (2016), or by median u-turn pockets (loons) as otherwise established by the City. Provide sufficient storage (outside of Middleton Rd right-of-way and on the street side of any private gates) to accommodate at a minimum the 95th percentile queue length for traffic entering or exiting Middleton Rd.
2. Middleton Rd private road approach (E. Quarry Lane): provide sufficient storage (outside of Middleton Rd right-of-way and on the street side of any private gates) to accommodate at a minimum the 95th percentile queue length for traffic entering or exiting Middleton Rd. This location may be suitable for relocation of the mid-mile roundabout shown on the Middleton Rd Corridor Plan as it provides access to the largest number of undeveloped parcels on the east side of Middleton Rd. A roundabout or signalized intersection should be provided to accommodate site traffic in the earliest stages of the residential development.
3. Lincoln Rd private road approach (Hammerstone Ave): Location of this approach approximately 160-feet east of the westerly boundary of the subject property does not allow for the use of a shared collector road access with the adjoining Parcel R34078010 immediately west of the subject property as intended by both CHD4 and Middleton access policy. It is CHD4's understanding that the adjoining parcel to the west will be developed as The Quarry West, and that this private road access is suitable to the owners. If approved, a private road access at this location should provide sufficient storage (outside of Lincoln Rd right-of-way and on the street side of any private gates) to accommodate at a minimum the 95th percentile queue length for traffic entering Lincoln Rd.
4. A temporary access to Middleton Rd may be constructed to provide secondary and/or emergency vehicle access to the project until a permanent alternate access to Lincoln Rd is provided. This access should be located at least 660' south of the E. Quarry Lane, and at least 660' feet north of

Lincoln Rd (west). Unless a left-turn lane is provided for NB Middleton Rd, this temporary access should be restricted to right-in-right-out only. Only a single temporary access to Middleton Rd should be in use at any time during development, including construction access points.

Middleton Rd Frontage

A 50-foot half width right-of-way should be provided for Middleton Rd adjacent to the subject property in accordance with city standards and the Middleton Rd Corridor Plan. Frontage improvements consistent with the Corridor Plan should be required, including median u-turn pockets (loons) to accommodate left turns to existing or future access on the east side of Middleton Rd. Consideration for the existing high voltage transmission towers on the west side of Middleton Rd should be made in planning for those frontage improvements. Middleton Rd has a current pavement condition index of 54 (fair condition) and will not require replacement of the existing asphalt mat or road base. A 3" overlay is recommended for maintenance of the existing asphalt surface in the next 12 years.

Lincoln Rd Frontage

A 50-foot half width right-of-way should be provided for Lincoln Rd adjacent to the subject property to accommodate a future minor arterial section. City of Caldwell plans to shift the future road centerline 6-foot south of the section line to allow separation from the existing high voltage transmission towers along the north side of the existing roadway. Frontage improvements consistent with a minor arterial standard should be constructed with the development. Lincoln Rd adjacent to the site was reconstructed in Summer 2021 and can be considered to have a pavement condition index of 100 (excellent condition). No replacement of the existing asphalt mat or road base should be necessary.

Middleton/Lincoln Intersection

The City of Caldwell has received applications for several large developments (> 200 total acres) west of Middleton Rd along Lincoln Rd, which will have further impacts to the Middleton/Lincoln intersection. CHD4 will assist in coordinating development activities affecting this intersection to facilitate a plan and proportionate share of impacts to fund design and construction including all three jurisdictions.

Please feel free to contact me with any questions on these comments.

Respectfully,



Chris Hopper, P.E.
District Engineer

CC: File: Middleton_Middleton Rd_The Quarry Subdivision



CANYON HIGHWAY DISTRICT No. 4

15435 HIGHWAY 44
CALDWELL, IDAHO 83607

TELEPHONE 208/454-8135
FAX 208/454-2008

June 1, 2021

Middleton City Planning and Zoning
1103 West Main Street
Middleton, ID 83644
Attention: Roberta Stewart

JUB Engineers
2760 W. Excursion Lane
Meridian, Idaho 83642
Attention: W. Scott Wonders, P.E.

**RE: The Quarry Subdivision
Preliminary Plat- Middleton Rd and Lincoln Rd Frontage**

Dear Roberta:

By agreement with the City, (Exchange Maintenance Agreement, 2006) Canyon Highway District No. 4 (CHD4) operates and maintains Middleton Rd and Lincoln Rd where utility work and frontage improvements are planned for the proposed subdivision. Under this agreement, CHD4 is responsible for access permitting and utility permitting along the corridor, and is provided opportunity to review and comment on development plans as they may impact the operation and maintenance of the highway.

CHD4 provides the following comments on the preliminary plat dated February 4, 2021:

General

By agreement with the City, CHD4 operates and maintains Middleton Rd and Lincoln Rd adjacent to the proposed development. This subject property is proposed for annexation into the City of Middleton.

Middleton Rd is designated as a principal arterial on the functional classification maps adopted by CHD4, Canyon County, and the City of Middleton. Lincoln Rd is classified as a minor arterial on these maps. Both CHD4 and City of Middleton prohibit direct private road access to arterial roadways in their access management policies. Direct access to arterials is generally limited to public collector roadways at one-half mile intervals in order to support the higher speeds and traffic volumes intended for arterial corridors.

Access

Principal access to the development is proposed via two private road connections, one to Middleton Rd approximately 2,700 feet north of Lincoln Rd (west), and one to Lincoln Rd approximately 2,440 feet west of Middleton Rd (160 feet east of SW property boundary). A third private road approach is proposed to Middleton Rd approximately 3,860 feet north of Lincoln Rd (west) to serve the proposed Phase 1 storage unit complex. Direct private road access to arterial road is prohibited or limited to right-in-right-out only (RIRO) under current CHD4 and Middleton access management policy. If exception to this policy is considered by the City, CHD4 recommends addressing the following items of concern:

1. Phase 1 (Storage Units) approach should be provided with a left turn lane (NB Middleton) in the near term and restricted to a RIRO only at buildout. Indirect U-turns to accommodate left turns at this approach should be accommodated by roundabouts at the central private road approach and at Bass Lane in accordance with the Middleton Rd Corridor Plan (2016), or by median u-turn pockets (loons) as otherwise established by the City. Provide sufficient storage (outside of

- Middleton Rd right-of-way and on the street side of any private gates) to accommodate at a minimum the 95th percentile queue length for traffic entering Middleton Rd.
2. Middleton Rd private road approach (E. Quarry Lane): provide sufficient storage (outside of Middleton Rd right-of-way and on the street side of any private gates) to accommodate at a minimum the 95th percentile queue length for traffic entering Middleton Rd. This location may be suitable for relocation of the mid-mile roundabout shown on the Middleton Rd Corridor Plan as it provides access to the largest number of undeveloped parcels on the east side of Middleton Rd. A roundabout or signalized intersection should be provided to accommodate site traffic in the earliest stages of the residential development.
 3. Lincoln Rd private road approach (Hammerstone Ave): Location of this approach approximately 160-feet east of the westerly boundary of the subject property does not allow for the use of a shared collector road access with the adjoining Parcel R34078010 immediately west of the subject property as intended by both CHD4 and Middleton access policy. The proposed private road stub (E. Siltstone St.) to this westerly parcel may be adequate if the owners of this parcel do not desire public roads for future development; this should be documented by agreement with those owners prior to preliminary plat approval. Otherwise, a public road access located on the common property line would best meet current access policies. If approved, a private road access at this location should provide sufficient storage (outside of Lincoln Rd right-of-way and on the street side of any private gates) to accommodate at a minimum the 95th percentile queue length for traffic entering Lincoln Rd.

Middleton Rd Frontage

A 50-foot half width right-of-way should be provided for Middleton Rd adjacent to the subject property in accordance with city standards and the Middleton Rd Corridor Plan. Frontage improvements consistent with the Corridor Plan should be required, including median u-turn pockets (loons) to accommodate left turns to existing or future access on the east side of Middleton Rd. Portions of these improvements may be eligible for funding or reimbursement through collection of impact fees if the Mid-Star Service Area Capital Improvement Plan is adopted by the City. Consideration for the existing high voltage transmission towers on the west side of Middleton Rd should be made in planning for those frontage improvements. Middleton Rd has a current pavement condition index of 54 (fair condition) and will not require replacement of the existing asphalt mat or road base. A 3" overlay is recommended for maintenance of the existing asphalt surface.

Lincoln Rd Frontage

A 50-foot half width right-of-way should be provided for Lincoln Rd adjacent to the subject property to accommodate a future minor arterial section. City of Caldwell plans to shift the future road centerline 6-feet south of the section line to allow separation from the existing high voltage transmission towers along the north side of the existing roadway. Frontage improvements consistent with a minor arterial standard should be constructed with the development. Lincoln Rd adjacent to the site is being reconstructed in Summer 2021 and can be considered to have a pavement condition index of 100 (excellent condition). No replacement of the existing asphalt mat or road base should be necessary.

Traffic Impacts

A Traffic Impact Study (TIS) dated January 2021 has been prepared by the developer. This study concludes that although all the intersections and/or road corridors included in the study will operate below an acceptable Level of Service (LOS) by the 2030 build-out year, no need for improvements to these intersections is triggered by the proposed development. CHD4 recommends the following:

1. Consider deferral approval or construction of the proposed development until adequate traffic infrastructure is in place to handle the increased demand on the system. Adding more than 3,500 vehicle trips/day to the Middleton Rd and Lincoln Rd corridors, and the intersections within the influence area of the development will further increase congestion and reduce safety for existing road users.
2. Consider a development agreement to mitigate the proportionate share of impacts to the transportation system from the proposed development, and include requirement(s) to construct those improvements directly necessitated by the project, including but not limited to the Middleton Rd corridor and the Middleton/Lincoln intersection.
3. Collect, through development impact fees, exactions, or a combination thereof, a proportionate share of the development's impact to the transportation system, and use those funds to construct (or to reimburse the developer for construction of) improvements sufficient to provide a safe and efficient transportation system capable of supporting the proposed development.

Please feel free to contact me with any questions on these comments.

Respectfully,

A handwritten signature in blue ink, appearing to read "Chris Hopper", with a long horizontal flourish extending to the right.

Chris Hopper, P.E.
District Engineer

CC: File: Middleton_Middleton Rd_The Quarry Subdivision

From: [Chris Hopper](#)
To: [Roberta Stewart](#); zoninginfo@canyoncounty.id.gov; [Stephen Fultz](#)
Cc: [Mark Tate](#); [Wendy Shrief](#); [Amy Woodruff](#); [Becky Crofts](#)
Subject: RE: New Middleton subdivision involving large County collector road
Date: Monday, November 29, 2021 4:19:50 PM
Attachments: [image003.png](#)
[image004.png](#)
[Middleton Rd- The Quarry Prelim Plat CHD4 comments 6.1.21.pdf](#)

Steve-

CHD4 has been largely silent on the issue of the proposed collector, as the Boise River and sparse rural development north of Lincoln Rd and the proposed subdivision limit the future traffic demand for an east-west mid-mile collector in this instance. If public roads were proposed for the entire development it would be a logical requirement, but where the majority of proposed housing units will be served by private roads on the east half of the project it isn't really necessary from a traffic perspective.

If you want to discuss this further with me please let me know.

Respectfully,

Chris Hopper, P.E.

District Engineer

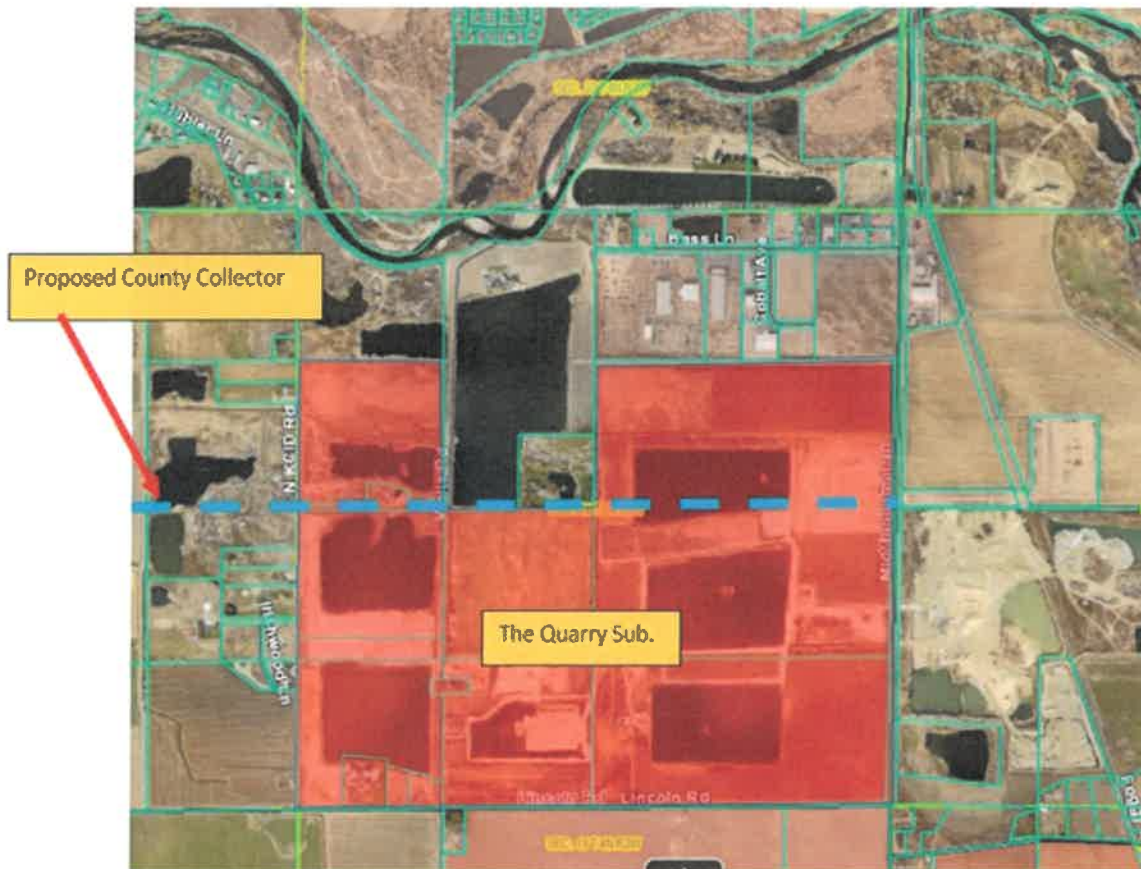
Canyon Highway District No. 4
15435 Hwy 44
Caldwell, Idaho 83607
208-454-8135

From: Roberta Stewart <rstewart@middletoncity.com>
Sent: Monday, November 29, 2021 3:58 PM
To: zoninginfo@canyoncounty.id.gov; [Stephen Fultz <SFultz@canyonco.org>](mailto:SFultz@canyonco.org)
Cc: [Chris Hopper <CHopper@canyonhd4.org>](mailto:CHopper@canyonhd4.org); [Mark Tate <MTate@m3companiesllc.com>](mailto:MTate@m3companiesllc.com); [Wendy Shrief <wshrief@jub.com>](mailto:wshrief@jub.com); [Amy Woodruff <amy@civildynamics.net>](mailto:amy@civildynamics.net); [Becky Crofts <bcrofts@middletoncity.com>](mailto:bcrofts@middletoncity.com)
Subject: New Middleton subdivision involving large County collector road

Hello Steve and Development Services: M3 has submitted an application for annexation and preliminary plat to build approximately 900 residential units in Middleton south of the Boise River (The Quarry Subdivision). See snippet below. It's located at the NW corner of Middleton Road and Lincoln Road.

The County's functional classification map shows a "major collector" running through the center of the proposed subdivision, but the subdivision contains local roads that will prevent the "major collector" from ever being built, particularly since the developer is proposing private roads and a gated community.

We have not received any comments from the County, and we just want to make sure you are aware of the proposed plan before we forge ahead.



Roberta L. Stewart

PLANNER
City of Middleton, Planning & Zoning
1103 W. Main St.
P.O. Box 487
Middleton, ID 83644

Tele - (208) 585-3133
Fax – (208) 585-9601
rstewart@middletoncity.com

www.middleton.id.gov



Roberta Stewart

From: Chris Hopper <CHopper@canyonhd4.org>
Sent: Friday, February 4, 2022 2:56 PM
To: Roberta Stewart; Travis Thomsen
Cc: Mark Tate; Amy Woodruff; Becky Crofts
Subject: RE: DA for M3 Quarry project - changes to 2016 Middleton Corridor Study

Thanks Roberta-

I think the draft DA addresses our largest concern with the project, which is the existing Middleton/Lincoln intersection currently operating at capacity without the traffic influence from the project. Requiring improvements to that intersection as a part of the development is important.

The abandonment of the Middleton Rd corridor plan is likely to be a significant hurdle for us. That plan was adopted by our Board, and we have worked to develop the remainder of the corridor consistent with that plan. The traffic signal being proposed for the private Quarry St access would increase delay on the corridor compared with the planned roundabout intersection, and would not provide the same level of safety benefits that a roundabout does.

If the City intends to depart from the mutually adopted corridor plan for Middleton Rd, we recommend you request to assume maintenance responsibility for Middleton Rd north of Lincoln Rd. This would provide a number of benefits as they relate to this development proposal:

- Under our exchange maintenance agreement, CHD4 would not provide comment on road segments maintained by the City. This would eliminate the need for us to raise objections to the project the city has negotiated, largely without input from CHD4. We do not believe a signalized private road intersection is consistent with the corridor plan, and is likely to increase delay and crash incidence on the corridor, and will protest the project as presented.
- CHD4 is not equipped or staffed to provide for maintenance of traffic signal equipment.
- CHD4 would not be involved in permitting of utility work or frontage improvements on Middleton Rd, which will streamline the plan approval, construction oversight, and acceptance processes for these phases of the project.
- CHD4 would not need to approve secondary access to Middleton Rd proposed for the project

Moving responsibility for maintenance of this segment of Middleton Rd to the City will not change the capital improvement responsibilities identified under the agreement; those are based on underlying percentage of jurisdiction.

A letter of intent to accept Middleton Rd for maintenance from Lincoln Rd to Sawtooth Drive prior to commencement of any construction for this development would be sufficient to allow CHD4 to limit comment on this project to the Middleton/Lincoln intersection, and the Lincoln Rd frontages. Please provide a response on this proposal by mid-next week to allow us time to structure comments on this development in advance of the Feb 14 preliminary plat hearing. I tried calling before I sent this message, as I wanted to discuss these concerns in person. If you and Becky would like to discuss, feel free to give me a call.

Respectfully,

Chris Hopper, P.E.
District Engineer

Canyon Highway District No. 4
15435 Hwy 44
Caldwell, Idaho 83607
208-454-8135

From: Roberta Stewart <rstewart@middletoncity.com>
Sent: Friday, February 4, 2022 12:18 PM
To: Chris Hopper <CHopper@canyonhd4.org>
Cc: Mark Tate <MTate@m3companiesllc.com>; Amy Woodruff <amy@civildynamics.net>; Becky Crofts <bcrofts@middletoncity.com>
Subject: DA for M3 Quarry project - changes to 2016 Middleton Corridor Study

Hi Chris: we are finalizing the development agreement for the 460 home lot subdivision south of Rule Steel known as The Quarry (M3 project). Attached is the latest version of the proposed DA. We met with our traffic engineer, Joel, to discuss the roundabouts shown on the Corridor study. We are proposing some changes to the roundabouts shown in the study:

1. The roundabout shown on the Corridor Study between Bass Lane and Lincoln lane is the entrance to The Quarry. We would like to see a traffic light at that location instead of a roundabout. We suspect that The Quarry feels the same way. Joel indicated that the traffic going in and out of The Quarry will not be enough to adversely affect the efficiency of the roundabout proposed for Lincoln and Middleton road, so he thought a traffic light would work just fine at the quarry entrance.
2. We are still proponents of the multi-lane roundabout at Lincoln and Middleton Road THAT TIES BOTH PARTS of Lincoln Road together. The DA provides that M3 will construct 100% of the RAB by phase 3 of their project, and we will ensure that they are reimbursed appropriately for any costs expended over and above their proportionate share.
3. The RAB at Bass Lane and Middleton Road is a "goner". There is not enough right of way reserved at Middleton Industrial Park #2 to construct the roundabout. There is only a 35' right of way triangle reserved for a traffic light. Joel indicated that ingress and egress out of the industrial subdivision is less than 10% of the total traffic on Middleton Road, so a light at that location will not affect the efficiency of any roundabouts

Just trying to keep you in the loop. Please let me know if you have any comments or questions about the above.

Roberta L. Stewart

PLANNER
City of Middleton, Planning & Zoning
1103 W. Main St.
P.O. Box 487
Middleton, ID 83644

Tele - (208) 585-3133
Fax - (208) 585-9601
rstewart@middletoncity.com

www.middleton.id.gov

Communities in Motion 2050 Development Review

The Community Planning Association of Southwest Idaho (COMPASS) is the metropolitan planning organization (MPO) for Ada and Canyon Counties. COMPASS has developed this review as a tool for local governments to evaluate whether land developments are consistent with the goals of *Communities in Motion 2050* (CIM 2050), the regional long-range transportation plan for Ada and Canyon Counties. This checklist is not intended to be prescriptive, but rather a guidance document based on CIM 2050 goals.

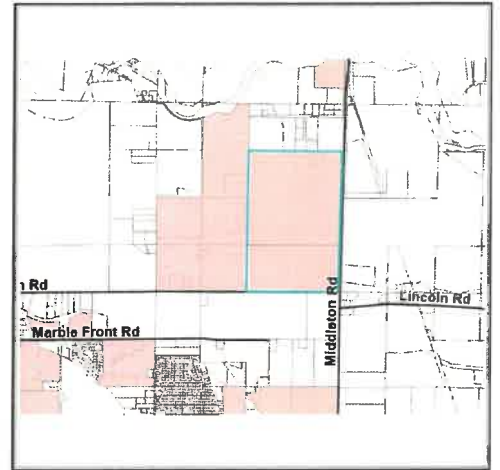
Development Name: The Quarry East

CIM Vision Category: Future Neighborhood

New Jobs: ±80

CIM Corridor: Middleton Road

New Households: 640



Safety

Level of Stress measures how safe and comfortable a bicyclist or pedestrian would feel on a corridor and considers multimodal infrastructure number of vehicle lanes, and travel speeds.

Pedestrian level of stress ☐

Bicycle level of stress ☐



Economic Vitality

These tools evaluate whether the location of the proposal supports economic vitality by growing near existing public services.

Activity Center Access ☐

Farmland Preservation ☐

Net Fiscal Impact ☐

Within CIM Forecast ☐



Convenience

Residents who live or work less than 1/2 mile from critical services have more transportation choices, especially for vulnerable populations.

Nearest bus stop ☐

Nearest public school ☐

Nearest public park ☐



Quality of Life

Checked boxes indicate that additional information is attached.

Active Transportation ☒

Automobile Transportation ☒

Public Transportation ☒

Roadway Capacity ☐



Improves performance



Does not improve or reduce performance



Reduces performance

Comments:

ValleyConnect 2.0 proposes an Express route with 30-minute frequencies along Middleton Road in the future. The proposed stop near Marble Front Road would be about 1/2 from the development. Work with VRT to identify a stop location and space requirements for a stop. Also consider a traffic calming measure to slow cars on Hammerstone Avenue and Seaham Avenue.

Communities in Motion 2050
[2020 Change in Motion Report](#)
[Development Review Process](#)

Web: www.compassidaho.org
Email: info@compassidaho.org



Public Transportation Infrastructure

Providing safe and comfortable transit stops and appropriate amenities can make public transportation a more convenient and competitive option, reduce the overall cost of housing + transportation, and expand the potential customer base for businesses.

While stop location and spacing will depend on the circumstances of the route, there are some general guidelines to improve the user experience:

- ✓ Locate bus stop amenities in areas that are expected to generate the most ridership, such as near employment centers, residential areas, retail centers, education centers, or major medical facilities.
- ✓ Provide sidewalks and/or bike paths designed to meet the needs of all users (including elderly, children, and individuals with disabilities) to connect development to transit stops.
Provide bicycle parking that includes covered bike racks at transit stops; ensure it does not conflict with vehicular or pedestrian travel.
- ✓ Provide shelters, benches, trash receptacles, lighting, and landscaping to enhance the overall comfort and attractiveness of transit; ensure amenities do not block pathways, sidewalks, or bike lanes.
- ✓ **Include doors with 32 inches of clear passage space, and at least one zero-step entrance and accessible bathroom on the main floor to support those with limited mobility.**
- ✓ Join the Valley Regional Transit group pass program:
<https://www.valleyregionaltransit.org/group-pass-programs>
- ✓ Use Valley Regional Transit's [Bus Stop Location and Transit Amenities Development Guidelines](#) for siting new bus stops and reviewing current and bus stops.

Bicycle and Pedestrian Infrastructure

An individual's trip is the entire journey from beginning to end. In many cases, a trip may combine a number of modes. While motorized vehicles will provide longer trips, users complete the first and last portion on their own. For example, almost every vehicle trip includes a walk or bike trip to the parking lot or transit stop. Good street connectivity increase the number of travel options and reduces the distances traveled to reach destinations. One way to measure route directness is take the ratio of the route distance to the straight line-distance. The closer the ratio is to 1, the better for connectivity of the area.

Some steps that can be taken to improve walk/bike infrastructure include:

- ✓ Providing sidewalks, crosswalks, and micropaths to connect destinations
Providing an improved pathway along a canal as a transportation and recreational option
Siting pathways and sidewalks as directly as conditions allow or provide wayfinding signs
- ✓ Reducing street lengths to discourage speeding on local roads
Providing sufficient and covered bike parking near destinations



A disconnected system means more trips onto arterial roads, resulting in fewer cyclists and pedestrians and less efficiency for vehicles.



A connected system provides options, including walking, cycling, or driving. More trips can be taken on local roads, avoiding busier arterials.

Fiscal Impact Analysis Supplemental for the Development Review Checklist

The purpose of the fiscal impact analysis is to better estimate expected revenues and costs to local governments as a result of new development so that the public, stakeholders, and the decision-makers can better manage growth. Capital and operating expenditures are determined by various factors that determine service and infrastructure needs, including persons per household, student generation rates, lot sizes, street frontages, vehicle trip and trip adjustment factors, average trip lengths, construction values, income, discretionary spending, and employment densities.


The COMPASS Development Checklist considers the level of fiscal benefits, how many public agencies benefit or are burdened by additional growth, and how long the proposal will take to achieve a fiscal break-even point, if at all. More information about the COMPASS Fiscal Impact Tool is available at: www.compassidaho.org/prodserv/fiscalimpact.htm.


Overall Net Fiscal Impact

Net Fiscal Impact, by Agency

City 

County 

Highway District 

School District 

Break Even: 1 Year

From: [Jennica Reynolds](#)
To: [Roberta Stewart](#)
Subject: FW: Agency Notice - The Quarry East PZ
Date: Tuesday, March 29, 2022 9:48:45 AM
Attachments: [image001.png](#)

Jennica Reynolds

City of Middleton
Deputy Clerk, Planning
208-585-3133
jreynolds@middletoncity.com



From: Julie Collette <gmpredjulia@gmail.com>
Sent: Tuesday, March 29, 2022 8:31 AM
To: Jennica Reynolds <jreynolds@middletoncity.com>
Subject: Re: Agency Notice - The Quarry East PZ

To Whom It May Concern at Canyon County Planning & Zoning:

Good morning,

On behalf of our Greater Middleton Parks & Rec Board of Directors and Executive Director, Tim O'Meara, we wanted to reach out to you and let Canyon County know that GMPRD is taking a stance on the growth happening in Middleton and the surrounding areas.

We are opposed to any further developments coming into Middleton at this time. Not only are the parks and recreation districts affected, but the school district and cemetery district as well, are struggling to keep up with the growth. The legislature has not added parks and recreation districts, nor school or cemetery districts to the legal verbiage, allowing the impact fee funding to help incur the costs from the additional growth happening around the city.

We are seeing high numbers of registrations at our sporting events coming from out of state. We have multiple sports clubs and recreation programs needing grass space to play on. We are using an outdated building to play sports in one gym and could use at least two more gyms. We are also seeing higher vandalism this year. We have already spent over \$5,000 dollars on new

camera systems at our parks and buildings to try to keep up with the increase in crime in our little town. Last year we were at 419% over budget for vandalism - spending almost \$900 dollars on repairs. This year alone we are already at 110% of our increased vandalism budget, spending over \$550 dollars on repairs thus far. However, in 2019 we only used 12.9% of our vandalism repairs budget. Spending only \$25 dollars on repairs. These numbers alone speak VOLUMES as to what is happening in our area and the negative impact our growth will continue to have, especially on our youth. Having positive sports programs, open play space, and local community classes helps keep kids out of trouble. We are outgrowing our resources at a record pace!

In order for growth to continue and to gain our support, developers will have to step up in some way to aid these growth related issues and problems.

Thank you for listening to our concerns.

Kind Regards,
Julie-

Julie Collette
District Clerk II/HR
Greater Middleton Parks and Recreation District
310 N. Hawthorne Ave., Middleton, ID 83644
208-585-3461
gmpd.org



On Thu, Mar 24, 2022 at 5:15 PM Jennica Reynolds <jreynolds@middletoncity.com> wrote:

Please see the attached Agency Notice.

Jennica Reynolds

Jennica Reynolds
Deputy Clerk, Planning
City of Middleton
208-585-3133
jreynolds@middletoncity.com



**Your Safety • Your Mobility
Your Economic Opportunity**

**Staff Technical Report
5/25/2021**

Development: Quarry Residential Development

Lead Agency: City of Middleton
Roberta Stewart
1103 W. Main St.
P.O. Box 487
Middleton, ID 83644
rstewart@middletoncity.com
Phone: (208) 585-3133

Location: West of Middleton Road and north of Lincoln Road (between US 20/26 and SH-44)
US 20/26 MP 27.25, SH-44 MP 4.17

Applicant: M3 Companies
Mark Tate
General Manager
1087 W. River Street
Suite 310
Boise, ID 83702
mtate@m3companiesllc.com
Phone: 208.939.6263

Consultant: John Ringert
Kittelson and Associates, Inc.
101 S Capitol Boulevard, Suite 600
Boise, ID 83702
jringert@kittelson.com
Phone: 208.338.2683

Staff Contact: Regan Hansen
ITD – District 3
Traffic Signal Engineer
regan.hansen@itd.idaho.gov
Phone: 208.332.7170

Proportionate Share Contribution

ITD District 3 has issued *Memo 39-Development Proportionate Share Contribution (Updated 11-13-2020)* as a means to request equitable contribution from developers to improve public facilities needed to serve new growth and development.

ITD does not have jurisdictional authority to require proportionate share contribution from the developer because they are not asking for a direct access approach. ITD and the city of Middleton have entered into an Intergovernmental Agreement for the city to collect proportionate share contributions on the department's behalf to be used towards future ITIP projects on the State highway system jointly selected by the two agencies.

Intersection	Proportionate Share Contribution
SH-44 & Cemetery Road	\$13,359
SH-44 & S Middleton Road	N/A
SH-44 & N Middleton Road	\$29,845
US-20/26 & KCID Road	N/A
US-20/26 & Middleton Road	\$456,335
TOTAL	\$499,539
<i>Per household unit</i>	<i>Approx. \$641</i>

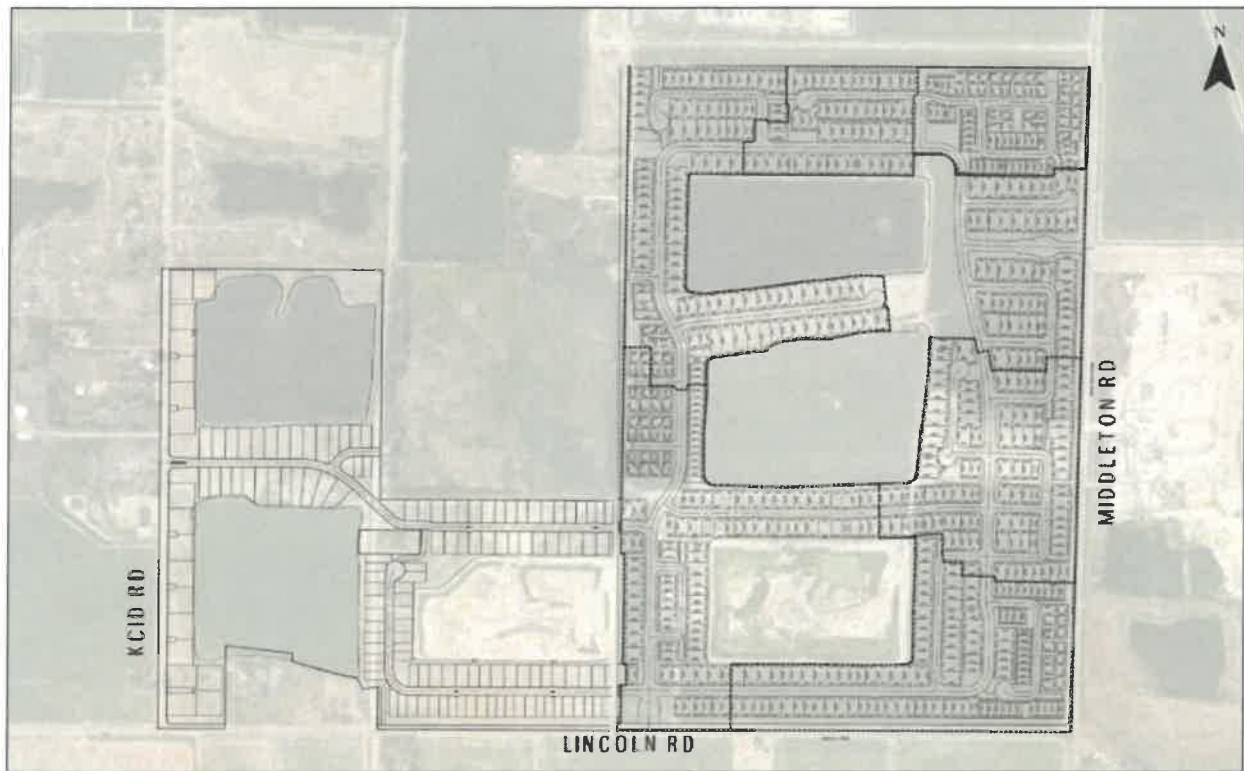
Traffic Impact Study Overview

1. Proposed Development

The proposed Quarry Residential Development will contain 779 senior-living homes and approximately 300 RV storage units. The proposed build-out year is 2030.

2. Vicinity Map





Site plan provided by M3

Preliminary Site Plan
Middleton, Idaho

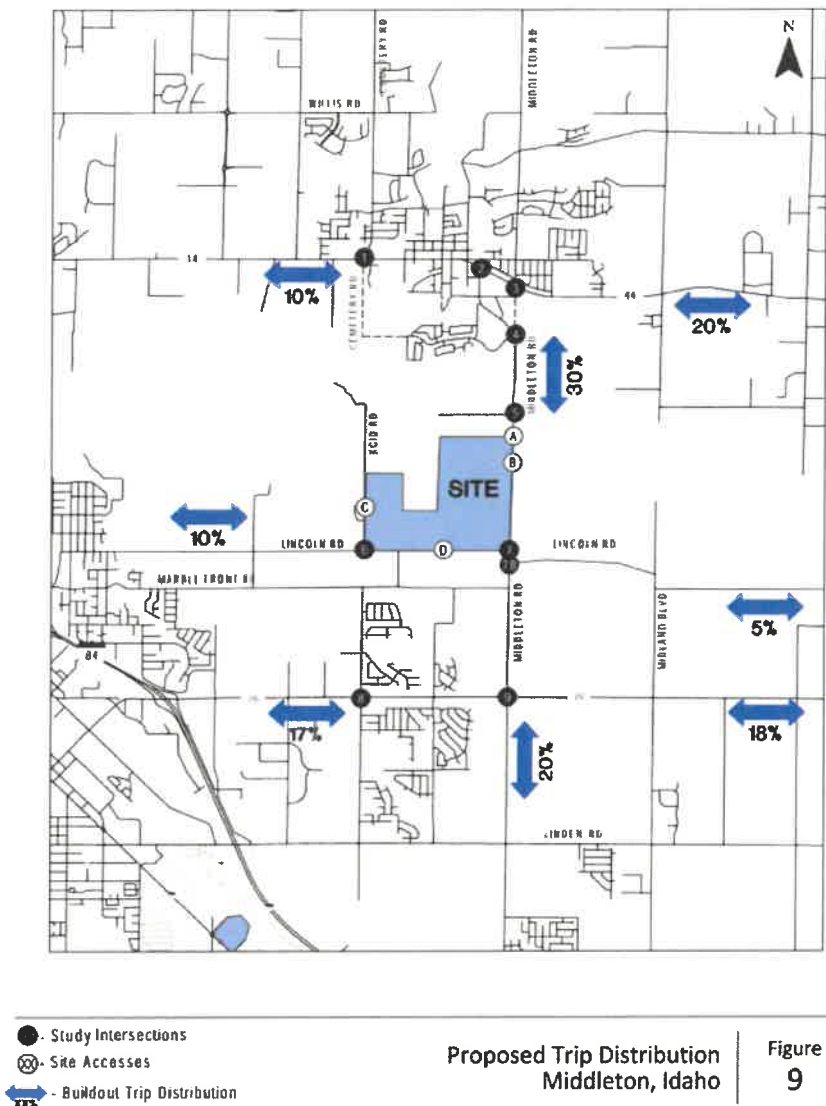
Figure
2

3. Trip Generation

Table 5. Proposed Trip Generation

Land Use	ITE Code	Size	Daily	Weekday AM Peak Hour			Weekday PM Peak Hour		
				Total	In	Out	Total	In	Out
Senior Adult Housing - Detached	251	779	3,426	194	64	130	238	145	93
RV Storage	N.A.	300	100	20	10	10	30	15	15
Total			3,526	214	74	140	268	160	108

4. Trip Distribution



5. Traffic Impact Study Recommendations

Dated: January 2021

Kittelson and Associates prepared a traffic impact study for the proposed Quarry Residential Development. Below is an executive summary of the findings and recommendations presented in the TIS by Kittelson and Associates. The following summary is not the opinion of ITD staff. ITD staff has reviewed the submitted traffic impact study for consistency with ITD policies and practices and may have additional and/or varied requirements beyond what is noted in the summary.

EXECUTIVE SUMMARY

M3 Companies is proposing to develop the Quarry Residential Development, a 779-unit active senior-living residential development situated in Middleton, Idaho. The development also includes RV storage planned for approximately 300 storage units on 10 acres. The development is located directly west of Middleton Road and north of Lincoln Road.

Access to the Quarry Residential Development is proposed via two new accesses onto Middleton Road, one access to Lincoln Road, and one access to KCID Road. The development is planned to be fully built-out by the year 2030. The TIS addresses the following study years and scenarios:

- Existing 2020 traffic conditions
- Background 2030 traffic conditions (without the proposed development)
- Total 2030 traffic conditions with buildout of the development
- Background buildout plus five years (2035) traffic conditions
- Total 2035 traffic conditions with buildout of the development

The results of the traffic impact analysis indicate that the proposed Quarry Residential development can be constructed while maintaining acceptable levels of service on the surrounding transportation system as long as the appropriate mitigations are in place. The findings of this analysis and recommendations are discussed below.

FINDINGS

Existing Conditions

- The study evaluated eight off-site intersections during the a.m. and p.m. peak period of a typical weekday.
- All study intersections were found to operate at acceptable operating standards during the existing weekday a.m. and p.m. peak hours with the exceptions of:
 - **N Middleton Road / SH 44 intersection (AM and PM Peak Hours)**
 - Southbound approach operates at LOS F with a v/c ratio of 0.54 during the weekday a.m. peak hour and 0.43 during the weekday p.m. peak hour.
 - The N Middleton Road / SH 44 intersection would operate at LOS A in the a.m. and p.m. peak hour and would meet City and ITD operation standards with a traffic signal.
 - Restriping the median on SH 44 for a southbound left-turn storage lane was identified as an interim mitigation option. This option results in LOS D operation for the southbound left-turn which meets ITD LOS standards, but not the City of Middleton.
 - **KCID Road / US 20-26 (AM Peak and PM Peak Hours)**

- The southbound approach of the KCID/US 20-26 intersection operates at LOS E (CHD4 operating standard is LOS D) with a v/c ratio of 0.65 in the a.m. peak hour. The northbound approach of the intersection operates at LOS F with a v/c ratio of 0.65 in the p.m. peak hour.
- This intersection would operate at LOS D or better and would meet ITD operating standards with a traffic signal as planned by ITD as part of the US 20-26 widening which is scheduled to be complete by 2022.

Year 2030 Background Traffic Conditions

- Year 2030 background traffic volumes were forecasted using a 3.5% compounded annual growth rate and included the addition of in-process traffic from the Mandalay Ranch Subdivision development.
- The following assumptions were made in the 2030 background traffic conditions due to the planned improvements in the area:
 - Sawtooth Lake Drive - S Cemetery Road roadway connection (City project)
 - Middleton Road Re-Alignment (City project)
 - Includes signalization of the N Middleton Road/SH 44 intersection
 - US 20/26 widening from I-84 to Middleton Road to six lanes (ITD project)
 - Includes signalization of KCID Road/US 20-26 intersection
- Year 2030 background traffic analysis (without inclusion of site-generated traffic) found that all study intersections are expected to operate at acceptable operating standards during the weekday a.m. and p.m. peak hours with the exceptions of:
 - **Cemetery Lane / SH 44 intersection (AM and PM Peak Hours)**
 - Projected to operate at LOS F in the a.m. and p.m. peak hours with a critical movement v/c ratio of 0.81 and 0.65, respectively unsignalized.
 - Through conversations with City staff, the City is aware of the need for a traffic signal at this location by year 2030 and is making efforts to include the project in their CIP. The intersection would operate at LOS A in the a.m. and p.m. peak hour and would meet City and ITD operation standards with a traffic signal.
 - **S Middleton Road / SH 44 (PM Peak Hour)**
 - Intersection V/C ratio is 0.91 and the eastbound through movement has a V/C ratio of 0.95 which are both above the 0.90 ITD threshold
 - Mitigation requires an eastbound right-turn lane. Intersection already has an eastbound right-turn lane.

- **Middleton Road / US 20-26 (AM Peak and PM Peak Hours)**
 - Middleton Road / US 20-26 intersection is expected to operate at LOS F in the a.m. and p.m. peak hours and to be over-capacity in the a.m. peak hour even with the ITD widening that is currently being designed. This is due to the design providing three westbound through lanes but only maintaining the existing single eastbound through lane.
 - The intersection operates at LOS C and meets CHD4 and ITD operating standards with a second eastbound through lane in the a.m. and p.m. peak hours.

Trip Generation and Distribution

- The *ITE Trip Generation Manual, 10th Edition* was used to estimate the trip generation for the proposed Quarry Residential development
- The proposed Quarry Residential development, 779 senior active living homes and 300 RV storage units is estimated to generate a total of 3,526 daily net new trips, 214 net new weekday a.m. peak hour trips (74 inbound / 140 outbound), and 268 weekday p.m. peak hour net new trips (160 inbound / 108 outbound).
- The distribution pattern for site-generated trips was developed by evaluating a select zone analysis from COMPASS' regional travel demand model and review with the City and CHD4 staff.

Year 2030 Total Traffic Conditions

- Year 2030 total traffic conditions found that the site-generated trips do not result in any additional mitigations at off-site intersections beyond the mitigations identified in the 2030 background traffic conditions.

Year 2035 Background Traffic Conditions

- Year 2035 background traffic volumes were forecasted using the same 3.5% compounded annual growth rate and included the addition of in-process traffic from the Mandalay Ranch Subdivision development.
- The year 2035 background analysis assumed the mitigated study intersection lane configurations from the year 2030 background analysis, with the purpose of identifying if any further mitigations would be needed in year 2035
- Year 2035 background traffic analysis (without inclusion of site-generated traffic) found that all study intersections are expected to operate at acceptable operating standards during the weekday a.m. and p.m. peak hours with the year 2030 background mitigations with the exceptions of:
 - **Middleton Road and US 20-26 Intersection (AM and PM Peak Hours):** Over-capacity in the a.m. peak hour and has a v/c ratio of 0.96 in the p.m. peak hour with the mitigated background 2030 lane configurations, which added a second eastbound through lane. Additional mitigation requires a second southbound left-turn lane.

Year 2035 Total Traffic Conditions

- Year 2035 total traffic conditions found that the site-generated trips do not impact any additional intersections not previously identified in the year 2035 background traffic conditions.
- All findings and mitigations required for intersections to meet operational standards outlined under year 2035 background conditions remain consistent in the 2035 total traffic scenario.

RECOMMENDATIONS

Based on the report's analyses and evaluation findings, recommendations were developed accordingly for time scenario's conditions:

- **N Middleton Road / SH 44 intersection:** The City should work with ITD to signalize this intersection with the re-alignment of Middleton Road. This need is not triggered by the proposed development as the deficiency is identified in existing conditions. Site generated traffic contributes 2.2% and 2.4% of the total entering vehicles at the intersection in the a.m. and p.m. peak hours, respectively, in year 2030 conditions.
- **S Middleton Road / SH 44 intersection:** The City should work with ITD to add an eastbound right-turn lane to this intersection. This need is not triggered by the proposed development as the deficiency is identified under year 2030 background conditions. Site generated traffic contributes 1.5% and 1.7% of the total entering vehicles at the intersection in the a.m. and p.m. peak hours, respectively, in year 2030 conditions. Intersection already has eastbound right-turn lane.
- **Cemetery Road / SH 44 intersection:** The City should work with ITD to signalize this intersection. This need is not triggered by the proposed development as the deficiency is identified under year 2030 background conditions. Site generated traffic contributes 1.2% and 1.6% of the total entering vehicles at the intersection in the a.m. and p.m. peak hours, respectively, in year 2030 conditions.
- **US 20-26/KCID Road:** CHD4 should work with ITD to construct a traffic signal at this location with the widening of US 20-26. This need is not triggered by the proposed development as the deficiency is identified in existing conditions and ITD has a signal at this location included in their widening project planned for construction 2022. Site generated traffic contributes 2.0% and 2.1% of the total entering vehicles at the intersection in the a.m. and p.m. peak hours, respectively, in year 2030 conditions.
- **US 20-26/Middleton Road:** ITD should construct a second eastbound lane at this intersection. This need is not triggered by the proposed development as the deficiency is identified under year 2030 background conditions. Site generated traffic contributes 3.8% and 4.5% of the total entering vehicles at the intersection in the a.m. and p.m. peak hours, respectively, in year 2030 conditions.

ITD Proportionate Share Contributions

1. SH-44 & Cemetery Road

**All supporting documentation in Appendix A*

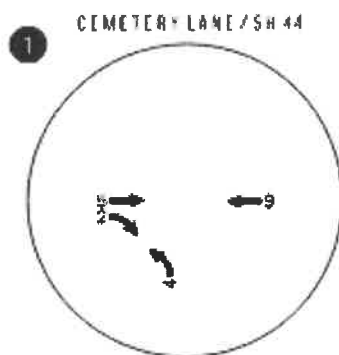
Per the TIS, the intersection fails under two-way-stop-control during the 2030 background and total traffic conditions with LOS F on the northbound and southbound left-turn movements. The TIS proposes a traffic signal, which is already being considered by the City of Middleton, as mitigation—the intersection performs acceptably under the 2030 background and total traffic conditions with the proposed mitigation. Consistent with other intersections on this corridor, ITD will require the construction of an eastbound right-turn lane at this intersection.

Staff estimates the design and construction costs for the signal and turn lane to be approximately \$982,286.

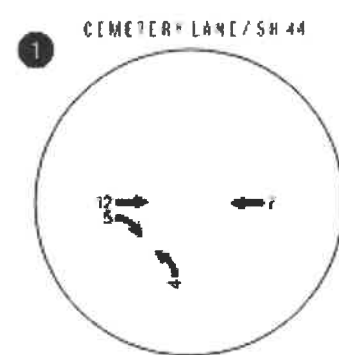
Proportionate share shall be based on the development's site traffic at this intersection versus 2030 total traffic volumes identified in the TIS.

Site Traffic

AM Peak

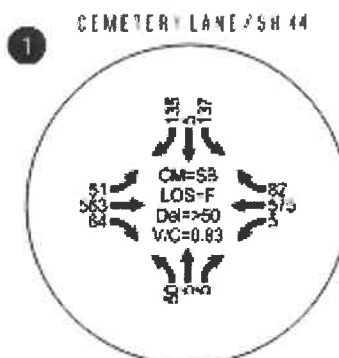


PM Peak

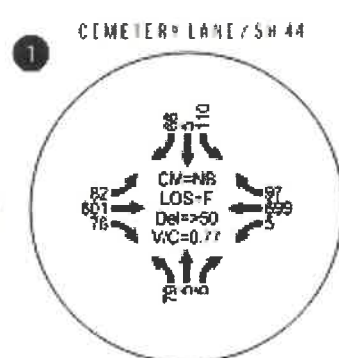


Total 2030 Traffic

AM Peak



PM Peak



Proportionate Share Calculations		
<i>AM Site =</i> 20	<i>AM Total =</i> 1,677	<i>AM % =</i> 1.19
<i>PM Site =</i> 28	<i>PM Total =</i> 1,830	<i>PM % =</i> 1.53
<i>Average Proportionate Share Percentage</i>		<i>Avg % =</i> 1.36
<i>SH-44 & Cemetery Road Cost Estimate</i>		<i>\$982,286</i>
<i>Proportionate Share Contribution</i>		<i>\$13,359</i>

Staff calculates the developer's proportionate share to be \$13,359 (1.36%) based on site trips versus total intersection trips at the 2030 build out year.

2. SH-44 & South Middleton Road

**All supporting documentation in Appendix B*

Per the TIS, the intersection of SH-44 and S Middleton Road operates acceptably in the existing conditions. The TIS shows that constructing an EB right-turn lane is required mitigation to perform acceptably 2030 total traffic conditions, but ITD already reconfigured this intersection to include a dedicated right-turn lane in the summer of 2020—no further mitigation is required.

Staff does not recommend any proportionate share contributions at the intersection of SH-44 and S Middleton Road.

3. SH-44 & North Middleton Road

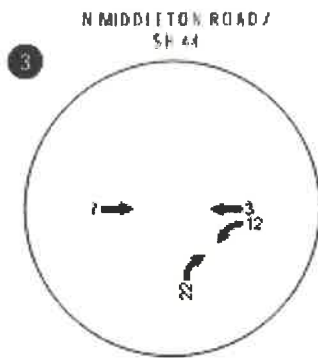
**All supporting documentation in Appendix C*

Per the TIS, the existing two-way-stop-control intersection fails with LOS E or F for the southbound thru and left movements. The TIS proposes a traffic signal, which is already being considered by the City of Middleton, as mitigation—the intersection performs acceptably under the 2030 background and total traffic conditions with the proposed mitigation. Consistent with other intersections on this corridor, ITD will require the construction of an eastbound right-turn lane at this intersection.

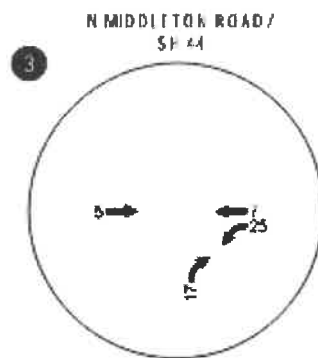
Staff estimates the design and construction costs for construction of the traffic signal and additional turn lanes to be approximately \$1,332,364.

Proportionate share shall be based on the development's site traffic at this intersection versus 2030 total traffic volumes identified in the TIS.

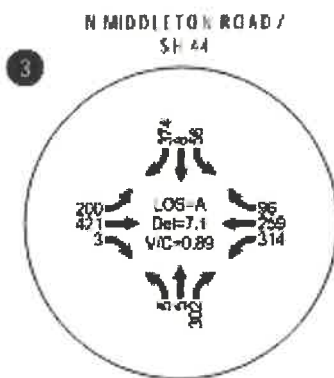
**Site Traffic
AM Peak**



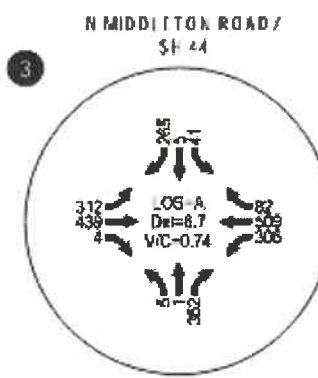
PM Peak



**Total 2030 Traffic
AM Peak**



PM Peak



<i>Proportionate Share Calculations</i>		
<i>AM Site =</i> 44	<i>AM Total =</i> 2,043	<i>AM % =</i> 2.15
<i>PM Site =</i> 54	<i>PM Total =</i> 2,321	<i>PM % =</i> 2.33
<i>Average Proportionate Share Percentage</i>		<i>Avg % =</i> 2.24
<i>SH-44 & N Middleton Road Cost Estimate</i>		<i>\$1,332,364</i>
<i>Proportionate Share Contribution</i>		<i>\$29,845</i>

Staff calculates the developer's proportionate share to be \$29,845 (2.24%) based on site trips versus total intersection trips at the 2030 build out year.

4. US-20/26 & KCID Road

**All supporting documentation in Appendix D*

Per the TIS, the intersection of US-20/26 and KCID Road fails under existing conditions, with the southbound approach operating at LOS E in the AM peak hour and the northbound approach operating at LOS F in the PM peak hour. ITD has a project (KN 22165) scheduled for construction in FY 2022 that will widen US-20/26 to 3 lanes in each direction in this vicinity and will install a traffic signal at the intersection of US-20/26 and KCID Road. With these improvements in place, this TIS shows that this intersection will operate at acceptable LOS and capacity in the 2030 background and 2030 total traffic conditions.

Staff does not recommend any proportionate share contributions at the intersection of US-20/26 and KCID Road.

5. US-20/26 & Middleton Road

**All supporting documentation in Appendix C*

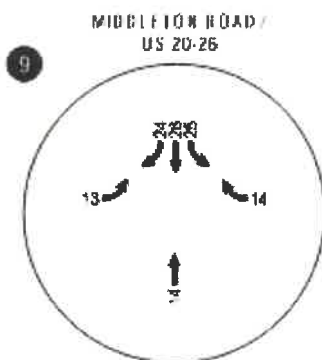
Per the TIS, the signalized intersection of US-20/26 and Middleton Road operates at an acceptable LOS and capacity during the existing year analysis but will fail under the 2030 background traffic conditions at LOS E and F in the AM and PM peak hours and v/c of 1.16 and 0.94 in the AM and PM peak hour, respectively. The TIS analysis shows that a second eastbound thru lane would be adequate mitigation to achieve acceptable LOS and capacity in both the 2030 background condition and the 2030 total traffic condition. Although the next phase of widening for this section of US-20/26 is not yet in the ITIP, the corridor plan includes widening this section to 5 lanes in an interim condition and 6 lanes in the ultimate configuration. The corridor plan also includes constructing a half CFI at the intersection of US-20/26 and Middleton Road.

Staff estimates the design and construction costs for the half CFI to be approximately \$16,239,692.

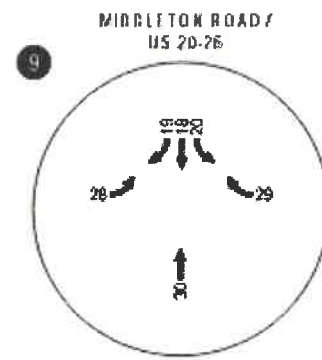
Proportionate share shall be based on the development's site traffic at this intersection versus 2040 traffic volumes identified in the corridor plan. Only PM peak trips are available in the 2040 corridor study.

Site Traffic

AM Peak



PM Peak



Total 2040 Traffic (PM Peak Only) (KN 07826)

Movement	EBT	EBR2	WBT	WBR2	NBL2	NBT	SBL2	SBT	SEL2	NWL2
Volume (vph)	2362	359	2564	194	517	290	116	188	114	639

Proportionate Share Calculations		
<i>AM Site = 116</i>	<i>AM Total = 3,162</i>	<i>AM % = 3.67</i>
<i>PM Site = 144</i>	<i>PM Total = 7,343</i>	<i>PM % = 1.96</i>
Average Proportionate Share Percentage		Avg % = 2.81
<i>US-20/26 & Middleton Road Cost Estimate</i>		<i>\$16,239,692</i>
Proportionate Share Contribution		\$456,335

Staff calculates the developer's proportionate share to be \$456,335 (2.81%) based on site trips versus total intersection trips at the 2040 corridor plan horizon year.

*ITD Staff Recommendations are intended to assure that the proposed development will not place an undue burden on the existing State Highway system within the vicinity impacted by the proposed development.

** Recommendations included in ITD's Staff Technical Report along with any development conditions (see associated Permit Committee Agenda/Minutes) is only valid for the period of one year from the date of the TIS report. ITD reserves the right to request an updated TIS to reflect current traffic conditions if an approved encroachment application and/or proportionate share contribution are not obtained/provided within one year.

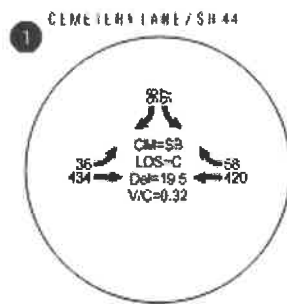
6. Appendices

Appendix A	SH-44 & Cemetery Road
Appendix B	SH-44 & S Middleton Road
Appendix C	SH-44 & N Middleton Road
Appendix D	US-20/26 & KCID Road
Appendix E	US-20/26 & Middleton Road

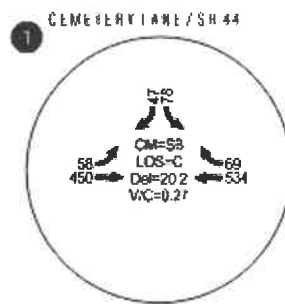
Appendix A

SH-44 & Cemetery Road

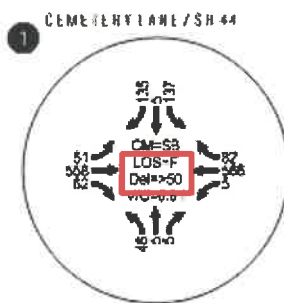
Existing Conditions AM Peak



PM Peak



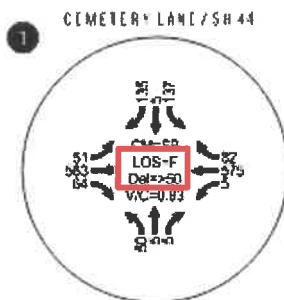
2030 Background Conditions AM Peak



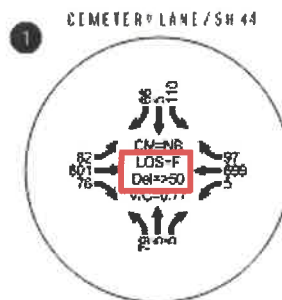
PM Peak



2030 Total Traffic Conditions AM Peak



PM Peak



Intersection

Int Delay, s/veh 9.5

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	51	558	62	5	566	82	46	5	5	137	5	135
Future Vol, veh/h	51	558	62	5	566	82	46	5	5	137	5	135
Conflicting Peds, #/hr	1	0	0	0	0	1	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	75	75	-	-	75	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	85	85	85	85	85	85	85	85	85	85	85	85
Heavy Vehicles, %	0	2	0	0	2	0	0	0	0	0	0	1
Mvmt Flow	60	656	73	6	666	96	54	6	6	161	6	159

Major/Minor	Major1		Major2		Minor1		Minor2					
Conflicting Flow All	763	0	0	729	0	0	1622	1588	693	1498	1528	667
Stage 1	-	-	-	-	-	-	813	813	-	679	679	-
Stage 2	-	-	-	-	-	-	809	775	-	819	849	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.1	6.5	6.2	7.1	6.5	6.21
Critical Hdwy Stg 1	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3	3.5	4	3.309
Pot Cap-1 Maneuver	859	-	-	884	-	-	83	109	447	~102	119	461
Stage 1	-	-	-	-	-	-	375	395	-	445	454	-
Stage 2	-	-	-	-	-	-	377	411	-	372	380	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	858	-	-	884	-	-	~48	95	447	~89	104	461
Mov Cap-2 Maneuver	-	-	-	-	-	-	126	202	-	198	219	-
Stage 1	-	-	-	-	-	-	330	348	-	392	448	-
Stage 2	-	-	-	-	-	-	241	406	-	318	335	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.7	0.1	47.3	45.2
HCM LOS			E	E

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	126	278	858	-	-	884	-	-	198	443
HCM Lane V/C Ratio	0.43	0.042	0.07	-	-	0.007	-	-	0.814	0.372
HCM Control Delay (s)	53.5	18.5	9.5	0	-	9.1	0	-	73	17.9
HCM Lane LOS	F	C	A	A	-	A	A	-	F	C
HCM 95th %tile Q(veh)	1.9	0.1	0.2	-	-	0	-	-	5.8	1.7

Notes

~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection

Int Delay, s/veh 8.9

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	82	589	71	5	692	97	75	5	5	110	5	66
Future Vol, veh/h	82	589	71	5	692	97	75	5	5	110	5	66
Conflicting Peds. #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	75	75	-	-	75	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	1	-	-	1	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	91	91	91	91	91	91	91	91	91	91	91	91
Heavy Vehicles, %	0	1	0	0	2	1	0	0	0	1	0	0
Mvmt Flow	90	647	78	5	760	107	82	5	5	121	5	73

Major/Minor	Major1		Major2		Minor1		Minor2					
Conflicting Flow All	867	0	0	725	0	0	1729	1743	686	1641	1675	760
Stage 1	-	-	-	-	-	-	866	866	-	770	770	-
Stage 2	-	-	-	-	-	-	863	877	-	871	905	-
Critical Hdwy	4.1	-	-	4.1	-	-	7.1	6.5	6.2	7.11	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.1	5.5	-	6.11	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.1	5.5	-	6.11	5.5	-
Follow-up Hdwy	2.2	-	-	2.2	-	-	3.5	4	3.3	3.509	4	3.3
Pot Cap-1 Maneuver	785	-	-	887	-	-	~70	88	451	~80	96	409
Stage 1	-	-	-	-	-	-	351	373	-	395	413	-
Stage 2	-	-	-	-	-	-	352	369	-	347	358	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	785	-	-	887	-	-	~48	70	451	~65	76	409
Mov Cap-2 Maneuver	-	-	-	-	-	-	126	163	-	162	186	-
Stage 1	-	-	-	-	-	-	283	300	-	318	408	-
Stage 2	-	-	-	-	-	-	283	365	-	271	288	-

Approach	EB		WB		NB		SB
HCM Control Delay, s	1.1		0.1		69.5		51.5
HCM LOS					F		F

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	126	239	785	-	-	887	-	-	162	377
HCM Lane V/C Ratio	0.654	0.046	0.115	-	-	0.006	-	-	0.746	0.207
HCM Control Delay (s)	76	20.8	10.2	0	-	9.1	0	-	73.7	17
HCM Lane LOS	F	C	B	A	-	A	A	-	F	C
HCM 95th %ile Q(veh)	3.5	0.1	0.4	-	-	0	-	-	4.6	0.8

Notes

~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Year 2030 Background Traffic Mitigation
























This section highlights the mitigation needed to address each of the intersections not meeting traffic operations standards under year 2030 background conditions. *Appendix I contains year 2030 background mitigated traffic operation and traffic signal worksheets for the intersections outlined below.*

Cemetery Lane / SH 44

The Cemetery Lane / SH 44 intersection is projected to operate at LOS F (City standard is LOS C) in the a.m. and p.m. peak hours with a critical movement v/c ratio of 0.81 and 0.65, respectively. Intersection improvements at this location are not identified in the Cemetery Road to Sawtooth Lake Drive extension in the *City of Middleton CIP*. Through conversations with City staff, the City is aware of the need for a traffic signal at this location by year 2030 and is looking for opportunities to add traffic control at the intersection as part of the extension project or in their future CIP. The intersection would operate at LOS A in the a.m. and p.m. peak hour and would meet City and ITD operation standards with a traffic signal under year 2030 background conditions. A planning-level review of signal warrants indicates that this intersection will likely meet signal warrants under 2030 background traffic conditions.
























HCM 6th Signalized Intersection Summary
1: Cemetery Lane & SH 44

TT 2030 AM - Mitigated
25517 - Wood Residential Development

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	51	563	64	5	575	82	50	5	5	137	5	135
Future Volume (veh/h)	51	563	64	5	575	82	50	5	5	137	5	135
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1900	1870	1870	1900	1870	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	60	662	75	6	676	96	59	6	6	161	6	159
Peak Hour Factor	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85	0.85
Percent Heavy Veh, %	0	2	2	0	2	0	0	0	0	0	0	0
Cap, veh/h	385	822	93	351	931	801	383	184	184	525	12	329
Arrive On Green	0.50	0.50	0.50	0.50	0.50	0.50	0.21	0.21	0.21	0.21	0.21	0.21
Sat Flow, veh/h	709	1650	187	732	1870	1609	1240	872	872	1424	59	1560
Grp Volume(v), veh/h	60	0	737	6	676	96	59	0	12	161	0	165
Grp Sat Flow(s), veh/h/ln	709	0	1836	732	1870	1609	1240	0	1743	1424	0	1619
Q Serve(g_s), s	2.2	0.0	10.4	0.2	8.8	1.0	1.4	0.0	0.2	3.1	0.0	2.8
Cycle Q Clear(g_c), s	11.0	0.0	10.4	10.6	8.8	1.0	4.1	0.0	0.2	3.3	0.0	2.8
Prop In Lane	1.00		0.10	1.00		1.00	1.00		0.50	1.00		0.96
Lane Grp Cap(c), veh/h	385	0	915	351	931	801	383	0	367	525	0	341
V/C Ratio(X)	0.16	0.00	0.81	0.02	0.73	0.12	0.15	0.00	0.03	0.31	0.00	0.48
Avail Cap(c_a), veh/h	445	0	1070	413	1090	937	845	0	1016	1055	0	943
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	10.4	0.0	6.5	10.9	6.1	4.1	12.5	0.0	9.7	11.0	0.0	10.7
Incr Delay (d2), s/veh	0.2	0.0	4.0	0.0	2.0	0.1	0.2	0.0	0.0	0.3	0.0	1.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.3	0.0	2.5	0.0	2.3	0.2	0.3	0.0	0.1	0.8	0.0	0.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	10.6	0.0	10.5	11.0	8.1	4.2	12.7	0.0	9.7	11.3	0.0	11.8
LnGrp LOS	B	A	B	B	A	A	B	A	A	B	A	B
Approach Vol, veh/h		797			778			71			326	
Approach Delay, s/veh		10.5			7.7			12.2			11.6	
Approach LOS		B			A			B			B	
Timer - Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		11.0		19.9		11.0		19.9				
Change Period (Y+Rc), s		4.5		4.5		4.5		4.5				
Max Green Setting (Gmax), s		18.0		18.0		18.0		18.0				
Max Q Clear Time (g_c+I1), s		6.1		13.0		5.3		12.6				
Green Ext Time (p_c), s		0.1		2.3		1.2		2.4				
Intersection Summary												
HCM 6th Ctrl Delay				9.6								
HCM 6th LOS				A								

HCM 6th Signalized Intersection Summary
1: Cemetery Lane & SH 44

12/15/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	82	601	76	5	699	97	79	5	5	110	5	66
Future Volume (veh/h)	82	601	76	5	699	97	79	5	5	110	5	66
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus. Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/in	1900	1885	1885	1900	1870	1885	1900	1900	1900	1885	1900	1900
Adj Flow Rate, veh/h	90	660	84	5	768	107	87	5	5	121	5	73
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	0	1	1	0	2	1	0	0	0	1	0	0
Cap, veh/h	373	890	113	399	1016	868	402	149	149	464	18	261
Arrive On Green	0.54	0.54	0.54	0.54	0.54	0.54	0.17	0.17	0.17	0.17	0.17	0.17
Sat Flow, veh/h	644	1639	209	728	1870	1598	1342	872	872	1416	104	1522
Grp Volume(v), veh/h	90	0	744	5	768	107	87	0	10	121	0	78
Grp Sat Flow(s), veh/h/in	644	0	1848	728	1870	1598	1342	0	1743	1416	0	1626
Q Serve(g_s), s	4.0	0.0	9.7	0.2	10.0	1.0	1.9	0.0	0.2	2.5	0.0	1.3
Cycle Q Clear(g_c), s	14.0	0.0	9.7	9.9	10.0	1.0	3.2	0.0	0.2	2.6	0.0	1.3
Prop In Lane	1.00		0.11	1.00		1.00	1.00		0.50	1.00		0.94
Lane Grp Cap(c), veh/h	373	0	1003	399	1016	868	402	0	299	464	0	279
V/C Ratio(X)	0.24	0.00	0.74	0.01	0.76	0.12	0.22	0.00	0.03	0.26	0.00	0.28
Avail Cap(c_a), veh/h	391	0	1055	420	1068	913	939	0	996	1030	0	929
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	11.0	0.0	5.5	9.3	5.6	3.5	12.8	0.0	10.9	12.0	0.0	11.4
Incr Delay (d2), s/veh	0.3	0.0	2.7	0.0	3.0	0.1	0.3	0.0	0.0	0.3	0.0	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/in	0.4	0.0	1.9	0.0	2.6	0.2	0.5	0.0	0.0	0.6	0.0	0.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	11.3	0.0	8.2	9.3	8.6	3.6	13.0	0.0	10.9	12.3	0.0	11.9
LnGrp LOS	B	A	A	A	A	A	B	A	B	B	A	B
Approach Vol, veh/h	834			880			97			199		
Approach Delay, s/veh	8.6			8.0			12.8			12.1		
Approach LOS	A			A			B			B		
Timer - Assigned Phs	2			4			6			8		
Phs Duration (G+Y+Rc), s	9.9			21.6			9.9			21.6		
Change Period (Y+Rc), s	4.5			4.5			4.5			4.5		
Max Green Setting (Gmax), s	18.0			18.0			18.0			18.0		
Max Q Clear Time (g_c+I1), s	5.2			16.0			4.6			12.0		
Green Ext Time (p_c), s	0.2			1.1			0.6			3.0		
Intersection Summary												
HCM 6th Ctrl Delay	8.9											
HCM 6th LOS	A											

Conceptual Cost Estimate

Estimated By: Regan Hansen
 Checked By:
 Location: SH-44; Cemetery Rd Signal

Date: 5/25/2021
 Date:

Scope:

Construct a traffic signal at the intersection of SH-44 and Cemetery Road. Construct an eastbound right-turn lane on SH-44. Construct sidewalk to tie in to existing sidewalk on the south side of SH-44. Construction of the south leg of Cemetery Road is assumed to be completed by others and this estimate would tie in to the south leg at the back of the curb returns.

Any additional R/W required is assumed to be acquired under project that builds south leg of Cemetery Road by others.

Item	Quantity	Unit Price	Cost	Note
SECTION 1				
205-005A EXCAVATION	35% *		\$ 14,401.21	*301-409
301-005A GRANULAR SUBBASE *	935.00 TON	\$ 17.00	\$ 15,895.00	KN 13492
303-021A 3/4" AGGR TY A FOR BASE *	695.00 TON	\$ 13.94	\$ 9,688.30	KN 13387
405-435A SUPERPAVE HMA PAV INCL ASPH&ADD CL SP-3 *	197.00 TON	\$ 79.00	\$ 15,563.00	KN 13492
614-015A SIDEWALK	222.00 SY	\$ 60.00	\$ 13,320.00	KN 22165
615-256A CURB TYPE 1	150.00 FT	\$ 20.00	\$ 3,000.00	KN 22165
656-005A TRAFFIC SIGNAL INSTALLATION	1.00 LS	\$ 350,000.00	\$ 350,000.00	KN 18872
675-005A SURVEY	1.00 LS	\$ 10,000.00	\$ 10,000.00	KN 19944
SP Traffic Control	1.00 LS	\$ 50,000.00	\$ 50,000.00	
Mobilization	10%		\$ 48,186.75	
SECTION 1 Sub-Total			\$ 530,054.26	
SECTION 2				
CN Change Order / Quantity Variance	5%		\$ 26,502.71	
CN Non-Bid Items	3.5%		\$ 18,551.90	
SECTION 2 Sub-Total			\$ 45,054.61	
SUMMARY				
Sub-Total: SECTION 1 & SECTION 2			\$ 575,108.87	
Contingency - Scoping Level	40%		\$ 230,043.55	
TOTAL CONSTRUCTION COST			\$ 805,152.41	

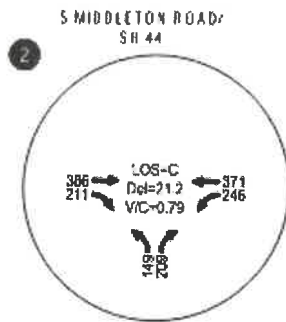
Summary of Project Costs		Amount	Note
Construction		\$ 805,152.41	
Design Services	10%	\$ 80,515.24	
Construction Services	12%	\$ 96,618.29	
Right-of-Way	0.00 ACRES	\$ -	
Total Cost		\$ 982,285.95	

Appendix B

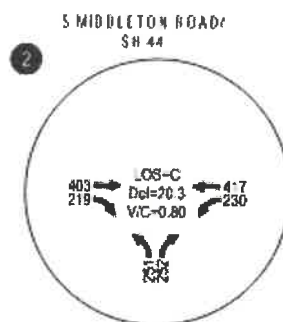
SH-44 & S Middleton Road

Existing Conditions*

AM Peak



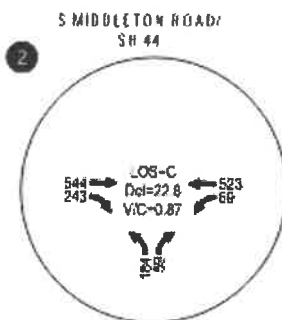
PM Peak



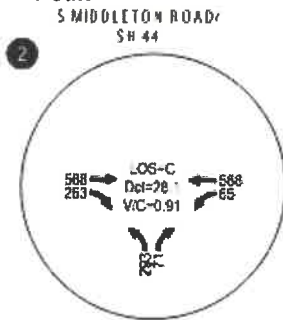
* The metrics shown in these figures are inaccurate because they incorrectly include a combined thru/right lane for eastbound traffic. An existing conditions analysis was not performed with the actual existing lane configuration, but since the actual existing conditions would improve performance and the TIS analysis meets ITD performance criteria, it is clear that the intersection would meet performance criteria with the correct lane configuration.

2030 Background Conditions*

AM Peak



PM Peak



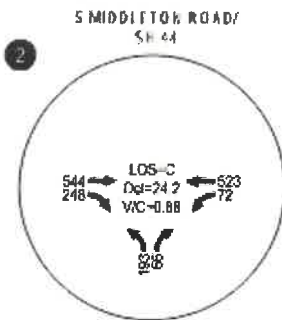
* The metrics shown in these figures are inaccurate because they incorrectly include a combined thru/right lane for eastbound traffic. The "mitigated" analyses are the actual existing lane configuration which includes separate thru and right lanes for eastbound traffic. The analysis results are:

AM: LOS = B, Del = 12.2, V/C = 0.73

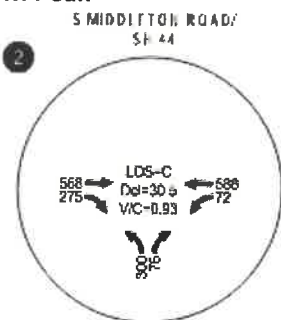
PM: LOS = B, Del = 14.1, V/C = 0.78

2030 Total Traffic Conditions*

AM Peak



PM Peak



* The metrics shown in these figures are inaccurate because they incorrectly include a combined thru/right lane for eastbound traffic. The "mitigated" analyses are the actual existing lane configuration which includes separate thru and right lanes for eastbound traffic. The analysis results are:

AM: LOS = B, Del = 12.5, V/C = 0.74

PM: LOS = B, Del = 14.5, V/C = 0.78

HCM 6th Signalized Intersection Summary 2: S Middleton Road & SH 44

This matches existing configuration.

TT 2030 AM - Mitigated
25517 - Wood Residential Development

	→	↘	↙	←	↖	↗
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑	↑	↑	↑	↑	↑
Traffic Volume (veh/h)	544	248	72	523	193	66
Future Volume (veh/h)	544	248	72	523	193	66
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/in	1870	1870	1870	1885	1856	1841
Adj Flow Rate, veh/h	633	288	84	608	224	77
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86
Percent Heavy Veh, %	2	2	2	1	3	4
Cap, veh/h	766	649	322	1125	290	256
Arrive On Green	0.41	0.41	0.07	0.60	0.16	0.16
Sat Flow, veh/h	1870	1585	1781	1885	1767	1560
Grp Volume(v), veh/h	633	288	84	608	224	77
Grp Sat Flow(s), veh/h/in	1870	1585	1781	1885	1767	1560
Q Serve(g_s), s	15.9	6.9	1.3	10.1	6.4	2.3
Cycle Q Clear(g_c), s	15.9	6.9	1.3	10.1	6.4	2.3
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	766	649	322	1125	290	256
V/C Ratio(X)	0.83	0.44	0.26	0.54	0.77	0.30
Avail Cap(c_a), veh/h	1556	1318	666	1568	624	551
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(f)	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	13.9	11.2	10.0	6.3	21.1	19.3
incr Delay (d2), s/veh	0.9	0.2	0.2	0.2	1.7	0.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%), veh/in	5.9	2.1	0.4	3.0	2.6	0.8
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	14.7	11.4	10.2	6.5	22.7	19.6
LnGrp LOS	B	B	B	A	C	B
Approach Vol, veh/h	921			692	301	
Approach Delay, s/veh	13.7			6.9	21.9	
Approach LOS	B			A	C	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	9.8	27.8			37.6	15.0
Change Period (Y+Rc), s	* 6.3	* 6.2			* 6.2	6.4
Max Green Setting (Gmax), s	* 14	* 44			* 44	18.6
Max Q Clear Time (g_c+l1), s	3.3	17.9			12.1	8.4
Green Ext Time (p_c), s	0.1	3.7			3.0	0.4
Intersection Summary						
HCM 6th Ctrl Delay			12.5			
HCM 6th LOS			B			
Notes						

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

HCM 6th Signalized Intersection Summary

2: S Middleton Road & SH 44

12/15/2020

	→	↘	↙	←	↖	↗
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑	↑	↑	↑	↑	↑
Traffic Volume (veh/h)	568	275	72	588	300	76
Future Volume (veh/h)	568	275	72	588	300	76
Initial Q (Q _b), veh	0	0	0	0	0	0
Ped-Bike Adj(A _{pbT})		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1885	1885	1841	1885	1885	1885
Adj Flow Rate, veh/h	604	293	77	626	319	81
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	1	1	4	1	1	1
Cap, veh/h	733	620	299	1062	382	340
Arrive On Green	0.39	0.39	0.06	0.56	0.21	0.21
Sat Flow, veh/h	1885	1594	1753	1885	1795	1598
Grp Volume(v), veh/h	604	293	77	626	319	81
Grp Sat Flow(s), veh/h/ln	1885	1594	1753	1885	1795	1598
Q Serve(g _s), s	16.2	7.7	1.3	12.2	9.6	2.4
Cycle Q Clear(g _c), s	16.2	7.7	1.3	12.2	9.6	2.4
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	733	620	299	1062	382	340
V/C Ratio(X)	0.82	0.47	0.26	0.59	0.83	0.24
Avail Cap(c _a), veh/h	1468	1241	617	1468	594	528
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	15.5	12.9	11.1	8.0	21.2	18.4
Incr Delay (d ₂), s/veh	0.9	0.2	0.2	0.2	3.4	0.1
Initial Q Delay(d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%), veh/ln	6.3	2.5	0.4	4.0	4.1	0.8
Unsig. Movement Delay, s/veh						
LnGrp Delay(d), s/veh	16.4	13.1	11.3	8.2	24.6	18.5
LnGrp LOS	B	B	B	A	C	B
Approach Vol, veh/h	897			703	400	
Approach Delay, s/veh	15.3			8.6	23.3	
Approach LOS	B			A	C	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	9.8	28.1			37.9	18.4
Change Period (Y+Rc), s	* 6.3	* 6.2			* 6.2	6.4
Max Green Setting (Gmax), s	* 14	* 44			* 44	18.6
Max Q Clear Time (g _c +1), s	3.3	18.2			14.2	11.6
Green Ext Time (p _c), s	0.1	3.5			3.1	0.4

Intersection Summary

HCM 8th Ctrl Delay	14.5
HCM 6th LOS	B

Notes

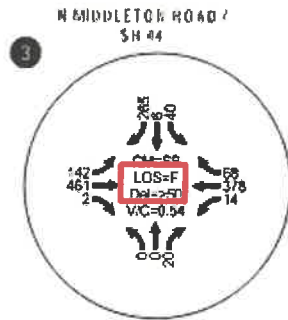
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Appendix C

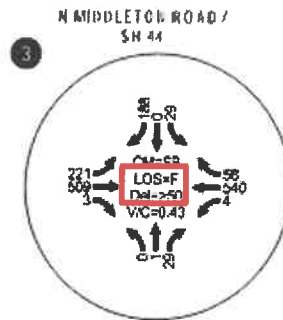
SH-44 & N Middleton Road

Existing Conditions

AM Peak

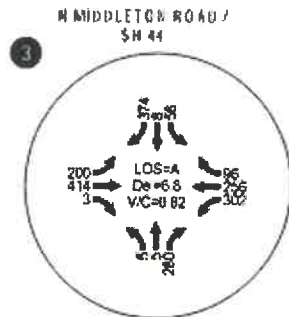


PM Peak

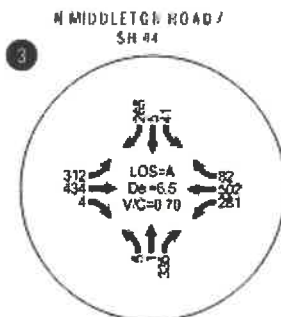


2030 Background Conditions*

AM Peak



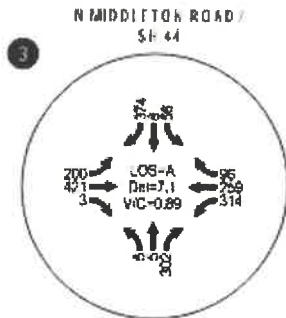
PM Peak



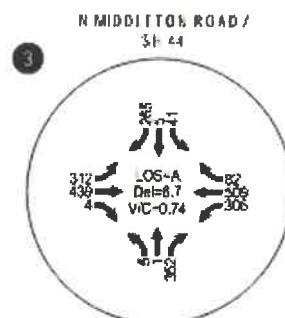
* The analysis shown in these figures includes mitigation, which includes the construction of a traffic signal and the addition of NB left- and NB-right turn lanes.

2030 Total Traffic Conditions*

AM Peak



PM Peak



* The analysis shown in these figures includes mitigation, which includes the construction of a traffic signal and the addition of NB left- and NB-right turn lanes.

Intersection												
Int Delay, s/veh	7.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↰	↱			↰	↱		↰	↱	↰	↱	↰
Traffic Vol, veh/h	142	461	2	14	378	68	0	0	20	40	6	265
Future Vol, veh/h	142	461	2	14	378	68	0	0	20	40	6	265
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	Yield
Storage Length	150	-	-	-	-	100	-	-	-	100	-	150
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	81	81	81	81	81	81	81	81	81	81	81	81
Heavy Vehicles, %	1	3	0	0	2	2	0	0	0	0	0	0
Mvmt Flow	175	569	2	17	467	84	0	0	25	49	7	327

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	551	0	0	571	0	0	1467	1505	570	1434	1422	467
Stage 1	-	-	-	-	-	-	920	920	-	501	501	-
Stage 2	-	-	-	-	-	-	547	585	-	933	921	-
Critical Hdwy	4.11	-	-	4.1	-	-	7.1	6.5	6.2	7.1	6.5	6.2
Critical Hdwy Stg 1	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Follow-up Hdwy	2.209	-	-	2.2	-	-	3.5	4	3.3	3.5	4	3.3
Pot Cap-1 Maneuver	1024	-	-	1012	-	-	107	122	525	113	137	600
Stage 1	-	-	-	-	-	-	327	352	-	556	546	-
Stage 2	-	-	-	-	-	-	525	501	-	322	352	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1024	-	-	1012	-	-	39	99	525	92	111	600
Mov Cap-2 Maneuver	-	-	-	-	-	-	39	99	-	92	111	-
Stage 1	-	-	-	-	-	-	271	292	-	461	532	-
Stage 2	-	-	-	-	-	-	230	488	-	254	292	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	2.2	0.3	12.2	26.7
HCM LOS			B	D

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2	SBLn3
Capacity (veh/h)	525	1024	-	-	1012	-	-	92	111	600
HCM Lane V/C Ratio	0.047	0.171	-	-	0.017	-	-	0.537	0.067	0.545
HCM Control Delay (s)	12.2	9.2	-	-	8.6	0	-	82.4	39.7	18
HCM Lane LOS	B	A	-	-	A	A	-	F	E	C
HCM 95th %tile Q(veh)	0.1	0.6	-	-	0.1	-	-	2.4	0.2	3.3

Intersection												
Int Delay, s/veh	5.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↰	↱			↰	↱		↰		↰	↱	↰
Traffic Vol, veh/h	221	509	3	4	540	58	0	1	29	29	0	188
Future Vol, veh/h	221	509	3	4	540	58	0	1	29	29	0	188
Conflicting Peds, #/hr	3	0	2	2	0	3	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	Yield
Storage Length	150	-	-	-	-	100	-	-	-	100	-	150
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	97	97	97	97	97	97	97	97	97	97	97	97
Heavy Vehicles, %	1	1	0	0	3	0	0	0	3	0	0	1
Mvmt Flow	228	525	3	4	557	60	0	1	30	30	0	194

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	620	0	0	530	0	0	1580	1613	529	1566	1554	560
Stage 1	-	-	-	-	-	-	985	985	-	568	568	-
Stage 2	-	-	-	-	-	-	595	628	-	998	986	-
Critical Hdwy	4.11	-	-	4.1	-	-	7.1	6.5	6.23	7.1	6.5	6.21
Critical Hdwy Stg 1	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.1	5.5	-	6.1	5.5	-
Follow-up Hdwy	2.209	-	-	2.2	-	-	3.5	4	3.327	3.5	4	3.309
Pot Cap-1 Maneuver	965	-	-	1048	-	-	89	105	548	91	114	530
Stage 1	-	-	-	-	-	-	301	329	-	511	510	-
Stage 2	-	-	-	-	-	-	494	479	-	296	328	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	962	-	-	1046	-	-	46	79	547	69	86	528
Mov Cap-2 Maneuver	-	-	-	-	-	-	46	79	-	69	86	-
Stage 1	-	-	-	-	-	-	229	250	-	389	505	-
Stage 2	-	-	-	-	-	-	311	475	-	213	250	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	3	0.1	13.4	25.9
HCM LOS			B	D

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2	SBLn3
Capacity (veh/h)	457	962	-	-	1046	-	-	69	-	528
HCM Lane V/C Ratio	0.068	0.237	-	-	0.004	-	-	0.433	-	0.367
HCM Control Delay (s)	13.4	9.9	-	-	8.5	0	-	92.2	0	15.7
HCM Lane LOS	B	A	-	-	A	A	-	F	A	C
HCM 95th %tile Q(veh)	0.2	0.9	-	-	0	-	-	1.7	-	1.7

Existing Traffic Conditions Mitigation
























This section identifies study intersections that do not meet agency operating standards under existing conditions and the mitigation required to meet those operating standards. *Appendix E includes the existing conditions mitigations Synchro worksheets.*

N Middleton Road / SH 44

The N Middleton Road / SH 44 intersection southbound approach operates at LOS F (City standard is LOS C) and with a v/c ratio of 0.54 during the weekday a.m. peak hour and 0.43 during the weekday p.m. peak hour under existing conditions. Therefore, while the southbound approach is under capacity, the delay for the southbound left-turn movement is very high due to the lack of gaps eastbound and westbound through movements on SH 44. The Middleton Road Re-alignment is identified in the *City of Middleton CIP* for construction in approximately 2026, and through conversations with City staff, is anticipated to include signalization of the N Middleton Road / SH 44 intersection. The N Middleton Road / SH 44 intersection would operate at LOS A in the a.m. and p.m. peak hour and would meet City and ITD operation standards with a traffic signal under existing conditions. Based on a planning level review of signal warrants, this intersection should meet peak hour traffic signal warrants.

HCM 6th Signalized Intersection Summary 3: N Middleton Road & SH 44

BK 2030 AM
25517 - Wood Residential Development

													
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations													
Traffic Volume (veh/h)	200	414	3	302	256	96	5	5	280	56	8	374	
Future Volume (veh/h)	200	414	3	302	256	96	5	5	280	56	8	374	
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0	
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Work Zone On Approach	No			No			No			No			
Adj Sat Flow, veh/h/ln	1885	1856	1856	1900	1870	1870	1900	1900	1900	1900	1900	1900	
Adj Flow Rate, veh/h	247	511	4	373	316	119	6	6	0	69	10	0	
Peak Hour Factor	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	
Percent Heavy Veh, %	1	3	3	0	2	2	0	0	0	0	0	0	
Cap, veh/h	680	1034	8	580	1052	892	442	297		445	297		
Arrive On Green	0.56	0.56	0.56	0.56	0.56	0.56	0.16	0.16	0.00	0.16	0.16	0.00	
Sat Flow, veh/h	961	1839	14	900	1870	1585	1427	1900	1610	1432	1900	1610	
Grp Volume(v), veh/h	247	0	515	373	316	119	6	6	0	69	10	0	
Grp Sat Flow(s) veh/h/ln	961	0	1853	900	1870	1585	1427	1900	1610	1432	1900	1610	
Q Serve(g_s), s	5.8	0.0	5.4	12.6	2.8	1.1	0.1	0.1	0.0	1.4	0.1	0.0	
Cycle Q Clear(g_c), s	8.7	0.0	5.4	18.0	2.8	1.1	0.3	0.1	0.0	1.5	0.1	0.0	
Prop In Lane	1.00		0.01	1.00		1.00	1.00		1.00	1.00		1.00	
Lane Grp Cap(c), veh/h	680	0	1042	580	1052	892	442	297		445	297		
V/C Ratio(X)	0.36	0.00	0.49	0.64	0.30	0.13	0.01	0.02		0.16	0.03		
Avail Cap(c_a), veh/h	680	0	1042	580	1052	892	1021	1069		1027	1069		
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	
Uniform Delay (d), s/veh	6.0	0.0	4.2	10.0	3.7	3.3	11.6	11.4	0.0	12.0	11.5	0.0	
Incr Delay (d2), s/veh	0.3	0.0	0.4	2.4	0.2	0.1	0.0	0.0	0.0	0.2	0.0	0.0	
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
%ile BackOfQ(50%),veh/ln	0.6	0.0	0.6	1.6	0.2	0.1	0.0	0.0	0.0	0.4	0.1	0.0	
Unsig. Movement Delay, s/veh													
LnGrp Delay(d),s/veh	6.3	0.0	4.6	12.4	3.8	3.4	11.6	11.5	0.0	12.2	11.5	0.0	
LnGrp LOS	A	A	A	B	A	A	B	B		B	B		
Approach Vol, veh/h	762			808			12			A	79		A
Approach Delay, s/veh	5.2			7.7			11.5			12.1			
Approach LOS	A			A			B			B			
Timer - Assigned Phs	2			4			6			8			
Phs Duration (G+Y+Rc), s	9.5			22.5			9.5			22.5			
Change Period (Y+Rc), s	4.5			4.5			4.5			4.5			
Max Green Setting (Gmax), s	18.0			18.0			18.0			18.0			
Max Q Clear Time (g_c+I1), s	2.3			10.7			3.5			20.0			
Green Ext Time (p_c), s	0.0			2.7			0.1			0.0			

Intersection Summary























HCM 6th Ctrl Delay 6.8
HCM 6th LOS A

Notes

Unsignalized Delay for [NBR, SBR] is excluded from calculations of the approach delay and intersection delay.

HCM 6th Signalized Intersection Summary 3: N Middleton Road & SH 44

12/16/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	312	434	4	281	502	82	5	1	335	41	5	265
Future Volume (veh/h)	312	434	4	281	502	82	5	1	335	41	5	265
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1885	1885	1885	1900	1856	1900	1900	1900	1856	1900	1900	1885
Adj Flow Rate, veh/h	322	447	4	290	518	85	5	1	0	42	5	0
Peak Hour Factor	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Percent Heavy Veh, %	1	1	1	0	3	0	0	0	3	0	0	1
Cap, veh/h	548	1049	9	630	1044	903	446	297		449	297	
Arrive On Green	0.56	0.56	0.56	0.56	0.56	0.56	0.16	0.16	0.00	0.16	0.16	0.00
Sat Flow, veh/h	822	1865	17	953	1856	1606	1434	1900	1572	1439	1900	1598
Grp Volume(v), veh/h	322	0	451	290	518	85	5	1	0	42	5	0
Grp Sat Flow(s), veh/h/ln	822	0	1882	953	1856	1606	1434	1900	1572	1439	1900	1598
Q Serve(g_s), s	12.5	0.0	4.4	8.1	5.4	0.8	0.1	0.0	0.0	0.8	0.1	0.0
Cycle Q Clear(g_c), s	17.9	0.0	4.4	12.5	5.4	0.8	0.2	0.0	0.0	0.8	0.1	0.0
Prop In Lane	1.00		0.01	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	548	0	1059	630	1044	903	446	297		449	297	
V/C Ratio(X)	0.59	0.00	0.43	0.46	0.50	0.09	0.01	0.00		0.09	0.02	
Avail Cap(c_a), veh/h	548	0	1059	630	1044	903	1028	1069		1034	1069	
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	9.7	0.0	4.0	7.6	4.2	3.2	11.5	11.4	0.0	11.7	11.4	0.0
Incr Delay (d2), s/veh	1.6	0.0	0.3	0.5	0.4	0.0	0.0	0.0	0.0	0.1	0.0	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.5	0.0	0.5	0.7	0.3	0.0	0.0	0.0	0.0	0.2	0.0	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	11.3	0.0	4.3	8.1	4.6	3.3	11.5	11.4	0.0	11.8	11.4	0.0
LnGrp LOS	B	A	A	A	A	A	B	B		B	B	
Approach Vol, veh/h	773			893			6			47		
Approach Delay, s/veh	7.2			5.6			11.5			11.8		
Approach LOS	A			A			B			B		
Timer - Assigned Phs	2			4			6			8		
Phs Duration (G+Y+Rc), s	9.5			22.5			9.5			22.5		
Change Period (Y+Rc), s	4.5			4.5			4.5			4.5		
Max Green Setting (Gmax), s	18.0			18.0			18.0			18.0		
Max Q Clear Time (g_c+I1), s	2.2			19.9			2.8			14.5		
Green Ext Time (p_c), s	0.0			0.0			0.1			1.6		
Intersection Summary												
HCM 6th Ctrl Delay	6.5											
HCM 6th LOS	A											

Intersection Widening - Conceptual Cost Estimate

Estimated By: E. Bowen Date: 11/6/2020
 Checked By: Date:

Location: Intersection of SH-44 and N. Middleton Road

Scope: Installation of a signal, along with an eastbound right turn lane, northbound right turn lane, northbound left turn lane and southbound right turn lane. Extension of the westbound right turn lane to meet design standards. Approximately 1 acre R/W needed.

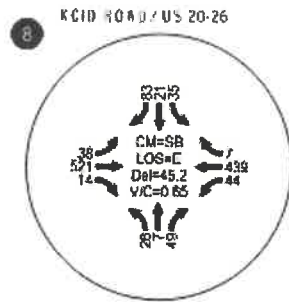
Item Description	Quantity	Unit Price	Cost	Source
SECTION 1				
201-010A CLEARING & GRUBBING	1.12 ACRE	\$ 6,833.00	\$ 7,675.11	KN 18872
203-015A REM OF BITUMINOUS SURF	991.70 SY	\$ 9.67	\$ 9,589.77	KN 18872
203-130A REMOVAL OF PAV MARKINGS	5365.00 FT	\$ 1.00	\$ 5,365.00	KN 18852
205-005A EXCAVATION	3305.68 CY	\$ 15.25	\$ 50,411.60	KN 18872
301-005A GRANULAR SUBBASE	2834.05 TON	\$ 15.90	\$ 45,061.37	KN 18872
303-022A 3/4" AGGR TY 8 FOR BASE	2513.23 TON	\$ 18.30	\$ 45,992.13	KN 18872
401-020A CSS-1 DIL EMUL ASPH FOR TACK COAT	529.48 GAL	\$ 3.48	\$ 1,842.61	KN 18872
405-435A SUPERPAVE HMA PAV INCL ASPH&BD CL SP-3	1419.68 TON	\$ 70.00	\$ 99,377.61	KN 18872
614-015A SIDEWALK	53.89 SY	\$ 80.00	\$ 4,311.11	KN 20294
614-025A CURB RAMP	40.00 SY	\$ 193.50	\$ 7,740.00	KN 19965
615-492A CURB & GUTTER TYPE 2	157.00 FT	\$ 50.00	\$ 7,850.00	KN 20294
621-005A SEED BED PREPARATION	0.63 ACRE	\$ 1,529.67	\$ 968.03	KN 18872
621-010A SEEDING	0.63 ACRE	\$ 1,669.33	\$ 1,052.62	KN 18872
615-651A TRAFFIC SEPARATOR TYPE 1	0.00 FT	\$ 20.00	\$ -	KN 19965
630-020A TRANSVERSE, WORD, SYMBOL, ARROW PAV MKG --	1200.00 SF	\$ 3.40	\$ 4,080.00	KN 13962
630-025A LONGITUDINAL PAVEMENT MARKING	8976.31 FT	\$ 1.05	\$ 9,425.13	KN 18872
656-005A TRAF SIGNAL INSTALLATION	1.00 LS	\$ 229,000.00	\$ 229,000.00	KN 18872
675-005A SURVEY	1.00 LS	\$ 20,000.00	\$ 20,000.00	KN 18872
Traffic Control	1.00 LS	\$ 73,000.00	\$ 73,000.00	KN 18872
Miscellaneous Minor Items	5%		\$ 31,137.10	
Mobilization	10%		\$ 62,274.21	
SECTION 1 Sub-Total			\$ 716,153.41	
SECTION 2				
CM Change Order / Quantity Variance	5%		\$ 35,807.67	
CM Non-Bid Items	3.5%		\$ 25,065.37	
SECTION 2 Sub-Total			\$ 60,873.04	
SUMMARY				
Sub-Total: SECTION 1 & SECTION 2			\$ 777,026.46	
Contingency - Scoping Level	30%		\$ 233,107.94	
TOTAL CONSTRUCTION COST			\$ 1,010,134.39	

Summary of Project Costs		Amount
Construction		\$ 1,010,134.39
Design Services	10%	\$ 101,013.44
Construction Services	12%	\$ 121,216.13
Right-of-Way	1.00 ACRES	\$ 100,000.00
Total Cost		\$ 1,332,363.96

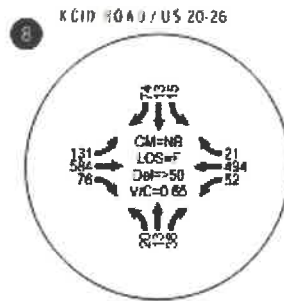
Appendix D

US-20/26 & KCID Road

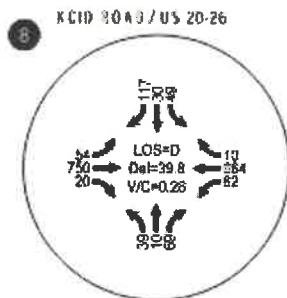
Existing Conditions AM Peak



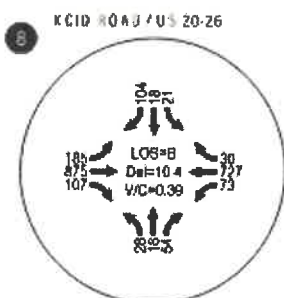
PM Peak



2030 Background Conditions* AM Peak

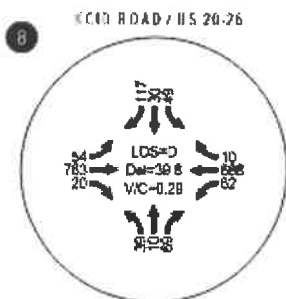


PM Peak

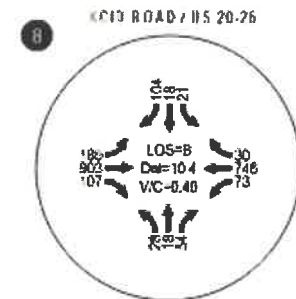


* The analysis shown in these figures includes mitigation, which includes the construction of a traffic signal, US-20/26 widening to 3 thru, 1 left-turn, and 1 right-turn lane in each direction, and KCID Road widening to 1 thru, 1 left-turn, and 1 right-turn lane in each direction.

2030 Total Traffic Conditions* AM Peak



PM Peak



* The analysis shown in these figures includes mitigation, which includes the construction of a traffic signal, US-20/26 widening to 3 thru, 1 left-turn, and 1 right-turn lane in each direction, and KCID Road widening to 1 thru, 1 left-turn, and 1 right-turn lane in each direction.

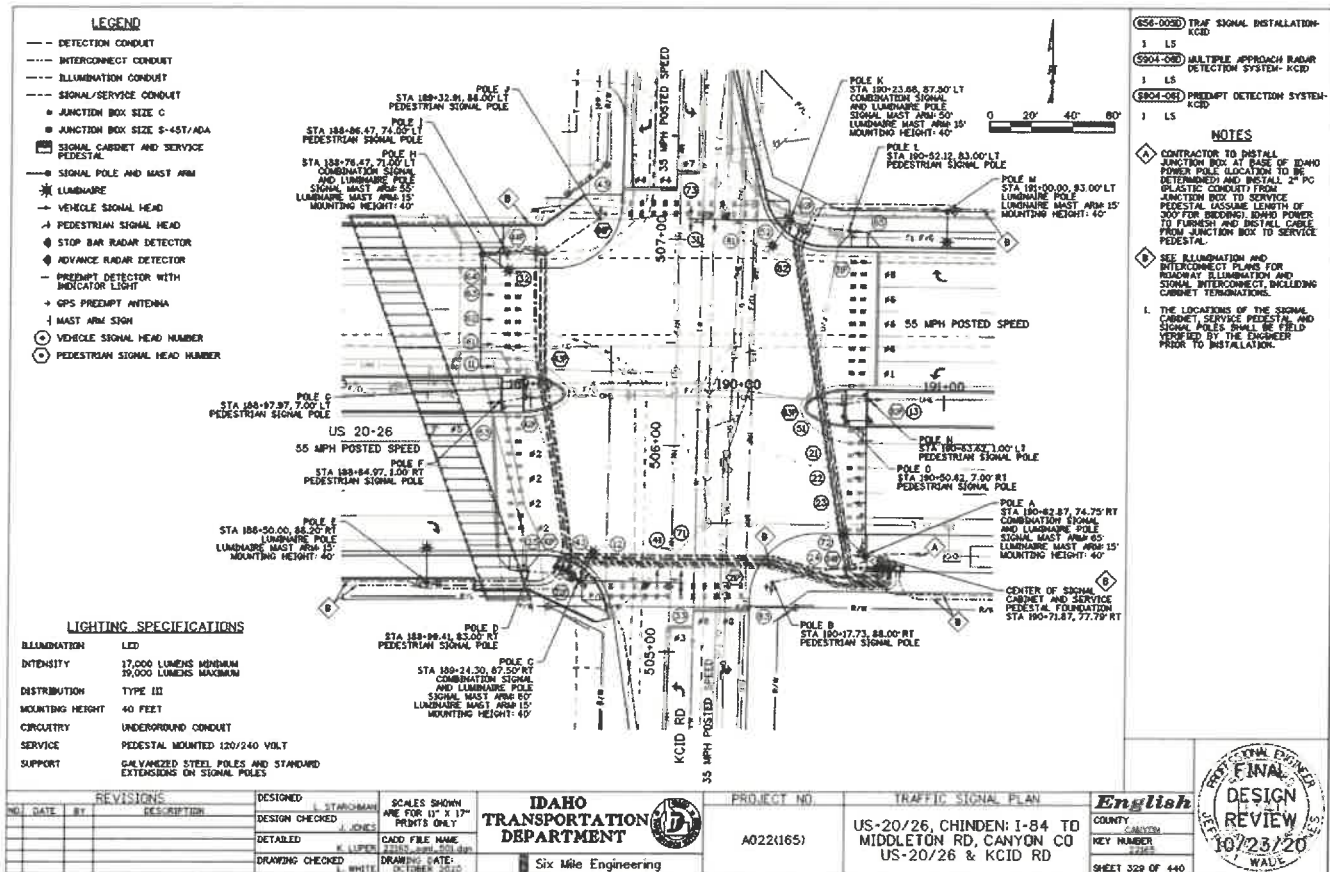
Existing Traffic Conditions Mitigation

This section identifies study intersections that do not meet agency operating standards under existing conditions and the mitigation required to meet those operating standards. *Appendix E includes the existing conditions mitigations Synchro worksheets.*

KCID Road / US 20-26

























The southbound approach of the KCID/US 20-26 intersection operates at LOS E (CHD4 and ITD operating standards are LOS D) with a v/c ratio of 0.65 in the a.m. peak hour. The northbound approach of the intersection operates at LOS F with a v/c ratio of 0.65 in the p.m. peak hour. This intersection is programmed in ITD's Statewide Transportation Improvement Program (STIP) (Reference 4) for signalization within the next 10 years as part of ITD's widening of US 20-26. This intersection would operate at LOS D or better and would meet ITD operating standards with a traffic signal. Based on a planning level review of signal warrants, this intersection should meet peak hour traffic signal warrants.

ITD KN 22165
































HCM 6th Signalized Intersection Summary
8: Kcid Road & US 20-26

BK 2030 AM
25517 - Wood Residential Development

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	54	750	20	62	664	10	39	10	69	49	30	117
Future Volume (veh/h)	54	750	20	62	664	10	39	10	69	49	30	117
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1900	1841	1900	1900	1811	1900	1900	1900	1900	1900	1900	1900
Adj Flow Rate, veh/h	60	833	22	69	738	11	43	11	77	54	33	130
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	0	4	0	0	6	0	0	0	0	0	0	0
Cap, veh/h	78	1097	352	90	1111	362	821	1177	998	881	1177	998
Arrive On Green	0.04	0.22	0.22	0.05	0.22	0.22	0.62	0.62	0.62	0.62	0.62	0.62
Sat Flow, veh/h	1810	5025	1610	1810	4944	1610	1242	1900	1610	1330	1900	1610
Grp Volume(v), veh/h	60	833	22	69	738	11	43	11	77	54	33	130
Grp Sat Flow(s), veh/h/ln	1810	1675	1610	1810	1648	1610	1242	1900	1610	1330	1900	1610
Q Serve(g_s), s	3.9	18.6	1.3	4.5	16.3	0.6	1.7	0.3	2.3	1.9	0.8	4.0
Cycle Q Clear(g_c), s	3.9	18.6	1.3	4.5	16.3	0.6	2.5	0.3	2.3	2.2	0.8	4.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	78	1097	352	90	1111	362	821	1177	998	881	1177	998
V/C Ratio(X)	0.77	0.76	0.06	0.77	0.66	0.03	0.05	0.01	0.08	0.06	0.03	0.13
Avail Cap(c_a), veh/h	294	2157	691	309	2163	704	821	1177	998	881	1177	998
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	0.97	0.97	0.97	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	56.8	43.9	37.2	56.3	42.4	36.3	9.3	8.7	9.1	9.2	8.8	9.4
Incr Delay (d2), s/veh	14.2	1.1	0.1	12.5	0.7	0.0	0.1	0.0	0.2	0.1	0.0	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.0	7.5	0.5	2.3	6.4	0.2	0.5	0.1	0.7	0.6	0.3	1.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	71.0	45.1	37.2	68.8	43.1	36.4	9.4	8.7	9.3	9.3	8.9	9.7
LnGrp LOS	E	D	D	E	D	D	A	A	A	A	A	A
Approach Vol, veh/h	915			818			131			217		
Approach Delay, s/veh	46.6			45.2			9.3			9.5		
Approach LOS	D			D			A			A		
Timer - Assigned Phs	2		3	4		6	7	8				
Phs Duration (G+Y+Rc), s	78.8		10.5	30.7		78.8	9.7	31.5				
Change Period (Y+Rc), s	4.5		4.5	4.5		4.5	4.5	4.5				
Max Green Setting (Gmax), s	34.5		20.5	51.5		34.5	19.5	52.5				
Max Q Clear Time (g_c+I1), s	4.5		6.5	20.6		6.0	5.9	18.3				
Green Ext Time (p_c), s	0.4		0.1	5.6		0.7	0.1	4.9				
Intersection Summary												
HCM 6th Ctrl Delay	39.8											
HCM 6th LOS	D											

HCM 6th Signalized Intersection Summary 8: Kcid Road & US 20-26

12/16/2020

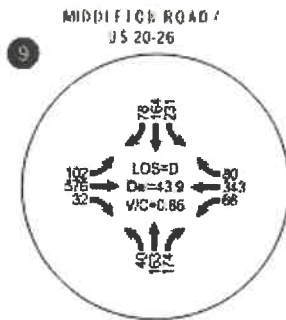
												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		  			  						 	
Traffic Volume (veh/h)	185	875	107	73	727	30	28	18	54	21	18	104
Future Volume (veh/h)	185	875	107	73	727	30	28	18	54	21	18	104
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1900	1870	1900	1870	1870	1900	1900	1900	1856	1900	1900	1900
Adj Flow Rate, veh/h	197	931	114	78	773	32	30	19	57	22	19	111
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	0	2	0	2	2	0	0	0	3	0	0	0
Cap, veh/h	271	2043	643	135	1666	524	374	269	223	383	269	228
Arrive On Green	0.15	0.40	0.40	0.08	0.33	0.33	0.14	0.14	0.14	0.14	0.14	0.14
Sat Flow, veh/h	1810	5106	1608	1781	5106	1608	1280	1900	1572	1344	1900	1610
Grp Volume(v), veh/h	197	931	114	78	773	32	30	19	57	22	19	111
Grp Sat Flow(s),veh/h/ln	1810	1702	1608	1781	1702	1608	1280	1900	1572	1344	1900	1610
Q Serve(g_s), s	3.7	4.7	1.6	1.5	4.2	0.5	0.7	0.3	1.1	0.5	0.3	2.2
Cycle Q Clear(g_c), s	3.7	4.7	1.6	1.5	4.2	0.5	1.0	0.3	1.1	0.8	0.3	2.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	271	2043	643	135	1666	524	374	269	223	383	269	228
W/C Ratio(X)	0.73	0.46	0.18	0.58	0.46	0.06	0.08	0.07	0.26	0.06	0.07	0.49
Avail Cap(c_a), veh/h	1871	8318	2620	1035	6004	1890	1227	1534	1270	1278	1534	1300
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	14.3	7.8	6.8	15.8	9.4	8.2	13.6	13.1	13.5	13.5	13.1	14.0
Incr Delay (d2), s/veh	3.7	0.2	0.1	3.9	0.2	0.0	0.1	0.1	0.6	0.1	0.1	1.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.2	0.7	0.3	0.6	0.8	0.1	0.2	0.1	0.3	0.1	0.1	0.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	18.0	7.9	7.0	19.6	9.6	8.2	13.7	13.2	14.1	13.5	13.2	15.6
LnGrp LOS	B	A	A	B	A	A	B	B	B	B	B	B
Approach Vol, veh/h	1242			883			106			152		
Approach Delay, s/veh	9.4			10.5			13.8			15.0		
Approach LOS	A			B			B			B		
Timer - Assigned Phs	2		3	4		6	7	8				
Phs Duration (G+Y+Rc), s	9.5		7.2	18.6		9.5	9.8	16.0				
Change Period (Y+Rc), s	4.5		4.5	4.5		4.5	4.5	4.5				
Max Green Setting (Gmax), s	28.5		20.5	57.5		28.5	36.5	41.5				
Max Q Clear Time (g_c+I1), s	3.1		3.5	6.7		4.2	5.7	6.2				
Green Ext Time (p_c), s	0.3		0.1	7.1		0.5	0.5	5.2				
Intersection Summary												
HCM 6th Ctrl Delay	10.4											
HCM 6th LOS	B											

Appendix E

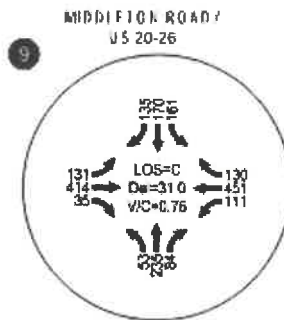
US-20/26 & Middleton Road

Existing Conditions

AM Peak

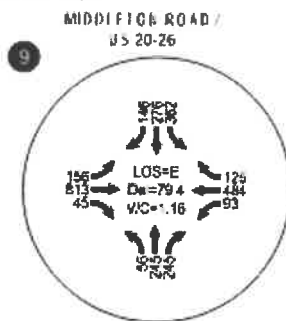


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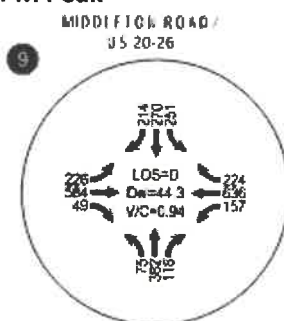


2030 Background Conditions

AM Peak

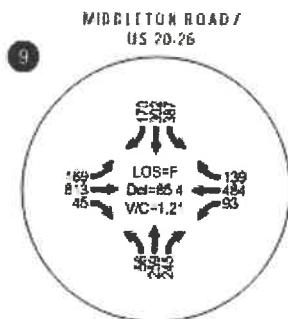


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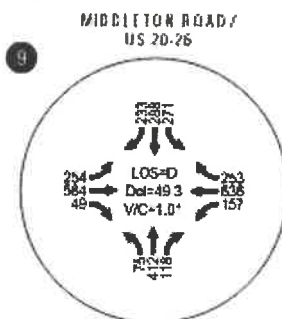


2030 Total Traffic Conditions

AM Peak



























PM Peak



HCM 6th Signalized Intersection Summary 9: Middleton Road & US 20-26

BK 2030 AM
25517 - Wood Residential Development

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	156	813	45	93	484	125	56	245	245	362	276	146
Future Volume (veh/h)	156	813	45	93	484	125	56	245	245	362	276	146
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1870	1811	1811	1870	1796	1841	1900	1841	1870	1841	1885	1885
Adj Flow Rate, veh/h	173	903	50	103	538	139	62	272	272	402	307	162
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	6	6	2	7	4	0	4	2	4	1	1
Cap, veh/h	196	814	690	119	1992	634	260	338	291	343	561	476
Arrive On Green	0.11	0.45	0.45	0.07	0.41	0.41	0.03	0.18	0.18	0.15	0.30	0.30
Sat Flow, veh/h	1781	1811	1535	1781	4904	1560	1810	1841	1585	1753	1885	1598
Grp Volume(v), veh/h	173	903	50	103	538	139	62	272	272	402	307	162
Grp Sat Flow(s), veh/h/ln	1781	1811	1535	1781	1635	1560	1810	1841	1585	1753	1885	1598
Q Serve(g_s), s	14.3	67.0	2.8	8.5	10.9	8.7	4.1	21.1	25.2	22.0	20.4	11.8
Cycle Q Clear(g_c), s	14.3	67.0	2.8	8.5	10.9	8.7	4.1	21.1	25.2	22.0	20.4	11.8
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	196	814	690	119	1992	634	260	338	291	343	561	476
V/C Ratio(X)	0.88	1.11	0.07	0.86	0.27	0.22	0.24	0.80	0.93	1.17	0.55	0.34
Avail Cap(c_a), veh/h	406	814	690	119	1992	634	260	349	301	343	573	485
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	65.4	41.1	23.4	68.9	29.5	28.9	47.3	58.3	60.0	43.7	43.9	40.9
Incr Delay (d2), s/veh	5.0	66.1	0.1	42.0	0.2	0.4	0.2	11.4	33.6	104.2	0.6	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	6.6	42.7	1.0	5.2	4.2	0.1	1.8	10.6	12.5	19.9	9.4	4.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	70.4	107.1	23.5	110.8	29.7	29.2	47.5	69.7	93.6	148.0	44.5	41.1
LnGrp LOS	E	F	C	F	C	C	D	E	F	F	D	D
Approach Vol, veh/h	1126			780			606			871		
Approach Delay, s/veh	97.7			40.3			78.2			91.6		
Approach LOS	F			D			E			F		
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	10.0	51.1	21.4	66.6	27.0	34.1	15.0	73.0				
Change Period (Y+Rc), s	5.0	6.7	5.0	* 6	5.0	6.7	5.0	6.0				
Max Green Setting (Gmax), s	5.0	45.3	34.0	* 44	22.0	28.3	10.0	67.0				
Max Q Clear Time (g_c+I1), s	6.1	22.4	16.3	12.9	24.0	27.2	10.5	69.0				
Green Ext Time (p_c), s	0.0	1.2	0.2	7.8	0.0	0.2	0.0	0.0				

Intersection Summary

HCM 6th Ctrl Delay

79.4

HCM 6th LOS

E

























Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

HCM Signalized Intersection Capacity Analysis

9: Middleton Road & US 20-26

12/16/2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	226	584	49	157	636	224	75	382	118	251	270	214
Future Volume (vph)	226	584	49	157	636	224	75	382	118	251	270	214
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	5.0	6.0	6.0	5.0	5.0	5.0	5.0	6.7	6.7	5.0	6.7	6.7
Lane Util. Factor	1.00	1.00	1.00	1.00	0.91	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	0.85	1.00	1.00	0.85	1.00	1.00	0.85	1.00	1.00	0.85
Flt Protected	0.95	1.00	1.00	0.95	1.00	1.00	0.95	1.00	1.00	0.95	1.00	1.00
Satd. Flow (prot)	1770	1845	1615	1805	5085	1583	1770	1900	1524	1752	1900	1583
Flt Permitted	0.32	1.00	1.00	0.95	1.00	1.00	0.59	1.00	1.00	0.13	1.00	1.00
Satd. Flow (perm)	605	1845	1615	1805	5085	1583	1091	1900	1524	249	1900	1583
Peak-hour factor, PHF	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95	0.95
Adj. Flow (vph)	238	615	52	165	669	236	79	402	124	264	284	225
RTOR Reduction (vph)	0	0	33	0	0	147	0	0	94	0	0	138
Lane Group Flow (vph)	238	615	19	165	669	89	79	402	30	264	284	87
Heavy Vehicles (%)	2%	3%	0%	0%	2%	2%	2%	0%	6%	3%	0%	2%
Turn Type	pm+pt	NA	Perm	Prot	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm
Protected Phases	3	8		7	4		1	6		5	2	
Permitted Phases	8		8			4	6		6	2		2
Actuated Green, G (s)	64.6	50.4	50.4	14.6	51.8	51.8	38.0	32.9	32.9	54.1	44.0	44.0
Effective Green, g (s)	64.6	50.4	50.4	14.6	51.8	51.8	38.0	32.9	32.9	54.1	44.0	44.0
Actuated g/C Ratio	0.47	0.37	0.37	0.11	0.38	0.38	0.28	0.24	0.24	0.40	0.32	0.32
Clearance Time (s)	5.0	6.0	6.0	5.0	5.0	5.0	5.0	6.7	6.7	5.0	6.7	6.7
Vehicle Extension (s)	2.0	5.0	5.0	2.0	5.0	5.0	2.0	2.0	2.0	2.0	2.0	2.0
Lane Grp Cap (vph)	406	679	595	192	1925	599	328	456	366	276	611	509
v/s Ratio Prot	0.06	c0.33		c0.09	0.13		0.01	0.21		c0.11	0.15	
v/s Ratio Perm	0.22		0.01			0.06	0.06		0.02	c0.26		0.05
v/c Ratio	0.59	0.91	0.03	0.86	0.35	0.15	0.24	0.88	0.08	0.96	0.46	0.17
Uniform Delay, d1	22.4	40.9	27.6	60.1	30.4	28.0	37.3	50.1	40.2	35.0	37.0	33.3
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	1.4	16.5	0.0	28.8	0.2	0.2	0.1	17.4	0.0	41.7	0.2	0.1
Delay (s)	23.8	57.5	27.7	88.8	30.6	28.2	37.5	67.4	40.3	76.7	37.2	33.4
Level of Service	C	E	C	F	C	C	D	E	D	E	D	C
Approach Delay (s)		46.9			39.1			58.0			49.6	
Approach LOS		D			D			E			D	
Intersection Summary												
HCM 2000 Control Delay		47.0										
HCM 2000 Volume to Capacity ratio		0.94										
Actuated Cycle Length (s)		136.8										
Intersection Capacity Utilization		92.4%										
Analysis Period (min)		15										
c Critical Lane Group												

Conceptual Cost Estimate

Estimated By: Dan Block Date: 10/02/2020
 Checked By: Date:
 Location: US-20/26; Middleton CFI (Segment 002070, MP 27.25)

Scope:

Widen US-20/26 at the Middleton Rd intersection to configure a half continuous flow intersection (CFI). Alignment and lane configurations for the CFI concept are based on the KN 07826 US-20/26 Corridor Study from Eagle Rd to Caldwell.

This estimate assumes that the starting point for construction quantities is the future intersection configuration to be constructed by the 2022 project: 22165 US-20/26, I-84 to Middleton.

Right-of-way needs along US-20/26 are based on the typical 200-FT R/W section shown in the preferred alternative from the KN 07826 Corridor Study. Right-of-way needs along Middleton are estimated based on impacts of the conceptual layout.

Item	Quantity	Unit Price	Cost	Note
SECTION 1				
201-010A CLEARING & GRUBBING	1.00 LS	\$ 10,000.00	\$ 10,000.00	KN 18872
205-005A EXCAVATION	50,220.00 CY	\$ 12.75	\$ 640,305.00	KN 13387
203-015A REM OF BITUMINOUS SURF	8,050.00 SY	\$ 5.64	\$ 45,402.00	KN 20073
203-055A REM OF CONC PAV (median concrete)	1,218.00 SY	\$ 12.61	\$ 15,358.98	KN 12431
203-060A REM OF CONC SIDEWALK	869.00 SY	\$ 10.79	\$ 9,376.51	KN 20798
203-070A REM OF CURB & GUTTER	735.00 FT	\$ 12.75	\$ 9,371.25	KN 13387
301-005A GRANULAR SUBBASE	66,202.00 TON	\$ 15.90	\$ 1,052,611.80	KN 19944
401-020A CSS-1 DIL EMUL ASPH FOR TACK COAT	5,783.00 GAL	\$ 3.24	\$ 18,736.92	KN 19944
405-435A SUPERPAVE HMA PAV INCL ASPH&ADD CL SP-3	15,724.00 TON	\$ 75.00	\$ 1,179,300.00	KN 19944
303-022A 3/4" AGGR TY B FOR BASE	23,959.00 TON	\$ 18.12	\$ 434,137.08	KN 19944
614-015A SIDEWALK	1,378.00 SY	\$ 43.13	\$ 59,433.14	KN 13387
615-258A CURB TYPE 3	8,724.00 FT	\$ 18.50	\$ 161,394.00	KN 19944
615-492A CURB & GUTTER TYPE 2	1,427.00 FT	\$ 20.00	\$ 28,540.00	KN 19086
621-005A SEED BED PREPARATION	2.48 ACRE	\$ 4,000.00	\$ 9,920.00	KN 13387
621-010A SEEDING	2.48 ACRE	\$ 2,400.00	\$ 5,952.00	KN 13387
630-025A LONGITUDINAL PAV MKG- WATERBORNE	132,624.00 FT	\$ 0.15	\$ 19,893.60	KN 20842
640-015A SUBGRADE SEPARATION GEOTEXTILE	48,195.58 SY	\$ 1.50	\$ 72,293.37	KN 19414
656-005A TRAFFIC SIGNAL INSTALLATION	6.00 EA	\$ 170,000.00	\$ 1,020,000.00	KN 19814
675-005A SURVEY	1.00 LS	\$ 75,000.00	\$ 75,000.00	KN 19944
SP MEDIAN CONCRETE	5,066.00 SY	\$ 38.79	\$ 196,510.14	KN 19944
SP BRIDGE (US-20/26 Mason Creek)	1.00 LS	\$ 1,575,000.00	\$ 1,575,000.00	
SP Traffic Control	1.00 LS	\$ 250,000.00	\$ 250,000.00	KN 19944
Mobilization	10%		\$ 688,853.58	
SECTION 1 Sub-Total			\$ 7,577,389.37	
SECTION 2				
CN Change Order / Quantity Variance	5%		\$ 378,869.47	
CN Non-Bid Items	3.5%		\$ 265,208.63	
SECTION 2 Sub-Total			\$ 644,078.10	
SUMMARY				
Sub-Total: SECTION 1 & SECTION 2			\$ 8,221,467.47	
Contingency - Scoping Level			\$ 3,288,586.99	
TOTAL CONSTRUCTION COST			\$ 11,510,054.46	
Summary of Project Costs				
Construction		Amount	Note	
Design Services	10%	\$ 1,151,005.45		
Construction Services	12%	\$ 1,381,206.54		
Right-of-Way		\$ 2,197,425.50		
Total Cost		\$ 16,239,691.94		



Lisa Boyd, Superintendent

5207 S. Montana Avenue
Caldwell, ID 83607
Phone (208) 454-0445
Fax (208) 454-0293

June 1, 2022

Re: Quarry East Subdivision

To Whom It May Concern:

The children within this proposed development would attend the following schools:

- East Canyon Elementary School
- Summitvue Middle School
- Ridgevue High School

Vallivue School District is concerned about the number of students that the Quarry East Subdivision would create for East Canyon Elementary School, which is currently overcapacity. East Canyon Elementary has a capacity for 625 students. The school ended the year with an enrollment of 665 students. In order to absorb the growth, 4 portable classrooms were installed and there is no more space on the campus to add portable classrooms to accommodate a higher enrollment.

Beyond portable buildings to accommodate growth, school districts often shift attendance boundaries to alleviate crowding. Skyway Elementary and Desert Springs Elementary are the next closest schools to the proposed development, and both schools are also overcapacity.

We are also concerned at the high school level. Ridgevue High School has a capacity for 1800 students and is currently educating an average of 1500 students. It is projected that Ridgevue High School will be overcapacity in the next five years.

The Vallivue School Board proposed a bond to alleviate crowding in March 2022. If the bond passed, the district would have been able to build two new elementary schools and secure funds to purchase land for a third high school. Unfortunately, the bond failed.

If the school board chooses to run a bond again this year and it passes, the elementary schools would not open until the fall of 2025. Vallivue School District simply does not have the space for the additional students brought in by this new development. Therefore, we ask that you deny or postpone this request until a new school building is in process.

Due to high enrollment districtwide, Vallivue School District does not allow open enrollment for students residing outside district boundaries.

Sincerely,

A handwritten signature in blue ink, appearing to read "L. Boyd", is written over a light blue horizontal line.

Lisa Boyd
Superintendent



Lisa Boyd, Superintendent

5207 S. Montana Avenue
Caldwell, ID 83607
Phone (208) 454-0445
Fax (208) 454-0293

May 18, 2022

Dear Mayor Rule and Caldwell City Council members,

It has come to my attention, through a subdivision approval notice, that part of the Vallivue School District lies within the Middleton City limits. I was naive in thinking that the area we encompass north of HWY 20/26 was simply part of Canyon County. While I am not thrilled about the new development, I am glad to know that we are part of Middleton, even if just a small area. I am writing because Vallivue is struggling with the incredible amount of growth that is currently happening in our area and I would like to open up communication between Middleton City Leaders and the Vallivue School District.

We do not "belong" to any town, so it is easy for us to be forgotten when development decisions are being made. Early this year I sent the attached letter to the Nampa and City Mayors and City Councils. I apologize for not sending it to you as well. With my letter, I was hoping to simply inform the groups of our current plight and what we are trying desperately to do to minimize the impact it has on our students.

We will make another run to pass our bond for two elementary schools in August. I know that Middleton will be trying to pass one as well. We desperately need to build two elementary schools. But right on the heels of building these two schools, when we are able to build, is a new high school and more elementary schools. Our growth is currently at 9% this year and it doesn't look to be slowing down going into next year. For us, that is around 650 students or an entire school full of students. Adding enrollment that equates to a full school building each year is not manageable. We are swimming hard, but it is upstream as we try to keep up.

I ask that you read the attached letter as it explains our issue in more depth. If you have any questions or concerns please feel free to reach out to myself or one of our Directors, Joey Palmer, as we would be very happy to talk with you. You may meet Joey soon as he attends many of the P&Z and Council meetings when developments in our areas are up for discussion.

Thank you for your time and consideration.

Sincerely,

A handwritten signature in blue ink, appearing to read "L. Boyd", is written over the printed name.

Lisa Boyd, EdS
Superintendent



Lisa Boyd, Superintendent

5207 S. Montana Avenue
Caldwell, ID 83607
Phone (208) 454-0445
Fax (208) 454-0293

February 23, 2022

Dear Mayor Kling, Mayor Wagoner, City Council Members, Planning and Zoning Commissioners, and Economic Development Specialists,

I am writing to you on behalf of the Vallivue School District. Listening to both Nampa and Caldwell Council meetings on February 22nd, I heard multiple requests presented for housing approvals or zoning changes. I appreciated the questions posed by Council members, a few of which inquired about school capacity. These questions helped me realize that while the Vallivue School District has been sending letters of concern to both cities about the impact these developments would have on our local schools, we also need to provide additional information for a more complete understanding of our situation.

The Vallivue School District is made up of 144 square miles, the majority of which used to be farm land. Development is eating up that land at an alarming rate. We are one of the fastest growing school districts in the state. This year alone, Vallivue's student enrollment spiked to 8.3% which is more than 650 students. For comparison, Nampa School District has grown 3.3% and the Caldwell School District has grown .6% this year. Our schools **cannot keep up** with this growth. In fact, Vallivue is behind meeting the demands of this growth in our elementary schools. We have worked to mitigate the impact of this growth over the last five years by adding 46 portable classrooms to our schools. We are now running out of space on our school sites to add even more portable classrooms.

Due to the fact that Vallivue covers two cities--Nampa and Caldwell--as well as some of the county, neighborhoods continue to be approved in ALL areas of our district, from Star Road to Van Slyke Road, from Orchard to areas north of Highway 20/26 reaching to the Boise River. Six of our seven elementary schools are at or over capacity. The only school that is not at capacity is West Canyon, which is to the far west of Caldwell off Pride Lane. We educate over 9,600 students.

As mentioned during both Council meetings, on March 8th we are running a bond for TWO new elementary schools. Honestly, with our growth, we could use three schools right now, but it is doubtful that patrons would approve a bond of that amount. We desperately hope that this bond passes, but as you know, passing a bond will take a 67% approval rate. This is a heavy lift. If the bond passes, these schools will not open until Fall of 2024. This means we will have to endure two more years of rapid building, which will pack our schools even tighter.

During one meeting, a question was posed about Vallivue and open enrollment. Vallivue does not allow for open enrollment to students who are outside of our boundary. We struggle to serve those that live in our boundaries. However, we *do* allow students who are in our district boundary to attend schools in different school zones, *if* there is room in the requested school. Sadly, there is no room. We have nowhere to rezone students to reduce building enrollments. Lakevue Elementary, which is the most recent school impacted by the multiple CBH and Hubble developments, already serves nearly 850 students, which is 125 students over capacity and continues to enroll new students daily.

It was also mentioned in one meeting that we run bonds "about every year." We do not run them every year, but it definitely feels like we do. Due to rapid growth, we have had to bond for a new high school (2013), a new elementary school (2015), a new middle school (2019), and now for two new elementary schools. We wish that there was a way for us to build schools without going through the bond process. It is a stressful and time consuming process. But it is the only process we have. We have to spend time trying to convince patrons that these schools are necessary. However, many patrons are so exhausted by the rapid growth that they vote NO, thinking that it will stop the growth, not realizing that the growth continues, but schools cannot be built. It forces us to cram more students into each classroom. This is not what is best for students, teachers, families or communities. When schools are not built to accommodate students, it makes recruiting and retaining teachers more difficult and forces a higher student-to-teacher ratio, which can compromise the quality of attention and instruction students would otherwise receive.

We have tried to create partnerships with local housing developers. We have met with representatives from the large, local developers, both of whom were kind but showed no interest in assisting Vallivue with land, building support, or the slowing of any developments. Thankfully, Toll Brothers—who own 282+ acres of land in Vallivue's boundaries—reached out to us and offered to provide land for a future elementary school. While we appreciate that gesture, the speed of development may not align with yet another future bond measure, meaning we may need to return the land to the developer.

I want to thank the Council members who spoke up for Vallivue and who were concerned about approving these developments that will land in our lap. We are often forgotten as we are a district without a designated town. So much is out of our hands. We don't determine who gets to build, or how much they get to build, we just do our best to mitigate the impact. Even our efforts to mitigate the impact is out of our hands as it is the patrons who decide whether or not we have space to educate our students. Nevertheless, we will strive to provide top notch education to all of our students whether they are in a brick and mortar school building or working in a \$250,000 portable classroom.

The Nampa/Caldwell community and education are very close to my heart. I was born in Nampa. I was educated in the Nampa School District. I was a teacher and a principal in the Nampa School District. I go to work each day in Caldwell to lead the Vallivue School District. Our community is one I wouldn't trade for anything. Our community is one that values education. While I understand the need for growth, I ask that when you are reviewing development proposals within the Vallivue School District boundaries, that you understand our concern about growth and the strain it is putting on our education system.

Thank you for your time,



Lisa Boyd, EdS.
Superintendent
Vallivue School District

EXHIBIT “D”

Engineering & Planner Comments



January 17, 2022

TO: Roberta Stewart, Planner

FROM: Amy Woodruff, PE, Civil Dynamics PC
City Engineer

RE: Quarry East Subdivision - Preliminary Plat

Thank you for the opportunity to review the above referenced preliminary plat submittal. The plat(s) and packet are very large and encompass a lot of detail. Every effort was made to identify all the review comments, but additional comments may come up as the application goes forward.

PP-01

MCC 5-4-4.2.b. Please add Preliminary Plat in the title block – typical of all sheets.

MCC 5-4-4.2.f. Add benchmark information.

MCC 5-4-4.2.j. Please add ownership information for adjacent parcels.

The SHARED DWY lots is not clear. Are these lots included in BUIDLABLE? Or in addition to?

Revise Not 14 to specify IC 31-3805.b, identify irrigation district and whether lots are subject to assessment.

Note 4. No discharge of stormwater into ponds or co-mingling stormwater with irrigation. Retention facilities (only) required.

Note 7. Delete

Note 9. Revise "*Municipal sewer shall be provided....*"

Note 15. Add "LOMR will be completed to remove lots from flood plain". Confirm with Roberta Stewart if required.

The boundary is required to be stamped by PLS.

Front two (2) lots in four pack configuration to front the street, not front the private drive. Add note.

PP-02

LABEL STREETS. All sheets.

Remove phase lines or revise in some other way. Very difficult to discern project details and the heavy line obscures text and dimensions. All sheets.

Label contours – all sheets.

Dimension roadway and centerline radii. All sheets.

50ft wide half right of way required for Middleton Road. Show Middleton Road improvements in plan view for the full frontage, per the section from Middleton Corridor Study.

Remove callout "ROAD IMPROVEMENT BY OTHERS."



Add north bound left turn lane for the storage units, per CHD4 review comments.

Extend sewer main in L27B2 to sewer L29 and L28.

Identify easement location for irrigation tile along north boundary.

MCC 5-4-4.3.a.b.c. Please plan to connect to the sewer main, terminated in clean out, at the northeast corner of the project. Record drawing attached.

MCC 5-4-4.3.a.b.c. Please plan to connect to the 12" water main in Middleton Road and extend the new 12" main for the full length, north to south, to the south subdivision boundary. Main may locate in streets/common lots closest to Middleton Road.

Extend utility service to L1B9.

Add hydrants in cul-de-sac street.

Streetlights are required along Middleton Road and at locations identified in Middleton Supplemental.

Add streetlight at end of cul-de-sac.

Pretreatment/O/S boxes need to be located near rights of way so they can be accessed by vac truck for cleaning. All sheets and as applicable.

Install Pretreatment/O/S boxes at each catchment. Transporting stormwater through long pipe runs and multiple manholes allows sediment to drop out and accumulate in the pipes. All sheets and as applicable.

Only retention facilities are allowed in City. No discharge to open waters or co-mingling with irrigation and/or groundwater. Please revise stormwater management preliminary design accordingly. All sheets and as applicable.

Many lot dimensions are not legible because of line work and conflicts. Dimension all lots including the access lots and common lots. All sheets.

PP-03

Extend sewer and water main to 11505 Bass Lane parcel. Utility extensions in common lot/L71 or agreed upon other.

Extend sewer and water main to R34083000 at or near L44. . Utility extensions in common lot/L71 or agreed upon other.

What is happening with irrigation water at L39/L38 area and south? Will this all be abandoned or relocated?

Revisit the linework and utility layout shown in L45 and the curve area. It isn't approvable as shown.

PP-04

What is happening with irrigation water along west boundary at L39/L38 area? Will this all be abandoned or relocated?

Please revise the phasing between PP-04 and PP-07 to capture the whole street, both sheets.



Iridum Street does not meet MCC 5-4-10-2.F. An approved turn around at the end needs to be developed or apply for variance from the code.

PP-05

MCC 5-4-4.3.a.b.c. Please plan to connect to the 12" water main in Middleton Road and extend the new 12" main for the full length, north to south, to the south subdivision boundary.

MCC 5-4-4.2.r. Middleton Connects map depicts a roundabout at Middleton and Quarry Street. Please coordinate with the City regarding rights of way needed and constructing the improvement or other mitigation. A roundabout at this location is also required in the Middleton Corridor Study and CHD4 preliminary plat review comment summary. CHD4 also identifies a signal as acceptable.

50ft wide half right of way required for Middleton Road. Show Middleton Road improvements in plan view for the full frontage, per the section from Middleton Corridor Study.

Remove callout "ROAD IMPROVEMENT BY OTHERS."

Extend sewer main in Quarry Street to Middleton Road.

Show the power structures clearly and in relationship with the Middleton Road improvements.

The topography needs to show the elements of the property.

PP-06

MCC 5-4-4.3.a.b.c. Please plan to connect to the 12" water main in Middleton Road and extend the new 12" main for the full length, north to south, to the south subdivision boundary.

50ft wide half right of way required for Middleton Road. Show Middleton Road improvements in plan view for the full frontage, per the section from Middleton Corridor Study.

Remove callout "ROAD IMPROVEMENT BY OTHERS."

Show the power structures clearly and in relationship with the Middleton Road improvements.

Irrigation (all) is required to be relocated out of the right of way.

Extend sewer in L34 to Middleton Road right of way. Extend water to Middleton Road right of way.

PP-07

Iridum and Posta Streets do not meet MCC 5-4-10-2.F. An approved turn around at the end needs to be developed or apply for variance from the code.

Add boxes/structures at the irrigation crossing between L112 and L81.

The stormwater management configuration shown L119 area is not approvable.

Move the match line to south of the street.



PP-08

The City of Middleton Supplemental Specification has guidance for required rights of way dedication at section line/quarter section line road intersections. Please review the proposed right of way dedication for Lincoln Road and revise accordingly. Lincoln is a minor arterial, 36ft to tbc.

Review CHD4 letter regarding moving the geometric centerline of Lincoln south 6ft.

MCC 5-4-4.3.a.b.c. Please plan to install a 12" water main in Siltstone Street and extend the new 12" main for the full length from the project west boundary to Middleton Road. Install 12" extension in Hammerstone to Lincoln.

Extend sewer south in Hammerstone to Lincoln.

Fully show the topography. If there is a well and septic on site at the structure to be removed, identify it, and add the appropriate notes.

PP-09

MCC 5-4-4.2.r. Middleton Connects map depicts a roundabout at Lincoln and Middleton. Please coordinate with the City regarding rights of way needed and construction of improvements or other mitigation.

The Middleton Corridor Study shows a roundabout at the intersection of Lincoln and Middleton Road.

50ft wide half right of way required for Middleton Road. Show Middleton Road improvements in plan view for the full frontage, per the section from Middleton Corridor Study.

What is happening with irrigation water at the southeast corner?

Extend 12" water and sewer south in L47 to Lincoln.

Extend 12" water and sewer in L32 to Middleton Road.

Reconfigure southeast corner of the project to reflect the requirement of Middleton Supplemental, 150 x 150 triangle.

PP-11

LABEL STREETS

The line work is very hard to discern. Please eliminate non-utility lines (as possible) like curb and gutter to add clarity. Perhaps a lighter line weight would be helpful.

PP-12

Revisit 84ft right of way street section. Multiple errors and inconsistencies.

Revisit 70ft right of way street section – left side.

Revise the Middleton Road Widening Section to reflect the Middleton Corridor Study. Include the north bound turn lane. What is planned for stormwater management for Middleton Road?

Add typical section for Lincoln – 36ft to tbc, 50ft half right of way. Confirm with Caldwell and CHD4 the section line/center line configuration.

Emergency Access Section and Shared Access Section – how is stormwater routed using a ribbon curb?



General Comment:

1. Submit to the City (in writing) the plan for secondary emergency access at 31 lots and secondary public access at 51 lots.
2. The Boise River Lift Station will be required to be expanded by Quarry to manage sewage for Quarry East and Quarry West. Add to development agreement or otherwise memorialize the requirement.

[illegible]

Eng	P16-17, 14-19
Fr	P16-17, 14-19
It	P16-17, 14-19
Ja	P16-17, 14-19
Sp	P16-17, 14-19
Ar	P16-17, 14-19
De	P16-17, 14-19
Gr	P16-17, 14-19
He	P16-17, 14-19
Ir	P16-17, 14-19
La	P16-17, 14-19
Li	P16-17, 14-19
Lu	P16-17, 14-19
Pr	P16-17, 14-19
Re	P16-17, 14-19
Rh	P16-17, 14-19
Sb	P16-17, 14-19
Se	P16-17, 14-19
Si	P16-17, 14-19
Te	P16-17, 14-19
Ti	P16-17, 14-19
V	P16-17, 14-19
W	P16-17, 14-19
Xe	P16-17, 14-19
Y	P16-17, 14-19
Zn	P16-17, 14-19
Zr	P16-17, 14-19

Hand-drawn sketch of a person in a dynamic pose, possibly a dancer or athlete. The sketch is on a piece of paper with a dashed line and a date stamp "JAN 28 1962".

Labels and annotations include:

- Top left: "JAN 28 1962"
- Top right: "Hand-drawn sketch of a person in a dynamic pose, possibly a dancer or athlete. The sketch is on a piece of paper with a dashed line and a date stamp 'JAN 28 1962'."
- Center: "Hand-drawn sketch of a person in a dynamic pose, possibly a dancer or athlete. The sketch is on a piece of paper with a dashed line and a date stamp 'JAN 28 1962'."
- Bottom left: "Hand-drawn sketch of a person in a dynamic pose, possibly a dancer or athlete. The sketch is on a piece of paper with a dashed line and a date stamp 'JAN 28 1962'."
- Bottom right: "Hand-drawn sketch of a person in a dynamic pose, possibly a dancer or athlete. The sketch is on a piece of paper with a dashed line and a date stamp 'JAN 28 1962'."

11. 3000

26. $\frac{1}{2} \ln(1 + \cos \theta)$ $\frac{1}{2} \ln(1 + \cos \theta)$ $\frac{1}{2} \ln(1 + \cos \theta)$

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11/11/23 8:08

C1.1

C. J. H. A. DE LANGE - 1977-1978-1979-1980-1981-1982-1983-1984-1985-1986-1987-1988-1989-1990-1991-1992-1993-1994-1995-1996-1997-1998-1999-2000-2001-2002-2003-2004-2005-2006-2007-2008-2009-2010-2011-2012-2013-2014-2015-2016-2017-2018-2019-2020-2021-2022-2023-2024-2025-2026-2027-2028-2029-2030-2031-2032-2033-2034-2035-2036-2037-2038-2039-2040-2041-2042-2043-2044-2045-2046-2047-2048-2049-2050-2051-2052-2053-2054-2055-2056-2057-2058-2059-2060-2061-2062-2063-2064-2065-2066-2067-2068-2069-2070-2071-2072-2073-2074-2075-2076-2077-2078-2079-2080-2081-2082-2083-2084-2085-2086-2087-2088-2089-2090-2091-2092-2093-2094-2095-2096-2097-2098-2099-2100-2101-2102-2103-2104-2105-2106-2107-2108-2109-2110-2111-2112-2113-2114-2115-2116-2117-2118-2119-2120-2121-2122-2123-2124-2125-2126-2127-2128-2129-2130-2131-2132-2133-2134-2135-2136-2137-2138-2139-2140-2141-2142-2143-2144-2145-2146-2147-2148-2149-2150-2151-2152-2153-2154-2155-2156-2157-2158-2159-2160-2161-2162-2163-2164-2165-2166-2167-2168-2169-2170-2171-2172-2173-2174-2175-2176-2177-2178-2179-2180-2181-2182-2183-2184-2185-2186-2187-2188-2189-2190-2191-2192-2193-2194-2195-2196-2197-2198-2199-2200-2201-2202-2203-2204-2205-2206-2207-2208-2209-2210-2211-2212-2213-2214-2215-2216-2217-2218-2219-2220-2221-2222-2223-2224-2225-2226-2227-2228-2229-2230-2231-2232-2233-2234-2235-2236-2237-2238-2239-2240-2241-2242-2243-2244-2245-2246-2247-2248-2249-2250-2251-2252-2253-2254-2255-2256-2257-2258-2259-2260-2261-2262-2263-2264-2265-2266-2267-2268-2269-2270-2271-2272-2273-2274-2275-2276-2277-2278-2279-2280-2281-2282-2283-2284-2285-2286-2287-2288-2289-2290-2291-2292-2293-2294-2295-2296-2297-2298-2299-2300-2301-2302-2303-2304-2305-2306-2307-2308-2309-2310-2311-2312-2313-2314-2315-2316-2317-2318-2319-2320-2321-2322-2323-2324-2325-2326-2327-2328-2329-2330-2331-2332-2333-2334-2335-2336-2337-2338-2339-2340-2341-2342-2343-2344-2345-2346-2347-2348-2349-2350-2351-2352-2353-2354-2355-2356-2357-2358-2359-2360-2361-2362-2363-2364-2365-2366-2367-2368-2369-2370-2371-2372-2373-2374-2375-2376-2377-2378-2379-2380-2381-2382-2383-2384-2385-2386-2387-2388-2389-2390-2391-2392-2393-2394-2395-2396-2397-2398-2399-2400-2401-2402-2403-2404-2405-2406-2407-2408-2409-2410-2411-2412-2413-2414-2415-2416-2417-2418-2419-2420-2421-2422-2423-2424-2425-2426-2427-2428-2429-2430-2431-2432-2433-2434-2435-2436-2437-2438-2439-2440-2441-2442-2443-2444-2445-2446-2447-2448-2449-2450-2451-2452-2453-2454-2455-2456-2457-2458-2459-2460-2461-2462-2463-2464-2465-2466-2467-2468-2469-2470-2471-2472-2473-2474-2475-2476-2477-2478-2479-2480-2481-2482-2483-2484-2485-2486-2487-2488-2489-2490-2491-2492-2493-2494-2495-2496-2497-2498-2499-2500-2501-2502-2503-2504-2505-2506-2507-2508-2509-2510-2511-2512-2513-2514-2515-2516-2517-2518-2519-2520-2521-2522-2523-2524-2525-2526-2527-2528-2529-2530-2531-2532-2533-2534-2535-2536-2537-2538-2539-2540-2541-2542-2543-2544-2545-2546-2547-2548-2549-2550-2551-2552-2553-2554-2555-2556-2557-2558-2559-2560-2561-2562-2563-2564-2565-2566-2567-2568-2569-2570-2571-2572-2573-2574-2575-2576-2577-2578-2579-2580-2581-2582-2583-2584-2585-2586-2587-2588-2589-2590-2591-2592-2593-2594-2595-2596-2597-2598-2599-2600-2601-2602-2603-2604-2605-2606-2607-2608-2609-2610-2611-2612-2613-2614-2615-2616-2617-2618-2619-2620-2621-2622-2623-2624-2625-2626-2627-2628-2629-2630-2631-2632-2633-2634-2635-2636-2637-2638-2639-2640-2641-2642-2643-2644-2645-2646-2647-2648-2649-2650-2651-2652-2653-2654-2655-2656-2657-2658-2659-2660-2661-2662-2663-2664-2665-2666-2667-2668-2669-2670-2671-2672-2673-2674-2675-2676-2677-2678-2679-2680-2681-2682-2683-2684-2685-2686-2687-2688-2689-2690-2691-2692-2693-2694-2695-2696-2697-2698-2699-2700-2701-2702-2703-2704-2705-2706-2707-2708-2709-2710-2711-2712-2713-2714-2715-2716-2717-2718-2719-2720-2721-2722-2723-2724-2725-2726-2727-2728-2729-2730-2731-2732-2733-2734-2735-2736-2737-2738-2739-2740-2741-2742-2743-2744-2745-2746-2747-2748-2749-2750-2751-2752-2753-2754-2755-2756-2757-2758-2759-2760-2761-2762-2763-2764-2765-2766-2767-2768-2769-2770-2771-2772-2773-2774-2775-2776-2777-2778-2779-2780-2781-2782-2783-2784-2785-2786-2787-2788-2789-2790-2791-2792

[illegible]

10	100%
9	90%
8	80%
7	70%
6	60%
5	50%
4	40%
3	30%
2	20%
1	10%
0	0%



- The only additional change to the Linden Road and Ustick Road intersections, beyond the planned roundabouts, is the addition of the four-lane cross-section on Middleton Road (i.e., no additional changes are made to Linden Road or Ustick Road).
- The design speed of the road is anticipated to be reduced over time from 50 mph to 45 mph in most sections south of the Boise River.

The existing and proposed cross-sections of Middleton Road are shown in Figure 9 and Figure 10. Figure 11 illustrates the proposed corridor plan for intersections along Middleton Road. Detailed drawings of the proposed Middleton Road are in Appendix G. The drawings in Appendix G are conceptual. It is expected that the final design of the corridor projects will follow appropriate design standards and that additional details regarding the projects will be developed during the final design phase.

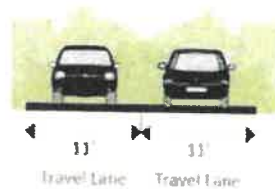


Figure 9. Existing Middleton Road Cross Section

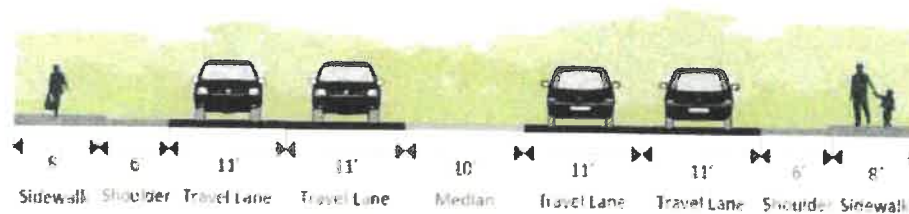


Figure 10. Proposed Middleton Road Cross Section

The following section describes the details of the improvements as well as the operations and safety analysis of these improvements.



February 7, 2022

TO: Roberta Stewart, Planner

FROM: Amy Woodruff, PE, Civil Dynamics PC
City Engineer

RE: Quarry East Subdivision - Preliminary Plat

Thank you for the opportunity to review the above referenced preliminary plat re-submittal.

PP-01

1. The SHARED DWY lots is not clear. Are these lots included in BUIDLABLE? Or in addition to?
2. Revise Not 14 to specify IC 31-3805.b, identify irrigation district and whether lots are subject to assessment. Combine with note 6.
3. **Note 4. No discharge of stormwater into ponds or co-mingling stormwater with irrigation. Retention facilities (only) required.**
4. Note 7. Delete. There is no specific "Right to Farm" in Middleton.
5. Note 15. Add "LOMR will be completed to remove lots from flood plain". Confirm with Roberta Stewart if required.
6. The boundary is required to be stamped by PLS.

PP-02

7. Dimension common lots.
8. Are the power structures in Middleton Road right of way to be moved?
9. North bound left turn lane for the storage units, per CHD4 review comments: Expand note "LEFT TURN LANE TO BE CONSTRUCTED". Or other similar. It reads as it is existing.
10. The plan view of Middleton Road doesn't match the typical section.
11. Add note or otherwise memorialize: Extend utility services to L1B9 – water service and sewer service if an office will be developed. If not then specify no utility service to L1B9.
12. Pretreatment/O/S boxes need to be located near rights of way so they can be accessed by vac truck for cleaning. All sheets and as applicable.
13. Install Pretreatment/O/S boxes at each catchment. Transporting stormwater through long pipe runs and multiple manholes allows sediment to drop out and accumulate in the pipes. All sheets and as applicable.
14. Only retention facilities are allowed in City. No discharge to open waters or co-mingling with irrigation and/or groundwater. Please revise stormwater management preliminary design accordingly. All sheets and as applicable.

PP-03

15. Extend sewer and water main to 11505 Bass Lane parcel. Add note: all utility extensions to adjacent parcels are to boundary or inside right of way. Water is too close to north lot line.



16. What is happening with irrigation water at L39/L38 area and south? Will this all be abandoned or relocated?
17. Revisit the linework and utility layout shown in L45 and the curve area. It isn't approvable as shown. Cross sewer at 90d both ways. Move DI and eliminate the stormdrain pipe crossing water at end of Bismuth Ct.

PP-04

18. What is happening with irrigation water along west boundary at L39/L38 area? Will this all be abandoned or relocated?
19. Please revise the phasing between PP-04 and PP-07 to capture the whole street, both sheets.
20. Private lane Lot 46 needs to be 75ft from intersection.
21. Dimension common lots. Please.

PP-05

22. MCC 5-4-4.2.r. Middleton Connects map depicts a roundabout at Middleton and Quarry Street. Please coordinate with the City regarding rights of way needed and constructing the improvement or other mitigation. A roundabout at this location is also required in the Middleton Corridor Study and CHD4 preliminary plat review comment summary. CHD4 also identifies a signal as acceptable.
23. Extend sewer main in Quarry Street to Middleton Road right of way. May also extend sewer in L22/B1 south area.
24. The plan view of Middleton Road doesn't match the typical section.

PP-06

25. Pretreatment/O/S boxes need to be located near rights of way so they can be accessed by vac truck for cleaning. All sheets and as applicable.
26. Install Pretreatment/O/S boxes at each catchment. Transporting stormwater through long pipe runs and multiple manholes allows sediment to drop out and accumulate in the pipes. All sheets and as applicable.
27. Only retention facilities are allowed in City. No discharge to open waters or co-mingling with irrigation and/or groundwater. Please revise stormwater management preliminary design accordingly. All sheets and as applicable.
28. Extend sewer in L34 to Middleton Road right of way. Extend water to Middleton Road right of way.
29. The plan view of Middleton Road doesn't match the typical section.
30. Dimension common lots. Add actual lot numbers too.
31. The gravity irrigation is shown to terminate near L91 "CONNECT TO EXISTING IRRIGATION PIPE" Where is pipe? The irrigation is open ditch on sheet 7.
32. Need separation from water and sewer in lot fronting L43. Lot needs numbered.

PP-07

33. Pretreatment/O/S boxes need to be located near rights of way so they can be accessed by vac truck for cleaning. All sheets and as applicable.



34. Install Pretreatment/O/S boxes at each catchment. Transporting stormwater through long pipe runs and multiple manholes allows sediment to drop out and accumulate in the pipes. All sheets and as applicable.
35. Only retention facilities are allowed in City. No discharge to open waters or co-mingling with irrigation and/or groundwater. Please revise stormwater management preliminary design accordingly. All sheets and as applicable.

PP-08

36. Lincoln is a minor arterial, 36ft to tbc.
37. MCC 5-4-4.3.a.b.c. Please plan to install a 12" water main in Siltstone Street and extend the new 12" main for the full length from the project west boundary to Middleton Road. Install 12" extension in Hammerstone to Lincoln.
38. Extend sewer south in Hammerstone to Lincoln.
39. Only retention facilities are allowed in City. No discharge to open waters or co-mingling with irrigation and/or groundwater. Please revise stormwater management preliminary design accordingly. All sheets and as applicable.

PP-09

40. MCC 5-4-4.2.r. Middleton Connects map depicts a roundabout at Lincoln and Middleton. Please coordinate with the City regarding rights of way needed and construction of improvements or other mitigation.
41. The Middleton Corridor Study shows a roundabout at the intersection of Lincoln and Middleton Road.
42. The plan view of Middleton Road doesn't match the typical section.
43. Extend 12" water and sewer south between L39 and L40 to Lincoln.
44. Extend 12" water and sewer in L32 to Middleton Road right of way.
45. Dimension and number lots – even common lots.
46. Reconfigure southeast corner of the project to reflect the requirement of Middleton Supplemental, 150 x 150 triangle.
47. The irrigation at L62 can cross at 90d.
48. Eliminate sewer fronting L62. Connect manholes fronting L40.

PP-12

49. Add typical section for Lincoln – 36ft to tbc, 50ft half right of way. The plan view doesn't match cross section.
50. Emergency Access Section and Shared Access Section – how is stormwater routed using a ribbon curb?
51. General Comment:
Submit to the City (in writing) the plan for secondary emergency access at 31 lots and secondary public access at 51 lots.



March 17, 2022

TO: Roberta Stewart, Planner

FROM: Amy Woodruff, PE, Civil Dynamics PC
City Engineer

RE: Quarry East Subdivision - Preliminary Plat

Thank you for the opportunity to review the above referenced preliminary plat re-submittal – plat dated March 7, 2022.

STORMWATER MANAGEMENT: We will recommend the City maintain the stormwater management policy of infiltration on site using retention facilities. Where infiltration on site cannot be achieved, the development may detain the 25-year event and release stormwater to the natural discharge location at the predevelopment rate. All conveyance from the detention facility to the discharge location shall be a grassy swale (or similar) unless otherwise approved.

All facilities shall be designed to convey the 100 year event.

The Homeowner's Association shall be responsible for maintenance of all facilities – both treatment and conveyance.

Please revise the Quarry East stormwater management layout accordingly. Please also note this is a recommendation to City Council. The final policy may be different than summarized above.

PP-01

1. Add "general access" to utility for easement purpose. (new comment)
1. Revise Note 14 to specify IC 31-3805.b, identify irrigation district and whether lots are subject to assessment. Combine with note 6.
2. Note 7. Delete. There is no "Right to Farm" in Middleton.
3. Note 15. Add "LOMR will be completed to remove lots from flood plain". Confirm with Roberta Stewart if required.
4. The boundary is required to be stamped by PLS.

PP-02

5. Dimension common lots.
6. Add note or otherwise memorialize: Extend utility services to L1B9 – water service and sewer service - if an office will be developed. If not, specify no utility service to L1B9.

PP-03

7. Revisit the linework and utility layout shown in L45 and the curve area. It isn't approvable as shown. Cross sewer at 90d both ways. Move DI and eliminate the stormdrain pipe crossing water at end of Bismuth Ct.

PP-04

8. Private lane Lot 46 is in very close proximity to the intersection but doesn't line up. There may be a problem with site distance if a house is constructed on the lot to the south. Needs to be reviewed and confirmed by JUB.
9. Dimension common lots. Please.



PP-05

10. MCC 5-4-4.2.r. Middleton Connects map depicts a roundabout at Middleton and Quarry Street. Please coordinate with the City regarding rights of way needed and constructing the improvement or other mitigation.
11. Extend sewer main in Quarry Street to Middleton Road right of way. May also extend sewer in L22/B1 south area.

PP-06

12. Extend sewer in L34 to Middleton Road right of way. Extend water to Middleton Road right of way.

PP-08

13. Lincoln is a minor arterial, 36ft to tbc. The centerline is 6ft south.
14. Extend sewer south in Hammerstone to Lincoln.

PP-09

15. MCC 5-4-4.2.r. Middleton Connects map depicts a roundabout at Lincoln and Middleton. Please coordinate with the City regarding rights of way needed and construction of improvements or other mitigation.
16. Extend 12" water and sewer south between L39 and L40 to Lincoln.
17. Extend 12" water and sewer in L32 to Middleton Road right of way.
18. Dimension and number lots – even common lots.
19. Reconfigure southeast corner of the project to reflect the requirement of Middleton Supplemental, 150 x 150 triangle.

PP-12

20. Revisit typical section for Lincoln – 36ft to tbc, 50ft half right of way.
21. Emergency Access Section and Shared Access Section – how is stormwater routed using a ribbon curb?
22. General Comment:
Submit to the City (in writing) the plan for secondary emergency access at 31 lots and secondary public access at 51 lots.



March 29, 2022

TO: Roberta Stewart, Planner

FROM: Amy Woodruff, PE, Civil Dynamics PC
City Engineer

A handwritten signature in blue ink, likely belonging to Amy Woodruff, written over the typed name.

RE: Quarry East Subdivision - Preliminary Plat

Thank you for the opportunity to review the above referenced preliminary plat re-submittal – plat dated March 24, 2022.

Review Comments:

1. Remove the linework showing direct discharge to surface water.

Add a note: Stormwater management will utilize on site facilities with infiltration. Where infiltration on site cannot be achieved, the development may detain the 25-year event and release stormwater to the natural discharge location at the predevelopment rate. All conveyance from the detention facility to the discharge location shall be a grassy swale (or similar) unless otherwise approved.

All facilities shall be designed to convey the 100 year event.

2. Add a note: The Homeowner's Association shall be responsible for maintenance of all stormwater management facilities – both treatment and conveyance.
3. Lincoln is a minor arterial.
 - a. Show 50ft right of way dedication from the section line.
 - b. Show centerline 6ft south of section line.
 - c. Show tbc at 36 ft from centerline.
 - d. Revise typical section. Plan view to be revised no later than Council hearing.
4. Reconfigure southeast corner of the project to reflect the requirement of Middleton Supplemental, 150 x 150 triangle. This is a clear code requirement – both roads are designated arterial. We acknowledge the current direction is a roundabout at the south intersection of Lincoln and Middleton, however, the area is still very much in flux and conditions are changing with development. If the south roundabout is advanced at or before the Phase 9 final plat, the right of way set aside shown on the preplat may be utilized for lots.

Show the right of way over the lot layout and add a note/callout: Right of way if required for intersection improvement. If right of way shown is not required for intersection improvements by Phase 9, the right of way dedication for Phase 9 may not include the 150x150 set aside and may be developed as residential lots.



June 16, 2022

TO: Roberta Stewart, Planner

FROM: Amy Woodruff, PE, Civil Dynamics PC
City Engineer

RE: Quarry East Subdivision - Preliminary Plat

Thank you for the opportunity to review the above referenced preliminary plat re-submittal – plat dated May 31, 2022.

Review Comments:

1. Lincoln is a minor arterial.
 - a. Show 50ft right of way dedication from the section line.
 - ~~b. Show centerline 6ft south of section line.~~
 - c. Show tbc at 36 ft from centerline.
 - d. Revise typical section. Plan view to be revised no later than Council hearing.
 - e. If this needs a phone call, please let me know.
2. Expand Note 6. Specify the irrigation district, reference IC 31-3805b, and whether the lots are subject to assessment. If the pressure irrigation source is not surface water by right, then specify the source. If this needs a phone call, please let me know.
- ~~3. Please show water main extension to north boundary at the west end of Lot 1 Block 9.~~
- ~~4. Add plat note specifying the storage unit access will be limited to right in-right out at build out.~~
- ~~5. Add plat note specifying temporary/emergency vehicle access (any) to be at least 660 ft south of E Quarry and 660 ft north of Lincoln west.~~
6. New comment: remove the language “metal power pole to remain” on Lincoln Road. The poles may be an obstruction during future widening and be required to be addressed. The City Engineer cannot authorize an obstruction stay in place.



CITY OF MIDDLETON

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The Quarry East – Planner Comments for Development Agreement & Preliminary Plat stamped 8/17/2021

October 28, 2021

The following comments pertain to the proposed Development Agreement and the revised preliminary plat stamped 8/17/2021.

DEVELOPMENT AGREEMENT COMMENTS: The following comments are just preliminary “general” comments. After further discussion on these items, we’ll hopefully be in a position to agree on language to put into the DA for Council consideration.

1. The current Development Agreement must be separated into two DAs. One will pertain to Quarry East and the other to Quarry West. The projects are too dissimilar to intermingle.
2. Paragraph 3.1 regarding M-U zoning is good.
3. We will want to attach a concept site plan generally showing the proposed development and use types. We will want to add language requiring Developer to construct the project “substantially consistent” with the attached concept plan. The concept plan needs to show where duplex lots are located as opposed to single family detached lots.
4. We also want to attach concept plans showing the elevations of the duplexes and 4 pack cluster homes with language that the homes built on those lots will be “substantially consistent” with the concept elevation pages.
5. We will have to recommend against Paragraph 3.2 regarding re-allocating density. However, Staff can agree to a provision that will allow developer to administratively amend phasing lines (and SLIGHTLY changed lot lines or road configurations if need be). The code requires all developers to take an amended pre-plat through the full public hearing process no matter how small or slight the requested change is. However, Council recently allowed the River Walk developer to employ an administrative procedure to change phasing lines. Quarry can probably obtain the same benefit. The application fee will be \$1000. If the City Engineer deems the changes to lot lines and roadways too significant, then you will have to employ the codified procedure and use the public hearing process.
6. Setbacks, paragraph 3.3.1: As to Duplex setbacks, Staff can recommend all the dimensions set forth except the front setback and streetside setback. Staff will recommend against any front setback that is not at least 20 feet. On street side setbacks, Middleton will want to see at least 15’ so the corner is not crowded by the home. Middleton considers itself a rural community, and it does not want to see houses crowding the frontage of the residential streets.
7. Setbacks for Cluster 4 pack. Until Staff can understand how the homes fit on the lots (e.g., what is the “front” as opposed to the “side”), Staff is hesitant to agree to any setbacks. However, City has indicated that it will not agree to front setbacks less than 25’ for the homes on the main street. Staff could probably agree to a lesser setback for the 2 rear homes since they front the private driveway. But without more of an understanding of what is considered the “front” and what is considered the “side” off the private drive, Staff cannot commit to any setbacks.

8. Setbacks and dimensions for Single Family detached lot, para. 3.3.1: Staff can only agree to a front setback of 25' (or 20' for living space if the garage is set back to at least 25'. This is already code). Side street should be 20'. Staff would be amenable to rear setback of 15' and side setback of 5' even though these dimensions are less than code because City Council has agreed to these narrower setbacks for the 55+ patio homes in the River Walk Subdivision and River Pointe subdivision.
9. Staff will strongly recommend against any single family detached lots less than 6000 sf minimum and 60' wide frontage. In the last few years, City Council has not allowed builders to deviate from the 8000 sf and 75' wide lots in R-3. EXCEPT, recently City Council allowed River Walk and River Pointe to develop 6000 sf and 60' wide lots because the lots were deed restricted 55+. Staff cannot recommend that you get smaller lots or narrower setbacks than was granted to River Walk and River Pointe in their 55+ deed restricted communities.
10. Staff will recommend that the 55+ component of Quarry East be Deed Restricted in compliance with Idaho law. City Council has required this restriction on all 55+ communities to date. Targeting 55+ is not enough.
11. Staff cannot recommend 3.3.2. Staff will recommend that the only permitted uses should be residential duplexes, cluster pack homes, single family detached homes and the mini-warehouse storage. However, Staff would also be agreeable to a sales office as described in 3.3.2.1. No other M-U uses would be allowed. This will be "tightened" by the concept plan showing generally where the different types of residential lots and the storage lot are located. You will not be able to build any "use" that is not specifically shown in the concept plan.
12. Paragraph 3.3.3 for initial building permits: City agrees with most of the language allowing up to 10 model home and community facility building permits before final plat. However, City will want to add a sentence that the model homes and community facilities cannot "operate" as model homes and community facilities until the infrastructure is completed and C of O is issued.
13. Paragraph 3.3.4 – Streets. The City is amenable to private streets. Additionally, City Engineer "preliminarily" stated that she is agreeable to the 47' local road, but that may change as she digs into her review a little more. Caldwell Fire indicated that it must have street width of 26'.
14. City will want to include a street maintenance agreement in the DA to ensure that the Developer, and later the HOA, are maintaining the streets to city standards. City will inspect the Quarry East roads at least once a year (or when numerous complaints are received), and if City determines the HOA is not properly maintaining the roads, the City will have the right to do any improvements to get the roads up to standard. The city will invoice the HOA for any work the City is required to do, and the invoice will include the cost of maintenance and a mark-up to cover the City's oversight cost and inconvenience. Failure to pay the invoice could result in a breach of contract and/or forfeiting ownership of the "private streets". The City will need to find a way to protect the residents in the event the HOA does not keep up its side of the bargain.
15. As to sewer and water, paragraphs 3.4 and 3.5, the City will require you to provide sewer and water like any other developer. We'll have to work our way through this with the help of the City Engineer, Administrator, and lawyers. If you over-size something, you will be entitled to a later-comer fee or other reimbursement that is mutually agreeable to the parties. The City will not agree in the DA to a credit for impact fees. That may be a viable form of reimbursement and the City may agree to that at a later date, but the City will not agree to that exact form of reimbursement in the DA.
16. Paragraph 3.8 on URD is acceptable but City will not agree to the URD language in paragraph 3.4 regarding Domestic Water. The city will not agree that water infrastructure in the URD "will" be reimbursed by URD. The City is a separate entity than URD and cannot make such

an agreement on behalf of the URD. Developer will have to apply to the URD for reimbursement, and any reimbursement is strictly between developer and URD.

17. As to Exhibit "E" Approved Modifications to Code, the City makes the following comments:

- a. The City will not agree to modify 1-14-5 for phase development. The City will only recommend a flexible administrative amendment to phase lines as noted above in item #5.
- b. Revisions to 4-1-1.H Driveways is good.
- c. Staff will not support the request for changes to 5-4-7.A.3 on bonding. Recent history indicates that City Council will not allow any bonding other than is in the current code. In other words, Council will not allow bonding for anything other than landscape, irrigation, and fencing.
- d. Staff will support revisions to 5-4-10-2 on roads.
- e. Staff will support revisions to cul de sac code 5-4-10-2E
- f. Staff will recommend radius turns of 96' (Caldwell fire number) rather than code required 125'. If knuckles are built into the road, City engineer may allow developer to go smaller than 96'.
- g. Staff will support lengthening block length to 1700 lf. The configuration of the large lakes requires this type of modification.
- h. Staff is ok with changes to 5-4-10-4.A on Lot design
- i. Staff is ok with changes to 5-4-10-4.C, particularly since private lanes set forth in the code take care of a lot of these issues.

18. We will want to add these further provisions to the DA

- j. Developer shall be required to construct all frontage improvements on Lincoln Road and Middleton Road and dedicate the same to the City. There is a multi-lane roundabout planned for the intersection of Lincoln and Middleton Road. The intersection is already failing. 900 homes in Quarry will overwhelm that intersection to an intolerable degree. City will require Quarry to build the roundabout, and City will reimburse Developer for the proportionate amount not attributable to Developer. That reimbursement may be via a credit for impact fees or some other method mutually agreeable to the parties.
- k. Developer will construct Middleton Road to the standards of the 2016 Middleton Road corridor study or better. If you want a better landscape buffer or sidewalk, we would certainly back you on that.
- l. Recognize that the Canyon County functional classification map requires a "major collector" to be built east to west through, what appears to be, the center of Quarry West and East. This issue must be addressed with the County and CHD4.
- m. Developer will pay all City required proportionate share traffic fees. These fees are in addition to the codified "Traffic Impact Fees" collected at building permit via the Mid-Star Traffic Impact CIP Schedule.
- n. Developer shall bring Phase 1 to final plat within 2 years of the preliminary plat approval (or 3 years with a 1-year extension request). If Developer does not achieve final plat for phase 1 within the time frame, City has the option to terminate the entire DA and the property will revert to R-3 zoning or possibly M-1 zoning (TBD).
- o. After final plat for phase 1, Developer to bring each phase to final plat every two years (or 3 years with a 1 year extension). If Developer fails to do so, the portions of the preliminary plat still yet to be final platted will become null and void.
- p. Per our Code, a storage facility is allowed in the M-U zone only with a special use permit, but we can take care of the storage unit via the DA instead. No SUP required.

We will want to still treat the DA like a SUP, so we will need to include information on type and size of storage facility, hours, elevations...etc., to make sure it is suitable for the surrounding area. All the terms will simply be written into the DA to avoid a separate application for SUP.

- q. Developer will complete a LOMR to remove all affected lots out of the floodplain.
- r. Add description of each amenity, e.g., clubhouse, pool, park facility...etc.
- s. Sewer and water capacity will not be reserved until construction drawings are approved by the City.
- t. All utilities are public, and a City access easement will be applied to all city utilities.
- u. Our attorney will revise the DA to ensure that the HOA, as an assignee, is responsible to abide by the terms of the DA, particularly with respect to road maintenance and paying invoices if the City is forced to step in and maintain the roads.

PRELIMINARY PLAT COMMENTS FOR QUARRY EAST:

1. A second access and the timing of construction is an issue. If multi-family housing is involved, Caldwell Fire will go up to 100 homes before the 2nd access is required. The City Code is 50 homes. A discussion must take place with Fire officials and City Engineer to determine whether emergency accesses will help alleviate this issue. However, all agreed that no emergency access can be attached to Middleton Road.
2. Pick another name for Walcott "Place" in Phase 3 because it is too disjointed from the other "Walcott" street to the south. Also, do not use "Place". In Middleton, a road can only end in "Street" (E/W) or "Avenue" (N/S) or "Court" (cul de sac).
3. The Loop road in Phase 5 should have only 1 street name, so pick either "Marl", "Cotta" or "Shale" Avenue and stick with the single name for the entire loop. House numbering will be a nightmare otherwise.
4. Caldwell Fire is concerned about the length of the cul de sacs and lack of loop roads. Loop roads help with connectivity and circulation while cul de sacs can be troublesome unless the dimensions are adequate.
5. Staff will recommend against any single family lots smaller than 6000 sf and 60' wide, and Staff will only recommend these dimensions in the 55+ deed restricted portion of Quarry East. Add a note to the pre-plat indicating the lot size limitation and also note that the community is 55+ deed restricted.
6. Show the community center and park facilities on the plat.
7. Add note that the plat is subject to Development Agreement, Instrument # _____
8. Change Note 12 regarding R-3 setbacks. State instead, "Building setbacks and dimensional standards shall be in compliance with the standards set forth in the Development Agreement, Instrument # _____."
9. Remove Note 16. There are exceptions.
10. Rewrite Note 18 to state that the utility easement along the front of the lots is 10' "wide". There is also a 10' wide easement along the rear lot lines for utilities and irrigation. Finally, add a note that all interior/side boundaries have a 5' easement, each side of the boundary, for utilities, drainage, and irrigation.
11. Add: "Sewer and water capacity shall be reserved at the time the Construction Drawings are approved by the City."
12. Add Note: "Any existing wells shall be abandoned per IDWR regulations."
13. Add Note: "All existing septic systems shall be completely removed and the area recompacted per geotechnical report."

14. Add Note: "A LOMR (Letter of Map Revision) will be completed to remove all affected lots from the floodplain on any particular phase prior to the issuance of any building permits for that phase."
15. Submit a landscape plan.
16. Reserve all your street names and subdivision name with Tony Almeida at Canyon County (talmeida@canyonco.org)
17. Show all your mailbox cluster locations.

thanks

Roberta L. Stewart

Middleton City Planner



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The Quarry East – Planner Comments for “Another” review of Preliminary Plat stamped 8/17/2021

1/18/2022

1. The “Notes” section needs to call out each “private lane” or “shared driveway” by Lot and block.
2. Small loop roads need only 1 name. So, for loop road off of Hammerstone in Phase 5, choose only one street name, either Marl, Cotta, or Shale and apply to entire loop. In Phase 2 (Block 2), pick only one name for loop off of Cinder (either Diabase or Statham).
3. Add to the DA the “variance” or waiver for dead-end roads that are prohibited in 5-4-10-2(F).
4. Show the street frontage improvements for full multi-lane roundabout at Lincoln Road and Middleton Road as shown on the City’s Comprehensive Plan Maps. The City is requesting that you construct the roundabout due to the large increase of traffic your project will create. We will need to change the DA to provide a time for RAB construction well before Phase 9 where it is currently located. City is discussing adding a DA provision that the multi-lane RAB and connected frontage will be built with current Phase 3 or 4 and will be a condition of approval for final plat of Phase 3 or Phase 4, whichever is chosen.
5. Please change the DA to add a provision that The Quarry has a duty to expand the Boise River Lift Station to help serve the site.
6. Add note that proposed zoning is M-U.
7. All common lot landscape buffers along Middleton Road and Lincoln Road must be 24’ wide per MCC 5-4-10-6. Looks like your large common lots adjacent to the roadway do the trick, but please add dimensions to clarify without a doubt that you are meeting the 24’ minimum width.
8. Increase the width of the landscape buffer between the storage unit lot and the residential lots to 25’. This is a recent update to the Middleton City Code that will be approved on 1/19/2022 by City Council. Currently you show only about 15’-17’ buffer in most locations along that northern boundary.
9. Your Blocks are way out of order and don’t make a lot of sense. Please renumber the blocks to make the progression more logical. Change “Notes” section accordingly.
10. Large number of your common lot designations in the “Notes” section are wrong. Please make sure all lot/block designations for common lots are shown correctly.
11. Your sheet page numbers are not corresponding to PP-01. Pages 5, 7, and 9 are mis-numbered.
12. Submit a plan on how to coordinate emergency response from Middleton PD and how the Police Department will be able to patrol the private streets on a regular basis and administer tickets when necessary.

thanks

Roberta L. Stewart

Middleton City Planner



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The Quarry East – Planner Comments for Preliminary Plat revised 1/27/2022

2/4/2022

Quite a few of these comments were listed two earlier planner comments. Please make the requested changes so we do not have to go over these details a fourth time.

1. In Phase 1 (Block 2), rename loop street "E. Diabase Street." Remove street names S. Chalk Ave., and S. Statham Ave., from the loop road. The Diabase street name should extend north to the dead end at Lot 44, Block 2.
2. Separate the Common lots known as Lot 2, Block 2 and Lot 2, Block 1. I think you have blended one big common lot and put the same lot in two different blocks. Looks like you need to break down common lot #2 into 2 separate pieces: one for block 1 and one for block 2.
3. Please show the various street lots #s better. I know the streets are general lot 1 of various blocks, but we can't see where those street lots start and stop.
4. You have two Lots 81 in Block 3, and you're missing Lot 82. Please delete one of the "Lot 81" and add a Lot 82 in Block 3.
5. I can't find these lots: L31/B6; L71/B6, L73/B6, L2/B7, L42/B7, L26/B8.
6. Show the 150' right of way triangle for the multi-lane roundabout at Lincoln & Middleton Rd.
7. Add hatching or some other marking on the pre-plat to show us which lots are being dedicated to duplex lots.
8. Make all duplex lots at least 4000 s.f., and 40' wide frontage per the DA requirements. Some lots are below 4000 s.f. and below 40' wide.
9. Make all traditional single family lots a minimum of 5000 s.f. and at least 50' wide frontage per the DA.
10. In the land use summary, breakdown the numbers of residential lots dedicated to (1) duplex lots, (2) cluster lots, and (3) traditional single family lots.
11. Add note that project is subject to the Development Agreement recorded in the records of Canyon County as Instrument #_____.
12. Change Note 2 to state: "All streets proposed in this development are private streets and shall be constructed in accordance with City of Middleton standards for public streets except for the variances to standards set forth in the Development Agreement, Instrument #_____, recorded in the records of Canyon County."
13. Revise note 12 to state that Building Setbacks are per the DA — not R-3 since the project is zoned M-U.
14. You might want to check Note 16. It's definitely wrong and quite funny under the circumstances.
15. Add a note as follows: "Sewer and water capacity shall be reserved at the time of Construction Drawing for each phase."
16. Add note: "Any existing wells shall be abandoned per IDWR regulations."
17. Add note: "All existing septic systems shall be completely removed and the area recompacted per geotechnical report."
18. Submit a landscape plan
19. Show mailbox clusters

Roberta L. Stewart

Middleton City Planner



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The Quarry East – Planner Comments for Preliminary Plat revised 3/7/2022

3/7/2022

1. Show on pre-plat the right of way needed for a multi-lane roundabout at the Middleton Road entrance. I think you'll need at least 150' triangle, but consult with Engineer Amy Woodruff to determine exactly what is needed.
2. Remove Note 15 about there being no exceptions to Middleton Code. There are tons of exceptions, so this note doesn't make sense.
3. Still need floodplain application.
4. Please understand that per the DA, you will not be able to get a 51st building permit until you have a 2nd full/public access. Based on your phasing plan, you are not showing a 2nd access until Phase 5. If you are planning on constructing a second full public access in phase 1 or 2, please show that on the preliminary plat. You will not be able to move past the 51st building permit with an emergency access, so you may want to make preparations now rather than getting stuck down the line.

Roberta L. Stewart

Middleton Planning & Zoning Director



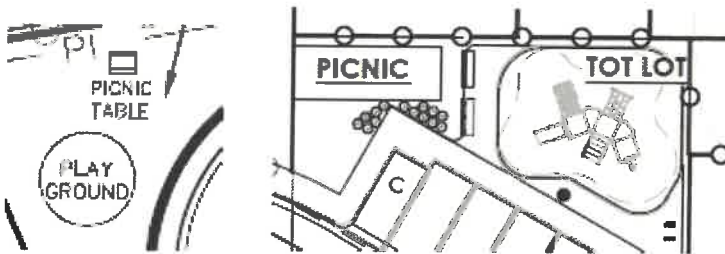
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The Quarry East – Planner Comments for Preliminary Plat revised 3/24/2022

3/29/2022

1. Change the street name “S. Marl Place” to “S. Marl Court”. The City only uses street, avenue, and court designations for street names.
2. Show all community amenities on the preliminary plat. Most engineers show symbols and notes as follows:



Per the DA, you will provide 6 pickle ball courts, clubhouse, swimming pool, beaches and picnic area. In order to better clarify your responsibilities, I added the following language in red font to the DA:

3.14 Community Amenities. Developer shall provide at a minimum the following community amenities: community swimming pool, clubhouse, six (6) pickleball courts, picnic area, and community beach. The design, the timing of construction, and programming of such community amenities is at the sole discretion of Developer. **Swimming Pool/clubhouse shall be located on common lot 62, Block 1 and shall be built generally consistent with the elevation rendering attached hereto as Exhibit _____.**

Please send an elevation rendering of the clubhouse to be attached to the DA as an exhibit.

3. On PP-05, remove word “potential” from note regarding roundabout right of way. Even if you construct only turn lanes or an interim light, a roundabout will be located at the intersection some time in the future pursuant to current planning documents. Therefore, that right of way will need to be more definitively shown on the preliminary plat and actually dedicated on the Phase 1 final plat. Also, enhance the right of way note to reference the Development Agreement, Instrument # _____. This will clarify that the ROW is required by the DA.

Roberta L. Stewart

Middleton Planning & Zoning Director



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The Quarry East – Planner Comments for Preliminary Plat revised 3/30/2022

5/16/2022

1. You have shown the right of way at the main entrance for a future roundabout. Thank you for tying it to the DA. Typically the right of way is 150', so I'm not sure the ROW shown is sufficient. This is an Amy Woodruff question.
2. On Note 3, Lots 157 and 163, Block 3 do not exist and are not common lots. Please remove.
3. On Note 3, add lot 15, Block 4 as common lot.
4. Note 3, add lot 43, block 5 as common lot
5. Is lot 1, Block 6 really a common lot? I think this is supposed to be Lot "10", Block 6.
6. What is Lot 118, Block 3? Is it a common lot? Please add appropriately to Note 3.
7. Lots 24 & 69, Block 1 do not look like emergency access lots. Plus there is no lot 69 in block 1. Please revisit. Also, Block 2 has no lots.
8. On Land Use Summary, note that there are only 71 cluster lots rather than 72.

Roberta L. Stewart

Middleton Planning & Zoning Director

EXHIBIT "E"

P&Z Commission FCR & Order



Middleton Planning & Zoning Commission

Findings of Facts, Conclusions of Law, Recommendation & Order

In the Matter of the applications of M3 and JUB Engineers for annexation/rezone, preliminary plat, development agreement, comprehensive plan map amendment and special use permit with respect to the Quarry East Subdivision located at 21500 Middleton Road and 11436 Lincoln Road:

A. Findings of Fact:

1. Hearing Facts: See Staff Report for the hearing date of April 11, 2022, which Report is attached hereto as Exhibit "A" and incorporated herein by this reference.
2. Process Facts: See Staff Report for the hearing date of April 11, 2022, Exhibit "A".
3. Application and Property Facts: See Staff Report for the hearing date of April 11, 2022, Exhibit "A".
4. Required Findings per Middleton City Code 1-14-2(E)(7), Idaho State Statue Title 67, Chapter 65, Idaho Standards for Public Works Construction and Middleton Supplement thereto, Middleton City Code 1-14, 1-15, 5-1, 5-2, 5-3 & 5-4 and Idaho Code Title 67, Chapter 65 & Title 50, Chapters 2 and 13: See Staff Report for the hearing date of April 11, 2022, Exhibit "A".

B. Conclusions of Law:

1. That the City of Middleton has exercised the powers conferred upon it by the "Local Land Use Planning Act of 1975," codified at Chapter 65, Title 67, Idaho Code (I.C. §67-6503).
2. That due consideration has been given to the comments received from the governmental subdivisions providing services in the City of Middleton planning jurisdiction, comments received from individuals of the public, and comments from City Planning Staff and City Engineer.
3. That notice of the application and public hearing was given according to law.
4. That Planning and Zoning Commission's public hearing was conducted according to law, and the City has kept a record of the application and related documents.
5. That codes and standards pertinent to the application are the Idaho Standards for Public Works Construction, the Middleton Supplement to the Idaho Standards for Public Works Construction, and Middleton City Code 1-14, 1-15, 5-1, 5-2, 5-3, and 5-4 and Idaho Code Title 67, Chapter 65 and Idaho Code Title 50, chapters 2 and 13.

6. That public facilities and services required by the proposed development will not impose expense upon the public if the attached conditions of approval are imposed.

C. Decision & Recommendation:

Pursuant to the Planning & Zoning Commission's authority as provided in Middleton City Code 1-5-5, and based upon the above Findings of Facts and Conclusions of Law, it is hereby recommended that:

1. City Council approve the application of M3 and JUB Engineers for Annexation/Rezone subject to the conditions of approval set forth in the Staff Report for the April 11, 2022, public hearing.
2. City Council approve the application of M3 and JUB Engineers for Development Agreement subject to the conditions of approval set forth in the Staff Report for the April 11, 2022, public hearing.
3. City Council approve the application of M3 and JUB Engineers for preliminary plat subject to the conditions of approval set forth in the Staff Report for the April 11, 2022, public hearing.
4. City Council approve the application of M3 and JUB Engineers for Comprehensive Plan Map Amendment subject to the conditions of approval set forth in the Staff Report for the April 11, 2022, public hearing.

D. Decision & Order:

Pursuant to the Planning & Zoning Commission's authority as provided in Middleton City Code 1-5-5, and based upon the above Findings of Facts and Conclusions of Law, it is hereby Ordered that:

1. The application of M3 and JUB Engineers for special use permit to construct duplex home lots in the Quarry Subdivision zoned M-U is approved subject to the conditions of approval set forth in the Staff Report for the April 11, 2022, public hearing.
2. The application of M3 and JUB Engineers for special use permit to construct a mini self-storage with RV parking in the Quarry Subdivision zoned M-U is approved subject to the conditions of approval set forth in the Staff Report for the April 11, 2022, public hearing.

WRITTEN RECOMMENDATION AND ORDER APPROVED ON: May 9TH, 2022.


Ray Waltemate, Chairman
Planning and Zoning Commission

Attest:



Roberta Stewart
Planning and Zoning Official

Please take notice that pursuant to MCC 1-14-2(E)(10), applicant shall have 14 days after a signed final decision to request reconsideration by the final-decision maker. Such request must identify specific deficiencies in the final decision. Failure to request reconsideration may invalidate a subsequent judicial appeal. Additionally, pursuant to Idaho State Statute 67-6521, any affected person aggrieved by a final decision may, within 28 days after all remedies have been exhausted under local ordinances, seek judicial review as provided in chapter 52, Title 67.

EXHIBIT F

Proposed Future Land Use Map

AREA OF IMPACT BOUNDARY PENDING CANYON COUNTY APPROVAL

City of Middleton Future Land Use Map

